

CITY OF
FOLSOM
DISTINCTIVE BY NATURE

PLANNING COMMISSION AGENDA
November 7, 2018
CITY COUNCIL CHAMBERS
6:30 p.m.
50 Natoma Street
Folsom, California 95630

CALL TO ORDER PLANNING COMMISSION: Jennifer Lane, Kevin Mallory, Ross Jackson, Aaron Ralls, Vice Chair John Arnaz, Kevin Duewel, Chair Justin Raithel

Any documents produced by the City and distributed to the Planning Commission regarding any item on this agenda will be made available at the Community Development Counter at City Hall located at 50 Natoma Street, Folsom, California and at the table to the left as you enter the Council Chambers. The meeting is available to view via webcast on the City's website the day after the meeting.

PLEDGE OF ALLEGIANCE

CITIZEN COMMUNICATION: The Planning Commission welcomes and encourages participation in City Planning Commission meetings, and will allow up to five minutes for expression on a non-agenda item. Matters under the jurisdiction of the Commission, and not on the posted agenda, may be addressed by the general public; however, California law prohibits the Commission from taking action on any matter which is not on the posted agenda unless it is determined to be an emergency by the Commission.

MINUTES

The minutes of October 17, 2018 will be presented for approval.

NEW BUSINESS

1. PN 18-348, Russell Ranch Phase 1, Villages 6 and 8 Residential Design Review

A Public Hearing to consider a request from The New Home Company for Residential Design Review approval for 95 single-family residential units located on two sites within Phase 1, Villages 6 and 8 of the Russell Ranch Subdivision generally located east of Placerville Road and north of Grand Prairie Road. The zoning classification for the site is SFHD PD (FPASP) and the General Plan land-use designation is SFHD. An Environmental Impact Report has previously been certified for the Russell Ranch Subdivision project on May 15, 2015 by the City Council in accordance with the requirements of the California Environmental Quality Act (CEQA) Guidelines. **(Project Planner: Principal Planner, Steve Banks / Applicant: The New Home Company)**

2. PN 18-349, Russell Ranch Phase 1, Villages 3, 5, and 7 Residential Design Review

A Public Hearing to consider a request from The New Home Company for Residential Design Review approval for 108 single-family residential units located on three sites within Phase 1, Villages 3, 5, and 7

of the Russell Ranch Subdivision generally located east of Placerville Road and north of Grand Prairie Road. The zoning classification for the site is SF PD and SFHD PD (FPASP) and the General Plan land-use designation is SF and SFHD. An Environmental Impact Report has previously been certified for the Russell Ranch Subdivision project on May 15, 2015 by the City Council in accordance with the requirements of the California Environmental Quality Act (CEQA) Guidelines. **(Project Planner: Principal Planner, Steve Banks / Applicant: The New Home Company)**

3. PN 18-179, The Shops at Folsom Ranch – Vesting Tentative Parcel Map and Planned Development Permit and Determination that the Project is Exempt from CEQA

A Public Hearing to consider a request from TK Consulting Inc. for approval of a Vesting Tentative Parcel Map and Planned Development Permit for development of a 27,900-square-foot commercial shopping center on a 5.9-acre site located at the southwest corner of the intersection of East Bidwell Street and Alder Creek Parkway within the Folsom Plan Area. The zoning classification for the site is SP-GC-PD and the General Plan land-use designation is GC. The project qualifies for the exemption and streamlined environmental review authorized by Public Resources Code section 21083.3 and CEQA Guidelines section 15183. **(Project Planner: Principal Planner, Steve Banks / Applicant: TK Consulting Inc.)**

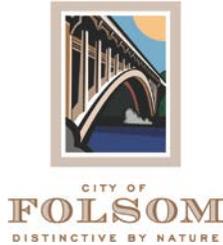
PLANNING COMMISSION / PLANNING MANAGER REPORT

The next Planning Commission meeting is scheduled for **December 5, 2018**. Additional non-public hearing items may be added to the agenda; any such additions will be posted on the bulletin board in the foyer at City Hall at least 72 hours prior to the meeting. Persons having questions on any of these items can visit the Community Development Department during normal business hours (8:00 a.m. to 5:00 p.m.) at City Hall, 2nd Floor, 50 Natoma Street, Folsom, California, prior to the meeting. The phone number is (916) 461-6203 and FAX number is (916) 355-7274.

In compliance with the Americans with Disabilities Act, if you are a disabled person and you need a disability-related modification or accommodation to participate in the meeting, please contact the Community Development Department at (916) 461-6203, (916) 355-7274 (fax) or kmullett@folsom.ca.us. Requests must be made as early as possible and at least two-full business days before the start of the meeting.

NOTICE REGARDING CHALLENGES TO DECISIONS

The appeal period for Planning Commission Action: Any appeal of a Planning Commission action must be filed, in writing with the City Clerk's Office no later than ten (10) days from the date of the action pursuant to Resolution No. 8081. Pursuant to all applicable laws and regulations, including without limitation, California Government Code Section 65009 and or California Public Resources Code Section 21177, if you wish to challenge in court any of the above decisions (regarding planning, zoning and/or environmental decisions), you may be limited to raising only those issues you or someone else raised at the public hearing(s) described in this notice/agenda, or in written correspondence delivered to the City at, or prior to, the public hearing



PLANNING COMMISSION MINUTES
October 17, 2018
CITY COUNCIL CHAMBERS
6:30 P.M.
50 Natoma Street
Folsom, CA 95630

CALL TO ORDER PLANNING COMMISSION: Kevin Mallory, Ross Jackson, Aaron Ralls, Vice Chair John Arnaz, Jennifer Lane, Chair Justin Raithel

ABSENT: Jackson

CITIZEN COMMUNICATION: None

MINUTES: The minutes of October 3, 2018 were approved as submitted.

Oath of Office Administered to Kevin Duewel

NEW BUSINESS

1. PN 18-342 Nomination of the Name Tucker to the Folsom Historic Street Name List and Determination that the Project is Exempt from the CEQA

The applicant, Daniel Tucker, has proposed that the name "Tucker" be added to the Historic Street Name list. The project is exempt from environmental review under Section 15061(b)(3) of the CEQA Guidelines (Review for Exemption). **(Project Planner, Stephanie Henry Traylor, Senior Planner)**

COMMISSIONER DUEWEL MOVED TO APPROVE THE ADDITION OF THE STREET NAME TUCKER TO THE FOLSOM HISTORIC STREET NAME LIST AS DOCUMENTED IN ATTACHMENT 1 WITH THE FOLLOWING FINDINGS: GENERAL FINDINGS A & B AND CEQA FINDING C.

COMMISSIONER LANE SECONDED THE MOTION, WHICH CARRIED THE FOLLOWING VOTE:

AYES: MALLORY, RALLS, ARNAZ, LANE, DUEWEL, RAITHEL
NOES: NONE
ABSTAIN: NONE
ABSENT: JACKSON

2. PN 18-314, Palladio at Broadstone Building 2100 Commercial Design Review and Determination that the Project is Exempt from CEQA

A Public Hearing to consider a request from Broadstone Land, LLC for Commercial Design Review approval for development of a 6,282-square-foot, single-story pad building (Building 2100) on an undeveloped .31-acre parcel within the Palladio at Broadstone Shopping Center located at 210 Palladio Parkway. The zoning classification for the site is C-3 PD and the General Plan land-use designation is RCC. The project is categorically exempt under Section 15303 (New Construction or Conversion of Small Structures) of the California Environmental Quality Act (CEQA) Guidelines. **(Project Planner: Principal Planner, Steve Banks / Applicant: Broadstone Land, LLC)**

COMMISSIONER RALLS MOVED TO APPROVE COMMERCIAL DESIGN REVIEW FOR DEVELOPMENT OF A 6,282-SQUARE-FOOT, SINGLE-STORY RETAIL PAD BUILDING (BUILDING 2100) WITHIN THE PALLADIO AT BROADSTONE SHOPPING CENTER AS ILLUSTRATED ON ATTACHMENTS 2 THROUGH 7 WITH THE FOLLOWING FINDINGS: GENERAL FINDINGS A & B, CEQA FINDINGS C-E, DESIGN REVIEW FINDINGS F-H, AND CONDITIONS OF APPROVAL NO. 1-31.

COMMISSIONER DUEWEL SECONDED THE MOTION, WHICH CARRIED THE FOLLOWING VOTE:

AYES: MALLORY, RALLS, ARNAZ, LANE, DUEWEL, RAITHEL
NOES: NONE
ABSTAIN: NONE
ABSENT: JACKSON

3. PN 18-111, Courts at Russell Ranch – Residential Design Review and Determination that No Additional Environmental Review is Required

A Public Hearing to consider a request from The New Home Company for Residential Design Review Approval for 114 single-family residential units on a 13.5-acre site situated within Phase 1, Village 4 (Courts at Russell Ranch) of the Russell Ranch Subdivision located at the southwest corner of the intersection of Grand Prairie Drive and Rustic Ridge Circle. The Specific Plan land-use designation is MLD PD (FPASP) and the General Plan land-use designation is MLD. An Environmental Impact Report has previously been certified for the Russell Ranch Subdivision project on May 15, 2015 by the City Council in accordance with the requirements of California Environmental Quality Act (CEQA). **(Project Planner: Principal Planner, Steve Banks / Applicant: The New Home Company)**

COMMISSIONER ARNAZ MOVED TO APPROVE THE DESIGN REVIEW APPLICATION FOR 114 SINGLE-FAMILY RESIDENTIAL UNITS AS ILLUSTRATED ON ATTACHMENTS 2 THROUGH 7 FOR THE RUSSELL RANCH PHASE 1, VILLAGE 4 SUBDIVISION (COURTS AT RUSSELL RANCH) PROJECT WITH THE FOLLOWING FINDINGS: GENERAL FINDING A & B, CEQA FINDINGS C-E, DESIGN REVIEW FINDINGS F-H, AND CONDITIONS OF APPROVAL NO. 1-13.

COMMISSIONER DUEWEL SECONDED THE MOTION, WHICH CARRIED THE FOLLOWING VOTE:

AYES: ARNAZ, LANE, DUEWEL, RAITHEL
NOES: MALLORY
ABSTAIN: RALLS
ABSENT: JACKSON

PLANNING MANAGER REPORT

None

RESPECTFULLY SUBMITTED,

Kelly Mullett, SENIOR OFFICE ASSISTANT

APPROVED:

Justin Raithel, CHAIRMAN

PLANNING COMMISSION STAFF REPORT

PROJECT TITLE	Russell Ranch Phase 1, Villages 6 and 8 Residential Design Review
PROPOSAL	Request for Residential Design Review Approval for 95 single-family residential units located within Phase 1, Villages 6 and 8 of the Russell Ranch Subdivision
RECOMMENDED ACTION	Approve, based upon findings and subject to conditions
OWNER/APPLICANT	The New Home Company
LOCATION	The two project sites (9.86-acres and 10.49-acres respectively) are generally located east of Placerville Road and north of Grand Prairie Road within Phase 1 of the Russell Ranch Subdivision
SITE CHARACTERISTICS	The project sites are currently in the process of being rough-graded, with grading work expected to be completed by early December
GENERAL PLAN DESIGNATION	SFHD (Single Family High Density)
ZONING DESIGNATION	SFHD PD (Single- Family High Density, Planned Development District)
ADJACENT LAND USES/ZONING	North: Undeveloped Residential Property (SF PD) with Open Space and Alder Creek Parkway Beyond South: Grand Prairie Road with Undeveloped Residential Property (SF PD) Beyond East: Grand Prairie Road with Undeveloped Residential Property (SF PD) Beyond West: Placerville Road with Undeveloped Residential Property (SFHD PD) Beyond

PREVIOUS ACTION

City Council approval of a General Plan Amendment, Specific Plan Amendment, Large-Lot Vesting Tentative Subdivision Map, Small-Lot Vesting Tentative Subdivision Map, Planned Development Permit, Design Guidelines, Inclusionary Housing Plan, and Amended and Restated Development Agreement Amendment for development of an 879-unit single-family residential subdivision (Russell Ranch Subdivision) on May 15, 2015

City Council approval of an Amended Large-Lot Vesting Tentative Subdivision Map and Small-Lot Vesting Tentative Subdivision Map for development of an 852-unit single-family residential subdivision (Russell Ranch Subdivision) on June 28, 2016

City Council approval of a General Plan Amendment, Specific Plan Amendment, Design Guidelines Amendment, Large-Lot Vesting Tentative Subdivision Map, Small-Lot Vesting Tentative Subdivision Map, and Development Agreement Amendment for the development of a 389 unit residential subdivision (Russell Ranch Lots 24-32 Subdivision) on March 27, 2018

Planning Commission approval of a Design Review Application for 114 single-family residential units located within Phase 1, Village 4 (Courts at Folsom Ranch) of the Russell Ranch Subdivision on October 17, 2018

FUTURE ACTION

Approval of a Final Map and Issuance of Building Permits

APPLICABLE CODES

FPASP (Folsom Plan Area Specific Plan)
Russell Ranch Design Guidelines
FMC 17.06, Design Review

ENVIRONMENTAL REVIEW

An Environmental Impact Report has previously been certified for the Russell Ranch Subdivision project on May 15, 2015 by the City Council in accordance with the requirements of the California Environmental Quality Act (CEQA) and the CEQA Guidelines

ATTACHED REFERENCE MATERIAL

1. Vicinity Map
2. Russell Ranch Phase 1 Lot Mix Exhibit and Site Plan
3. Typical Lot Layout, dated September, 2018
4. Color Street Scene
5. Building Elevations and Floor Plans, dated September 21, 2018
6. Color and Material Palette, dated March 16, 2018
7. Russell Ranch Design Guidelines

PROJECT PLANNER

Steve Banks, Principal Planner

BACKGROUND

On May 15, 2018, the City Council approved a General Plan Amendment, Specific Plan Amendment, Large-Lot Vesting Tentative Subdivision Map, Small-Lot Vesting Tentative Subdivision Map, Planned Development Permit, Design Guidelines, Inclusionary Housing Plan, and Amended and Restated Development Agreement Amendment for development of an 879-unit single-family residential subdivision known as the Russell Ranch Subdivision within the eastern portion of the Folsom Plan Area. As part of the aforementioned approvals, the Russell Ranch Design Guidelines were established to act as an implementation tool for residential development within the Russell Ranch Subdivision.

On June 28, 2016, the City Council approved an Amended Large-Lot Vesting Tentative Subdivision Map and an Amended Small-Lot Vesting Tentative Subdivision Map for development of an 852-unit single-family residential subdivision (Russell Ranch Subdivision). On March 27, 2018, the City Council approval of a General Plan Amendment, Specific Plan Amendment, Design Guidelines Amendment, Large-Lot Vesting Tentative Subdivision Map, Small-Lot Vesting Tentative Subdivision Map, and Development Agreement Amendment for the development of an 389-unit residential subdivision (Russell Ranch Lots 24-32 Subdivision). The Design Guidelines Amendment provided additional direction in terms of the architecture and design of the active-adult community, the community center, and the townhome portions of the Russell Ranch Subdivision.

On October 17, 2018, the Planning Commission approved a Design Review Application for 114 single-family "court-style" residential units located within Phase 1, Village 4 (Courts at Russell Ranch) of the previously approved Russell Ranch Subdivision project. The design review approval for Village 4 included three individual master plans with three distinct California heritage-themed architectural styles (Bungalow, California Cottage, and Craftsman) and nine color and material alternatives.

APPLICANT'S PROPOSAL

The applicant, The New Home Company, is requesting residential design review approval for 95 traditional single-family residential units located within Phase 1, Villages 6 and 8 of the previously approved Russell Ranch Subdivision project. Specifically, the applicant is requesting design review approval for four (4) individual master plans within Villages 6 and 8. The master plans include three (3) distinct California heritage-themed architectural styles (California Prairie, Modern Farmhouse, and Spanish Eclectic) and nine (9) color and material alternatives. The proposed master plans, which feature three, two-story models and one, single-story model, range in size from 1,779 to 2,953 square feet (2BR/2BA to 5BR/4.5BA) and include an attached two-car garage. The three classic design themes are characterized by a variety of unique architectural elements including

varied roof shapes and forms, covered entries, distinct window design, and enhanced decorative elements. Proposed building materials include stucco, vertical and horizontal board and batten siding, brick veneer, stone veneer, decorative wood shutters, wood outlookers and rafter tails, decorative ceramic tiles, form window trim, multi-paned windows, themed garage doors, decorative light fixtures, metal roofing, and concrete roof tiles. In addition, there are 9 distinct color and material alternatives available for each of the master plans resulting in 108 different visual expressions.

Architecture/Design

As described previously, the proposed project features three distinct architectural themes that have been chosen from the traditional heritage of California home styles including California Prairie, Modern Farmhouse, and Spanish Eclectic. The following is a description of each of the aforementioned architectural styles proposed for Villages 6 and 8 within the Russell Ranch Subdivision:

California Prairie

The California Prairie design, also referred to as Modern Prairie, is a late 19th and early 20th century architectural style that has its roots in the City of Chicago. Frank Lloyd Wright, the most famous proponent of this architectural style, promoted the idea of “organic architecture”, the primary principal of which was that a structure should look as if it belongs on the site as if it naturally grew there. California Prairie is a distinctly American style defined by simple structured massing, horizontal lines, and flat or hipped roofs with broad overhanging eaves. The California Prairie style is also recognized for its use of brick, stone, and stucco exteriors with restrained application of ornamental materials.

Modern Farmhouse

Representative of farmhouse vernaculars scattered throughout the American plains and coastal communities, the Modern Farmhouse style recalls the classic spirit of one of the nation’s most enduring designs. Reflecting the essential elements of the aesthetic, the Modern Farmhouse style is simplistic in its form and features predominately gable roofs. The detailing is modest, expressed through wood post columns, distinctive wood trim, and metal roof elements. Porches and single-story elements break up the two-story form and add a sense of movement. Adding to its allure, a playful material palette composed of stucco, board and batten siding, flat concrete roof tiles, and brick veneer further contribute to the inviting nature of this aesthetic.

Spanish Eclectic

Derived from the architectural traditions of colonial Spain, the Spanish Eclectic style features a mixture of formal and casual design details. The Spanish Eclectic style evolved in California and the southwest as an adaptation of Mission Revival infused with additional elements and details from Latin America. The style attained widespread popularity after its use in the Panama-California Exposition of 1915. Key features of this style were adapted to the California lifestyle. Plans were informally organized around a courtyard with the front elevation very simply articulated and detailed. The charm of this style lies in the directness, adaptability, and contrasts of materials and textures.

The proposed project, which is located within the eastern portion of the Folsom Plan Area, is subject to the Russell Ranch Design Guidelines, which were originally approved by the City Council in 2015 and modified on March 27, 2018. The Design Guidelines are a complementary document to the Folsom Plan Area Specific Plan and the Folsom Plan Area Specific Plan Community Guidelines. The Design Guidelines, which are intended to act as an implementation tool for residential development within the Russell Ranch Subdivision, provide the design framework for architecture, streetscene, and landscaping to convey a master plan identity. The Design Guidelines also establish the pattern and intensity of development for the Russell Ranch Subdivision to ensure a high quality and aesthetically cohesive environment. While these Design Guidelines establish the quality of architectural and landscape development for the overall subdivision, they are not intended to prevent alternative designs and/or concepts that are compatible with the overall project theme.

As a regulatory tool, the Design Guidelines are intended to assist applicants in creating single-family residential neighborhoods that reflect the City's rich history, reinforce the sense of community, and utilize sustainable best practices. The Design Guidelines also provide the framework for design review approval of residential projects within the Russell Ranch Subdivision. In addition, the Design Guidelines are intended to be used by builders and developers when designing their Master Plot Plans. Any development project that is submitted to the City must be reviewed for consistency with these Design Guidelines. The following are the general architectural principles intended to guide the design of the Russell Ranch Subdivision to ensure quality development:

- Master Home Plan series must include a minimum of three unique building elevations
- Provide an elegant and diverse streetscape
- Building massing must be appropriate and authentic to the architectural style
- Horizontal and vertical articulation is required
- Include varied roof shapes and forms
- Avoid repetition of identical architectural styles
- Ensure four-sided architecture is provided
- Highly visible lots require additional architectural enhancements

In addition to the general architectural principles referenced above, the Design Guidelines also provide specific direction regarding a number of architectural situations and features including: building forms, roof details, wall materials, window and door designs, and decorative details. Each of the architectural styles is required to include a specific set of style elements (form, roof, walls, windows/doors, and details) based on the particular design theme. Based on the highly visible nature of the residential lots within the Russell Ranch Subdivision, the Design Guidelines also require that corner lots and lots adjacent to open space provide additional enhanced elements above and beyond the minimum required style elements for the building elevations facing the street(s) and

open space areas respectively. The following are examples of required and enhanced style elements that are relevant to the proposed project:

- Provide symmetrical or asymmetrical form
- Utilize simple massing, front or side gabled
- Include a deep front entry porch
- Provide low-pitched roofs with large over-hanging eaves
- Wall materials should include stucco, lap siding, wood shingles, and masonry/brick
- Use windows individually or in groups
- Provide head and sill window trim or full window surrounds
- Utilize two stories with combination of one and two-story elements (enhanced element)
- Include steep pitched accent gable (enhanced element)
- Provide smooth or imperfect smooth stucco (enhanced element)
- Utilize casement windows (enhanced element)
- Feature entry porch columns with single or multiple posts (enhanced element)
- Include exposed rafter tails or eaves (enhanced element)

In reviewing the architecture and design of the project, staff determined that the design of the four proposed master plans (which also include 3 elevation plans, 9 color and material alternatives, and 108 architectural and visual expressions) accurately reflect the level and type of high quality design features recommended by the Russell Ranch Design Guidelines. Specifically, the master plans are responsive to views on all four building elevations and include a variety of unique architectural elements that create an interesting streetscape scene including: varied roof shapes and forms, covered entries, themed garage doors, distinct window design, and enhanced decorative elements. In addition, each of the proposed master plans meet or exceed the minimum and enhanced style element requirements as articulated in the Design Guidelines.

The proposed building materials (stucco, vertical and horizontal board and batten siding, brick veneer, stone veneer, decorative wood shutters, wood outlookers and rafter tails, decorative ceramic tiles, form window trim, multi-paned windows, themed garage doors, decorative light fixtures, metal roofing, and concrete roof tiles) are consistent with the materials recommended by the Russell Ranch Design Guidelines. In addition, the proposed project includes distinct color schemes, which are consistent with each of the architectural styles, thus enhancing the visual interest of each of the master plans. Taking into consideration the aforementioned architectural details, materials, and colors, staff has determined that the master plans are consistent with the design principles established by the Russell Ranch Design Guidelines. Staff forwards the following design recommendations to the Commission for consideration:

1. This approval is for four, one and two-story master plans (3 building elevations with 9 color and material options and 108 visual expressions) for the Russell Ranch Villages 6 and 8 Subdivision. The applicant shall submit building plans that comply with this approval, the attached building elevations dated September 21, 2018.
2. The design, materials, and colors of the proposed Russell Ranch Villages 6 and 8 Subdivision single-family residential units shall be consistent with the submitted building elevations, materials samples, and color scheme to the satisfaction of the Community Development Department.
3. The Community Development Department shall approve the individual lot permits to assure no duplication or repetition of the same house, same roof-line, same elevation style, side-by-side, or across the street from each other.
4. All mechanical equipment shall be ground-mounted and concealed from view of public streets, neighboring properties and nearby higher buildings. For lots abutting the open space areas, mechanical equipment shall be located out of view from open space areas.
5. Lighting shall be designed to be directed downward onto the project site and away from adjacent properties and public rights-of-way. In addition, final exterior building and site lighting plans shall be submitted for review and approval by Community Development Department for aesthetics, level of illumination, glare and trespass prior to the issuance of any building permits.

These recommendations listed above are included in the conditions of approval presented for consideration by the Planning Commission (Condition No. 11).

ENERGY CONSERVATION

To reduce impacts in terms of energy and water consumption, the proposed project is required to meet the 2017 Title 24 Building Envelope Energy Efficiency Standards. The project will be allowed to achieve this performance standard through a combination of measures to reduce energy use for heating, cooling, water heating and ventilation. Because energy use for each different system type (i.e., heating, cooling, water heating, and ventilation) as well as appliances is defined, this method will also easily allow for application of individual measures aimed at reducing the energy use of these devices in a prescriptive manner.

ENVIRONMENTAL REVIEW

An Environmental Impact Report has previously been certified for the Russell Ranch Subdivision project on May 15, 2015 by the City Council in accordance with the requirements of the California Environmental Quality Act (CEQA) and the CEQA Guidelines. Staff has determined that no new impacts will result from development of the subject project that were not already considered with the previous approval. No further environmental review is required.

RECOMMENDATION/PLANNING COMMISSION ACTION

MOVE TO APPROVE THE DESIGN REVIEW APPLICATION FOR 95 SINGLE-FAMILY RESIDENTIAL UNITS AS ILLUSTRATED ON ATTACHMENTS 2 THROUGH 6 FOR THE RUSSELL RANCH PHASE 1, VILLAGES 6 AND 8 SUBDIVISION PROJECT WITH THE FOLLOWING FINDINGS AND CONDITIONS (NO. 1-13).

GENERAL FINDINGS

- A. NOTICE OF HEARING HAS BEEN GIVEN AT THE TIME AND IN THE MANNER REQUIRED BY STATE LAW AND CITY CODE.
- B. THE PROJECT IS CONSISTENT WITH THE GENERAL PLAN, THE FOLSOM PLAN AREA SPECIFIC PLAN, AND THE RUSSELL RANCH SUBDIVISION DESIGN GUIDELINES.

CEQA FINDING

- C. AN ENVIRONMENTAL IMPACT REPORT HAS PREVIOUSLY BEEN CERTIFIED FOR THE RUSSELL RANCH SUBDIVISION PROJECT ON MAY 15, 2015 BY THE CITY COUNCIL IN ACCORDANCE WITH THE REQUIREMENTS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA).
- D. NO NEW IMPACTS WILL RESULT FROM DEVELOPMENT OF THE SUBJECT PROJECT THAT WERE NOT ALREADY CONSIDERED WITH THE PREVIOUS APPROVAL.
- E. NO FURTHER ENVIRONMENTAL REVIEW IS REQUIRED.

DESIGN REVIEW FINDING

- F. THE PROJECT IS IN COMPLIANCE WITH THE GENERAL PLAN, THE FOLSOM PLAN AREA SPECIFIC PLAN, AND THE APPLICABLE ZONING ORDINANCES.
- G. THE PROJECT IS IN CONFORMANCE WITH THE RUSSELL RANCH DESIGN GUIDELINES.
- H. THE BUILDING MATERIALS, TEXTURES, AND COLORS OF THE PROJECT WILL BE COMPATIBLE WITH SURROUNDING DEVELOPMENT AND CONSISTENT WITH THE GENERAL DESIGN THEME OF THE NEIGHBORHOOD.

Submitted,


PAM JOHNS
Community Development Director

CONDITIONS

See attached tables of conditions for which the following legend applies.

RESPONSIBLE DEPARTMENT		WHEN REQUIRED	
CD	Community Development	I	Prior to approval of Improvement Plans
(P)	Planning Division	M	Prior to approval of Final Map
(E)	Engineering Division	B	Prior to issuance of first Building Permit
(B)	Building Division	O	Prior to approval of Occupancy Permit
(F)	Fire Division	G	Prior to issuance of Grading Permit
PW	Public Works Department	DC	During construction
PR	Park and Recreation Department	OG	On-going requirement
PD	Police Department		

**CONDITIONS OF APPROVAL FOR THE RUSSELL RANCH VILLAGES 6 AND 8 SUBDIVISION PROJECT (PN 18-348)
 EAST OF PLACERVILLE ROAD AND NORTH OF GRAND PRAIRIE ROAD
 RESIDENTIAL DESIGN REVIEW**

Mitigation Measure	Condition/Mitigation Measure	When Required	Responsible Department
1.	<p>The applicant shall submit final site development plans to the Community Development Department that shall substantially conform to the exhibits referenced below:</p> <ul style="list-style-type: none"> • Russell Ranch Phase 1 Lot Mix Exhibit and Site Plan • Typical Lot Layout, dated September, 2018 • Color Street Scene • Building Elevations and Floor Plans, dated September 21, 2018 • Color and Material Palette, dated March 16, 2018 <p>This project approval is for Russell Ranch Villages 6 and 8 Subdivision Residential Design Review, which includes architectural and design details for 95 single-family residential units situated within Villages 6 and 8 of the Russell Ranch Subdivision project. Implementation of the project shall be consistent with the above-referenced items as modified by these conditions of approval.</p>	B	CD (P)(E)
2.	<p>Building plans shall be submitted to the Community Development Department for review and approval to ensure conformance with this approval and with relevant codes, policies, standards and other requirements of the City of Folsom.</p>	B	CD (P)(E)(B)
3.	<p>The project approvals granted under this staff report (Residential Design Review) shall remain in effect for two years from final date of approval (November 7, 2020). Failure to obtain the relevant building (or other) permits within this time period, without the subsequent extension of this approval, shall result in the termination of this approval.</p>	B	CD (P)

**CONDITIONS OF APPROVAL FOR THE RUSSELL RANCH VILLAGES 6 AND 8 SUBDIVISION PROJECT (PN 18-348)
EAST OF PLACERVILLE ROAD AND NORTH OF GRAND PRAIRIE ROAD
RESIDENTIAL DESIGN REVIEW**

Mitigation Measure	Condition/Mitigation Measure	When Required	Responsible Department
4.	<p>The owner/applicant shall defend, indemnify, and hold harmless the City and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void, or annul any approval by the City or any of its agencies, departments, commissions, agents, officers, employees, or legislative body concerning the project. The City will promptly notify the owner/applicant of any such claim, action or proceeding, and will cooperate fully in the defense. The City may, within its unlimited discretion, participate in the defense of any such claim, action or proceeding if both of the following occur:</p> <ul style="list-style-type: none"> • The City bears its own attorney's fees and costs; and • The City defends the claim, action or proceeding in good faith <p>The owner/applicant shall not be required to pay or perform any settlement of such claim, action or proceeding unless the settlement is approved by the owner/applicant.</p>	OG	CD (P)(E)(B) PW, PR, FD, PD, NS
DEVELOPMENT COSTS AND FEE REQUIREMENTS			
5.	The owner/applicant shall pay all applicable taxes, fees and charges at the rate and amount in effect at the time such taxes, fees and charges become due and payable.	B	CD (P)(E)
6.	If applicable, the owner/applicant shall pay off any existing assessments against the property, or file necessary segregation request and pay applicable fees.	B	CD (E)
7.	The City, at its sole discretion, may utilize the services of outside legal counsel to assist in the implementation of this project, including, but not limited to, drafting, reviewing and/or revising agreements and/or other documentation for the project. If the City utilizes the services of such outside legal counsel, the applicant shall reimburse the City for all outside legal fees and costs incurred by the City for such services. The applicant may be required, at the sole discretion of the City Attorney, to submit a deposit to the City for these services prior to initiation of the services. The applicant shall be responsible for reimbursement to the City for the services regardless of whether a deposit is required.	B	CD (P)(E)

**CONDITIONS OF APPROVAL FOR THE RUSSELL RANCH VILLAGES 6 AND 8 SUBDIVISION PROJECT (PN 18-348)
 EAST OF PLACERVILLE ROAD AND NORTH OF GRAND PRAIRIE ROAD
 RESIDENTIAL DESIGN REVIEW**

Mitigation Measure	Condition/Mitigation Measure	When Required	Responsible Department
8.	<p>If the City utilizes the services of consultants to prepare special studies or provide specialized design review or inspection services for the project, the applicant shall reimburse the City for actual costs it incurs in utilizing these services, including administrative costs for City personnel. A deposit for these services shall be provided prior to initiating review of the Final Map, improvement plans, or beginning inspection, whichever is applicable.</p>	B	CD (P)(E)
9.	<p>This project shall be subject to all City-wide development impact fees, unless exempt by previous agreement. This project shall be subject to all City-wide development impact fees in effect at such time that a building permit is issued. These fees may include, but are not limited to, fees for fire protection, park facilities, park equipment, Humbug-Willow Creek Parkway, Light Rail, TSM, capital facilities and traffic impacts. The 90-day protest period for all fees, dedications, reservations or other exactions imposed on this project will begin on the date of final approval (November 7, 2018). The fees shall be calculated at the fee rate in effect at the time of building permit issuance.</p>	B	CD (P)(E), PW, PK
10.	<p>The owner/applicant agrees to pay to the Folsom-Cordova Unified School District the maximum fee authorized by law for the construction and/or reconstruction of school facilities. The applicable fee shall be the fee established by the School District that is in effect at the time of the issuance of a building permit. Specifically, the owner/applicant agrees to pay any and all fees and charges and comply with any and all dedications or other requirements authorized under Section 17620 of the Education Code; Chapter 4.7 (commencing with Section 65970) of the Government Code; and Sections 65995, 65995.5 and 65995.7 of the Government Code.</p>	B	CD (P)

ARCHITECTURE/SITE DESIGN REQUIREMENTS

The project shall comply with the following architecture and design requirements:

1. This approval is for four, one and two-story master plans (3 building elevations with 9 color and material options and 108 visual expressions) for the Russell Ranch Villages 6 and 8 Subdivision. The applicant shall submit building plans that comply with this approval, the attached building elevations dated September 21, 2018.
2. The design, materials, and colors of the proposed Russell Ranch Villages 6 and 8 Subdivision single-family residential units shall be consistent with the submitted building elevations, materials samples, and color scheme to the satisfaction of the Community Development Department.
3. The Community Development Department shall approve the individual lot permits to assure no duplication or repetition of the same house, same roof-line, same elevation style, side-by-side, or across the street from each other.
4. All mechanical equipment shall be ground-mounted and concealed from view of public streets, neighboring properties and nearby higher buildings. For lots abutting the open space areas, mechanical equipment shall be located out of view from open space areas.
5. Lighting shall be designed to be directed downward onto the project site and away from adjacent properties and public rights-of-way. In addition, final exterior building and site lighting plans shall be submitted for review and approval by Community Development Department for aesthetics, level of illumination, glare and trespass prior to the issuance of any building permits.

B

CD (P) (B)

11.

FIRE DEPARTMENT REQUIREMENT		
12.	The building shall have illuminated addresses visible from the street or drive fronting the property. Size and location of address identification shall be reviewed and improved by the Fire Marshal.	B
POLICE/SECURITY REQUIREMENT		
13.	<p>The owner/applicant shall consult with the Police Department in order to incorporate all reasonable crime prevention measures. The following security/safety measures shall be required:</p> <ul style="list-style-type: none"> • A security guard shall be on-duty at all times at the site or another approved security measure shall be in place including but not limited to a six-foot security fence shall be constructed around the perimeter of construction areas. (This requirement shall be included on the approved construction drawings). • Security measures for the safety of all construction equipment and unit appliances shall be employed. • Landscaping shall not cover exterior doors or windows, block line-of-sight at intersections or screen overhead lighting. 	B
		PD

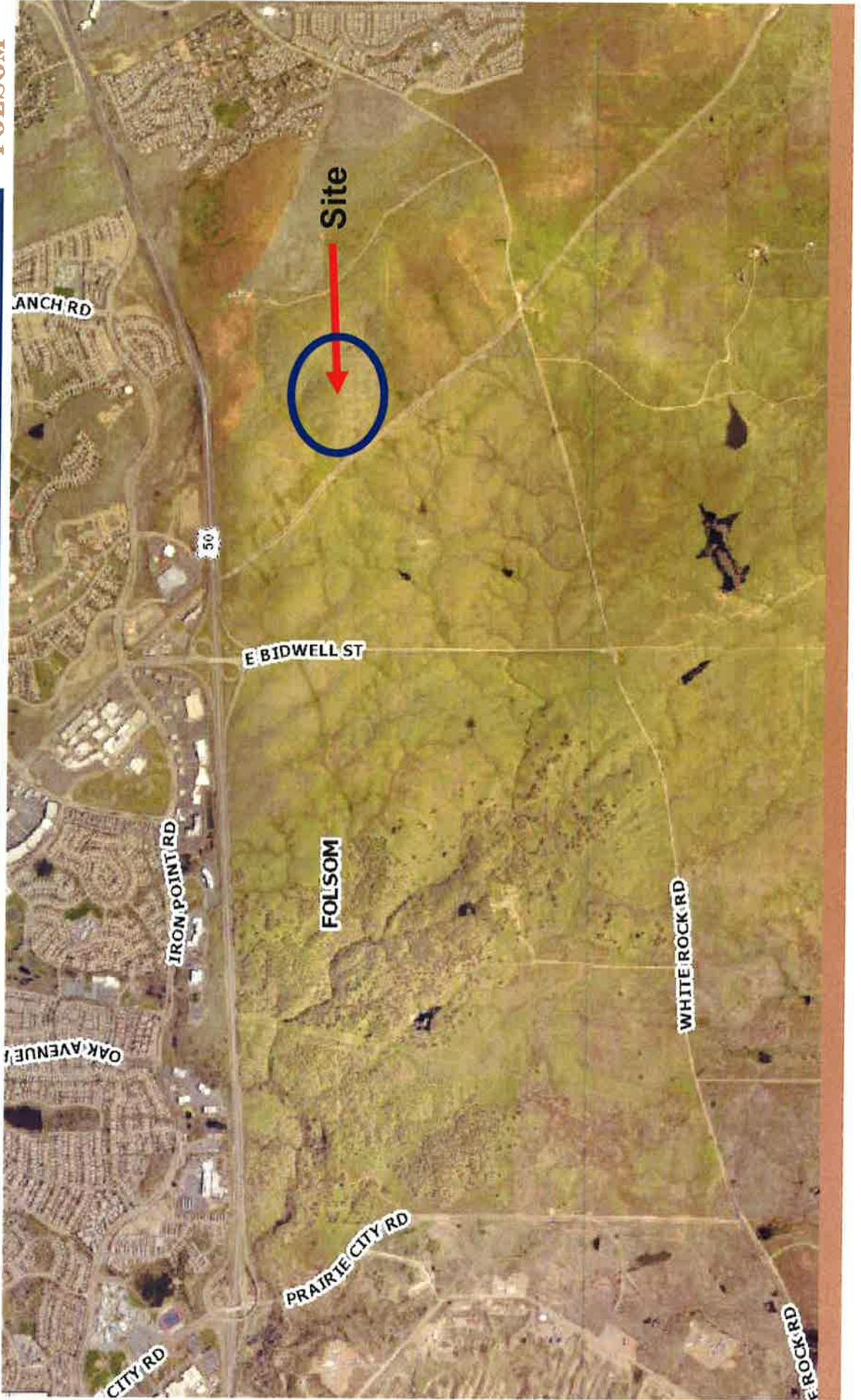
Attachment 1

Vicinity Map



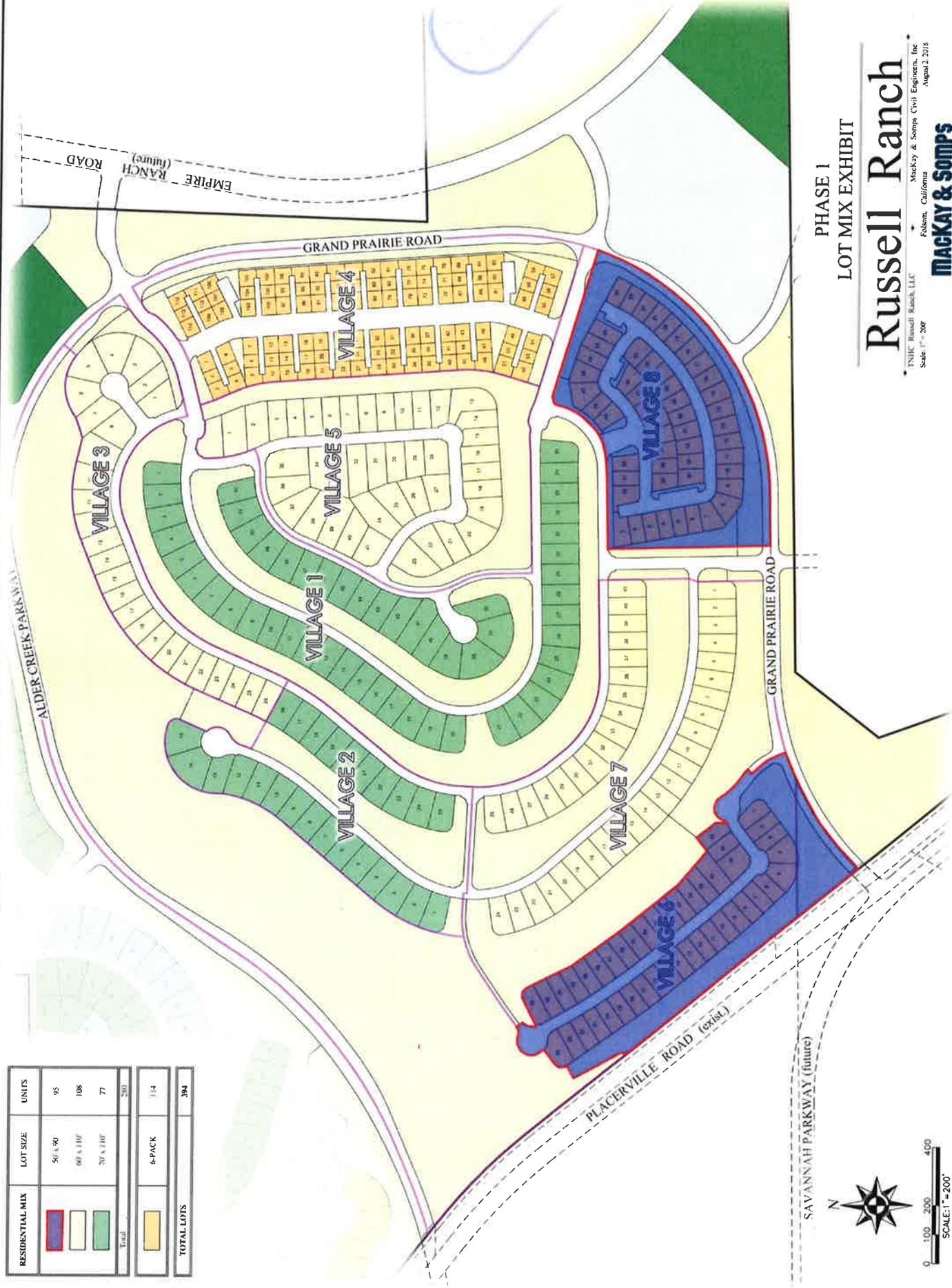
CITY OF
FOLSOM

Vicinity Map



Attachment 2

Russell Ranch Phase 1 Lot Mix Exhibit and Site Plan



RESIDENTIAL MIX	LOT SIZE	UNITS
■	56' x 90'	95
■	60' x 110'	108
■	30' x 110'	77
Total:		280
■	6-PACK	114
TOTAL LOTS		394

PHASE 1
LOT MIX EXHIBIT

Russell Ranch

TNHC Russell Ranch, LLC Mackky & Somps Civil Engineers, Inc.
Folsom, California August 2, 2016
Scale: 1" = 200'

MACKAY & SOMPS
ENGINEERS PLANNERS SURVEYORS



Attachment 3

Typical Lot Layout, dated September, 2018

Attachment 4
Color Street Scene

RUSSELL RANCH

Folsom, California



PLAN THREE | MODERN FARMHOUSE

PLAN TWO | CALIFORNIA PRAIRIE

PLAN ONE | MODERN FARMHOUSE

PLAN FOUR | SPANISH ECLECTIC

SHEET INDEX

- SITE PLAN
- L1.1 PRELIMINARY LANDSCAPE PLAN
- L2.1 PRELIMINARY LANDSCAPE MATERIALS
- A1 SPANISH ECLECTIC STYLE SHEET
- A2 MODERN FARMHOUSE STYLE SHEET
- A3 CALIFORNIA PRAIRIE STYLE SHEET
- A4 PLAN 1 ELEVATIONS
- A5 PLAN 1 "A" ELEVATIONS
- A6 PLAN 1 "B" ELEVATIONS
- A7 PLAN 1 "C" ELEVATIONS
- A8 FLOOR PLAN 1
- A9 SLAB PLAN 1
- A10 PLAN 2 ELEVATIONS
- A11 PLAN 2 "A" ELEVATIONS
- A12 PLAN 2 "B" ELEVATIONS
- A13 PLAN 2 "C" ELEVATIONS

- A14 FLOOR PLAN 2
- A15 SLAB PLAN 2
- A16 PLAN 3 ELEVATIONS
- A17 PLAN 3 "A" ELEVATIONS
- A18 PLAN 3 "B" ELEVATIONS
- A19 PLAN 3 "C" ELEVATIONS
- A20 FLOOR PLAN 3
- A21 FLOOR PLAN 3 OPTIONS
- A22 SLAB PLAN 3
- A23 PLAN 4 ELEVATIONS
- A24 PLAN 4 "A" ELEVATIONS
- A25 PLAN 4 "B" ELEVATIONS
- A26 PLAN 4 "C" ELEVATIONS
- A27 FLOOR PLAN 4
- A28 SLAB PLAN 4

PROPOSED GENERAL STATISTICS:

- Home count: 95 Single Family Homes
- Plan 1: 2 BR + Opt 3 @ Study
2 BA + Opt Powder @ Laundry
1779 S.F.
- Plan 2: 4 BR
3 BA
2407 S.F.
- Plan 3: 4 BR + Opt 5 @ Loft
3 BA
2720 S.F.
- Plan 4: 5 BR
4.5 BA
2953 S.F.

Construction Type: 1-story and 2-story detached single family homes



Builder:
The New Home Company
2220 Douglas Blvd., Suite 240
Roseville, CA 95641
Office: 916-740-3921



Architect:
woodleyarchitecturalgroup.com
2943 Pullman St, Suite A
Santa Ana, CA 92705
Office: 949.553.9919

Attachment 5

Building Elevations and Floor Plans,
Dated September 21, 2018

SPANISH ECLECTIC

Derived from the architectural traditions of colonial Spain, the Spanish Eclectic style has a mix of formal and casual design details.

DESIGN ELEMENTS

- TWO-STORY MASSING
- STUCCO EXTERIOR FINISH
- 'S' SHAPED CONCRETE TILE

ENHANCED DESIGN ELEMENTS

- EXPOSED RAFTER TAILS
- SHAPED RAFTER TAILS
- GLAZED SPANISH TILE
- ARCHED WINDOW



EAGLE CONCRETE ROOFING
COLOR: REFER TO COLOR SCHEDULE

MATERIAL NOTES

1. 'S' CONCRETE ROOF
2. STUCCO
3. CEMENTITIOUS SIDING
4. RAFTER TAILS
5. DECORATIVE SHUTTERS
6. DECO CERAMIC TILE

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SPANISH ECLECTIC | ARCHITECTURAL STYLE

RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA

NOTE: SQUARE FOOTAGE MAY VARY BASED ON CALCULATION METHODS.
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california // 2943 bulldozer st. suite A
san diego, ca 92105 / 760.532.8719



MODERN FARMHOUSE

The Modern Farmhouse is a fresh take on the classic American farmhouse characterized by simple geometric forms with gable roofs and a variety of textures.

DESIGN ELEMENTS

- TWO-STORY HORIZONTAL MASSING
- ASYMMETRICAL MASSING
- GABLED ROOF
- FLAT CONCRETE ROOF TILE
- STUCCO AND BRICK BODY
- RECTANGULAR WINDOWS

ENHANCED DESIGN ELEMENTS

- IMPERFECT SMOOTH STUCCO
- CEMENT BOARD & BATT
- BRICK VENEER AT KEY LOCATIONS
- STANDING SEAM METAL ROOF



1
EAGLE CONCRETE ROOFING 1
COLOR: REFER TO COLOR SCHEDULE

- MATERIAL NOTES**
1. CONCRETE FLAT ROOF
 2. STUCCO
 3. BOARD & BATT
 4. METAL ROOF
 5. BRICK VENEER
 6. OUTLOOKERS
 7. DECORATIVE BRACKET

5
EL DORADO BRICK VENEER
PROFILE: TUNDRA BRICK
COLOR: REFER TO COLOR SCHEDULE

09.21.18

MODERN FARMHOUSE | ARCHITECTURAL STYLE

RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA

NOTE: SQUARE FOOTAGE MAY VARY BASED ON CALCULATION METHODS

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san joaquin, ca 92085 / 949.558.8919



CALIFORNIA PRAIRIE

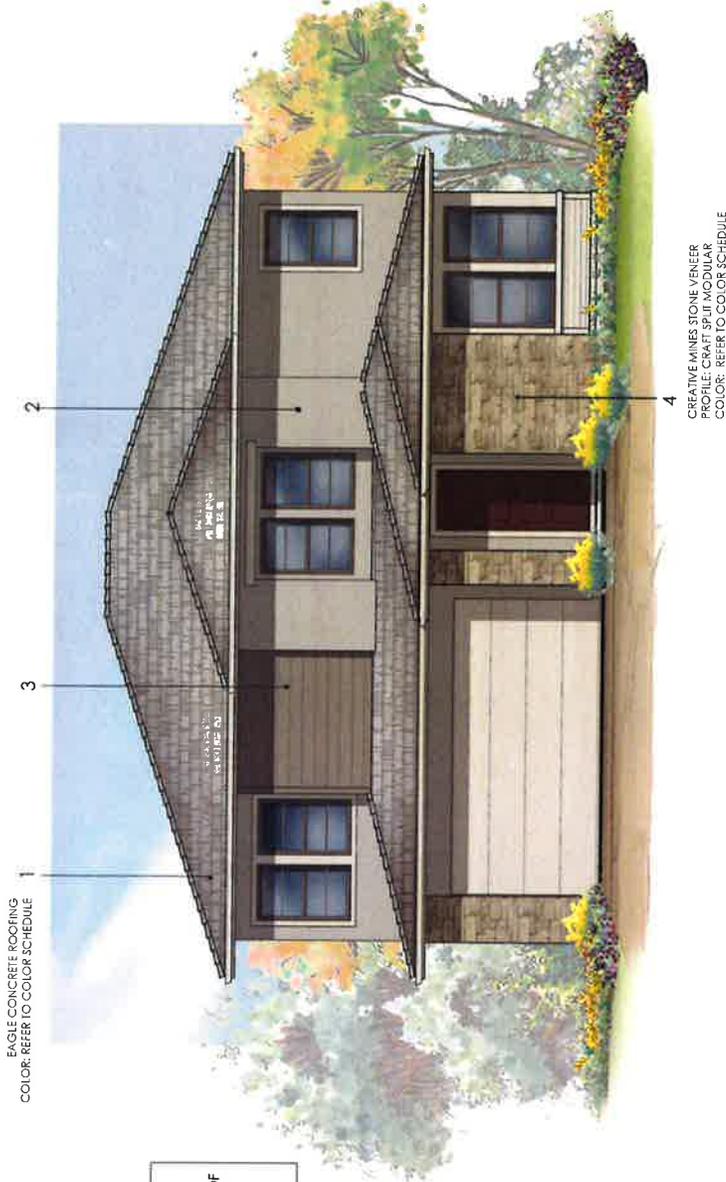
The Modern Prairie is a distinctly American style defined by simple structured massing and horizontal lines. It is recognized by its low hipped roofs and generous roof overhangs.

DESIGN ELEMENTS

- TWO-STORY HORIZONTAL MASSING
- LOW-PITCHED ROOF WITH 36" EAVE OVERHANGS
- FLAT CONCRETE ROOF TILE
- STUCCO AND LEDGE STONE BODY
- RECTANGULAR WINDOWS

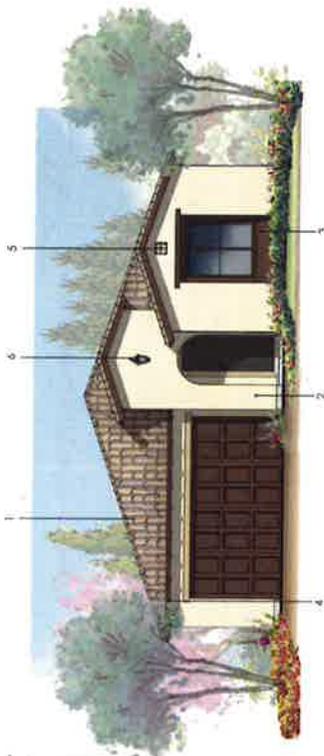
ENHANCED DESIGN ELEMENTS

- 18" OVERHANG
- CEMENT PLANK LAP SIDING
- LEDGE STONE TO EMPHASIZE THE HORIZONTAL PLANE
- ACCENTED COLORED WINDOW FRAME
- WOOD FASCIA



MATERIAL NOTES

1. CONCRETE FLAT ROOF
2. STUCCO
3. HORIZONTAL SIDING
4. STONE VENEER



- MATERIAL NOTES**
1. S. CONCRETE ROOF
 2. STUCCO
 3. CONVENTIONAL SIDING
 4. CERAMIC TILE
 5. ERGO CERAMIC TILE
 6. COACH LIGHT

elevationA- SPANISH ECLECTIC



- MATERIAL NOTES**
1. CONCRETE FLAT ROOF
 2. BOARD & BATT
 3. BOARD & BATT
 4. METAL ROOF
 5. STUCCO
 6. JUILOHNS
 7. DECORATIVE BRACKET
 8. COACH LIGHT

elevationB- MODERN FARMHOUSE



- MATERIAL NOTES**
1. CONCRETE FLAT ROOF
 2. STUCCO
 3. METAL SIDING
 4. STONE VENEER

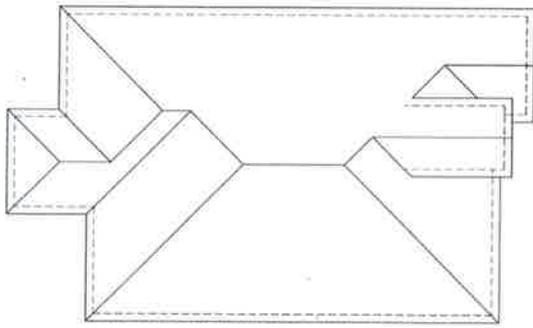
elevationC- CALIFORNIA PRAIRIE

09.21.18

PLAN 1 | FRONT ELEVATIONS

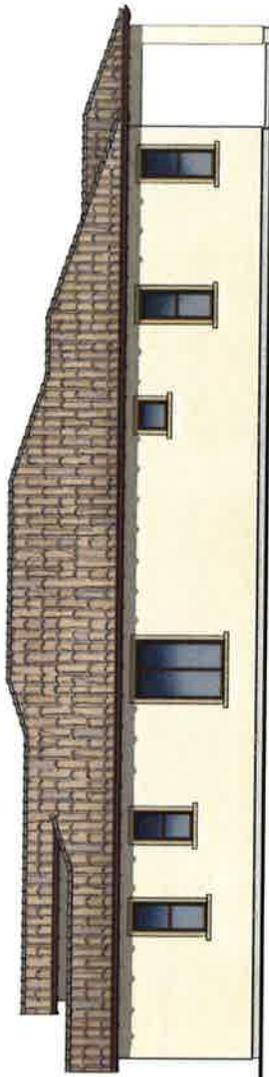
RUSSELL RANCH | 50 X 90 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA

woodley architectural group inc
ARCHITECTS
10000 WOODLEY AVENUE, SUITE 100
FOLSOM, CA 95630
TEL: 916.209.2720
WWW.WOODLEYARCHITECTS.COM



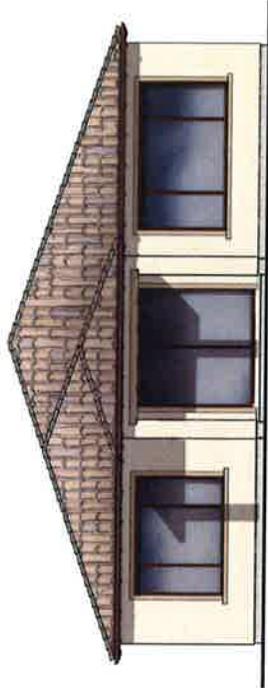
ROOF PLAN

SCALE: 1/8" = 1'-0"



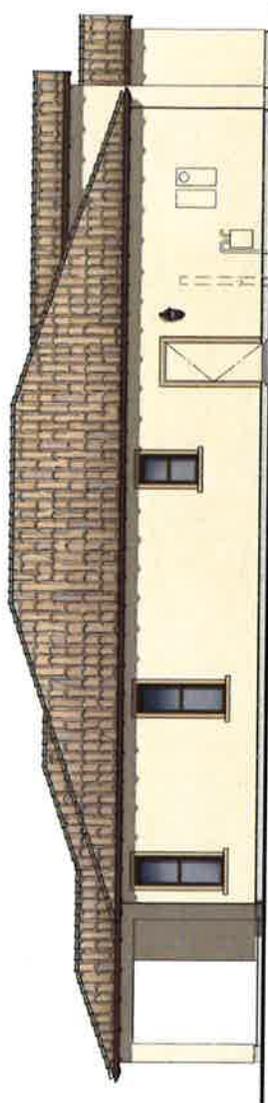
RIGHT ELEVATION

SCALE: 1/8" = 1'-0"



REAR ELEVATION

SCALE: 1/8" = 1'-0"



LEFT ELEVATION

SCALE: 1/8" = 1'-0"

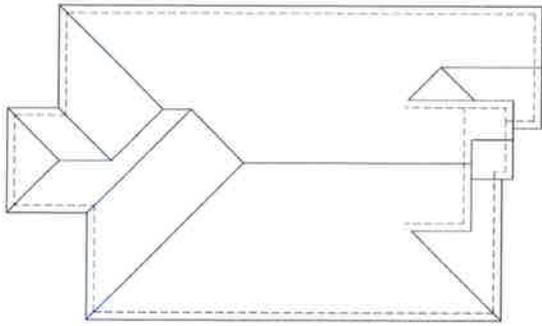
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PLAN 1 | ELEVATION 'A' | SPANISH ECLECTIC
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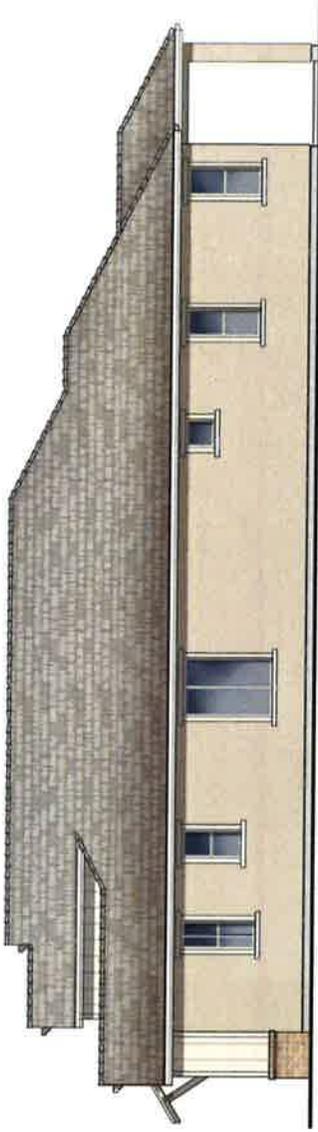
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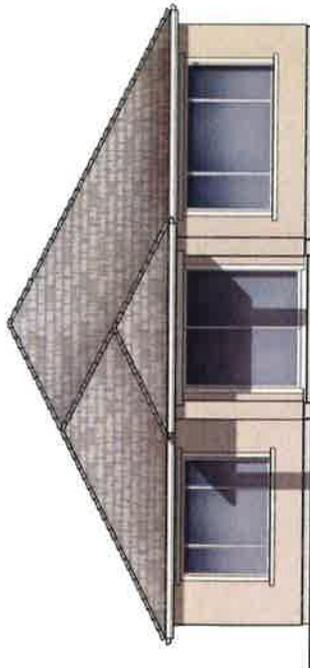
ROOF PLAN

SCALE 1/8" = 1'-0"



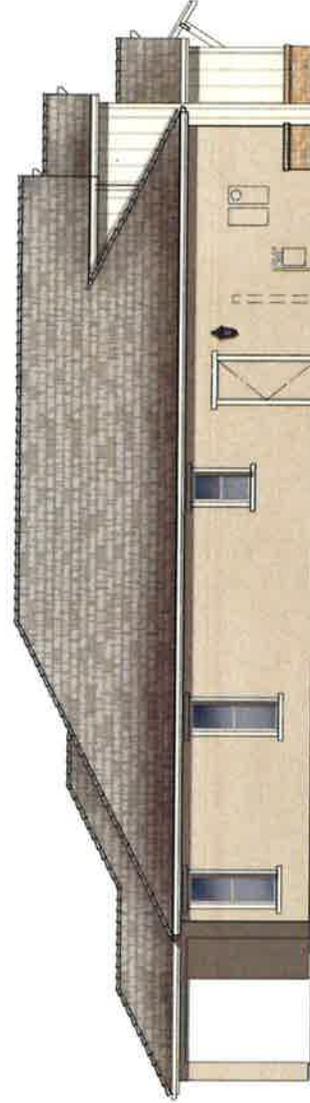
RIGHT ELEVATION

SCALE 1/4" = 1'-0"



REAR ELEVATION

SCALE 1/4" = 1'-0"



LEFT ELEVATION

SCALE 1/4" = 1'-0"

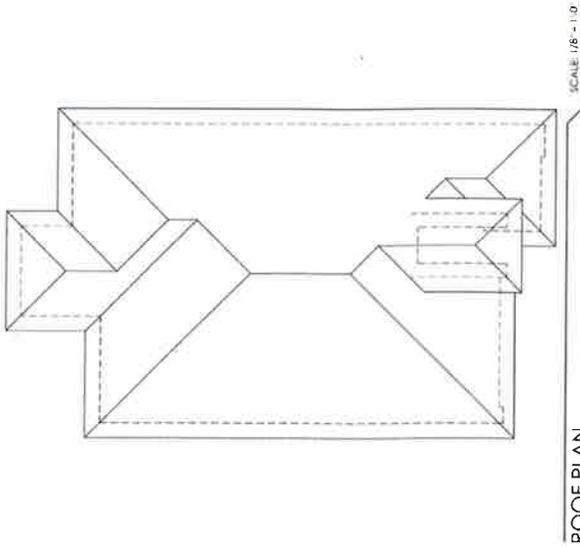
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PLAN 1 | ELEVATION 'B' | MODERN FARMHOUSE
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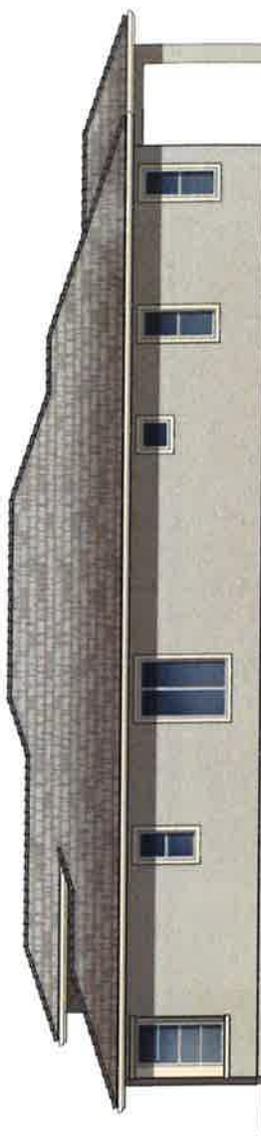


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 colorado 11/28, 1300 563 7334
 colorado 11/28, 1300 563 7334
 colorado 11/28, 1300 563 7334

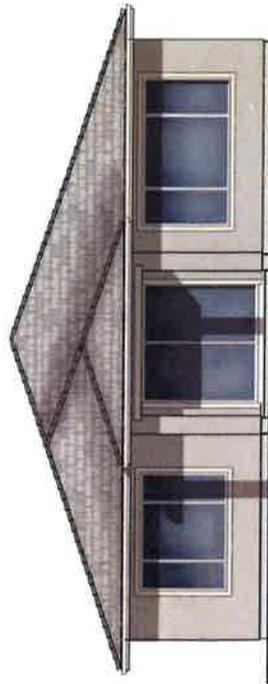
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ROOF PLAN
SCALE: 1/8" = 1'-0"



RIGHT ELEVATION
SCALE: 1/4" = 1'-0"



REAR ELEVATION
SCALE: 1/4" = 1'-0"



LEFT ELEVATION
SCALE: 1/4" = 1'-0"

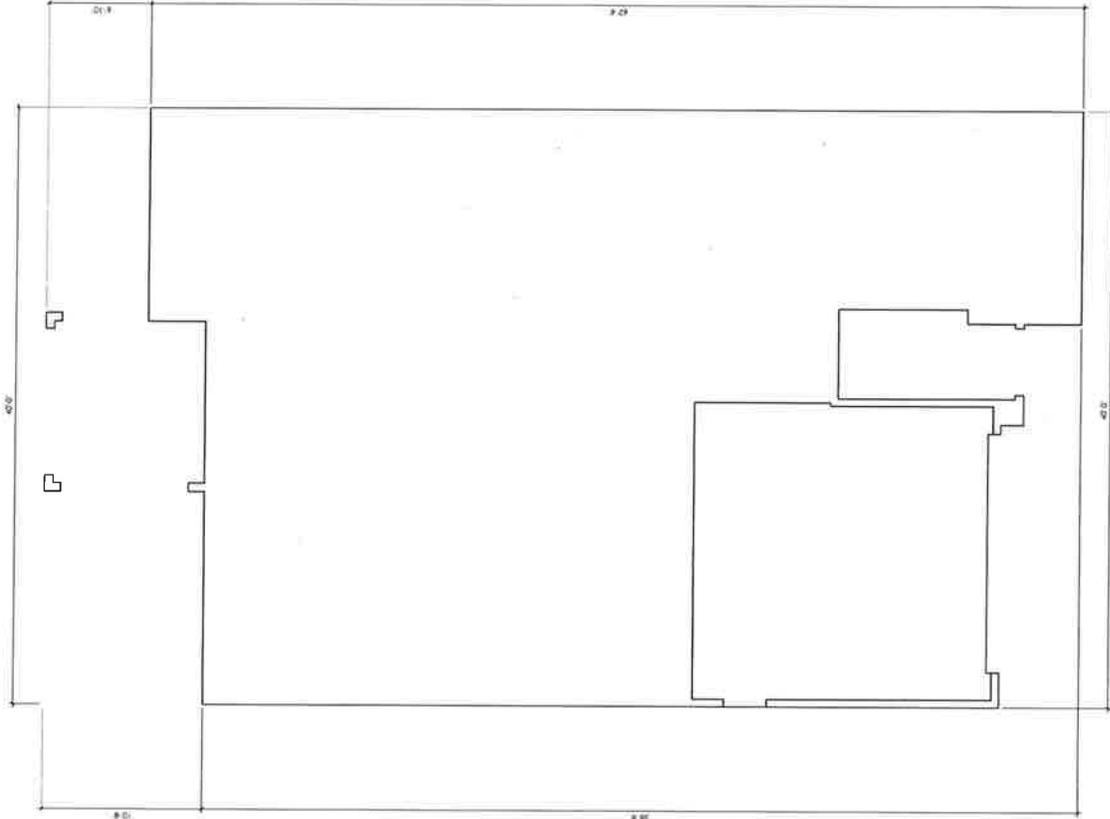
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PLAN 1 | ELEVATION 'C' | CALIFORNIA PRAIRIE
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 FOLSOM, CALIFORNIA



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SLAB EDGE PLAN 'A'
1/4" = 1'-0"

SPANISH ECLECTIC

PLAN ONE

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FOLSOM, CALIFORNIA

NOTE: SEE ARCHITECTURAL SPECIFICATIONS FOR MATERIALS AND FINISHES.

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- MATERIAL NOTES
- 1 CONCRETE ROOF
 - 2 STUCCO
 - 3 CEMENTIOUS SIDING
 - 4 DECORATIVE BRACKETS
 - 5 DECORATIVE SHUTTERS

elevationA- SPANISH ECLECTIC SCALE: 1/4" = 1'-0"



- MATERIAL NOTES
- 1 CONCRETE FLAT ROOF
 - 2 BOARD & BATT
 - 3 METAL ROOF
 - 4 OUTDOOR LIGHTS
 - 5 DECORATIVE BRACKET
 - 6 DECORATIVE SHUTTERS

elevationB- MODERN FARMHOUSE SCALE: 1/4" = 1'-0"



- MATERIAL NOTES
- 1 CONCRETE FLAT ROOF
 - 2 STUCCO
 - 3 CEMENTIOUS SIDING
 - 4 STONE VENEER

elevationC- CALIFORNIA PRAIRIE SCALE: 1/4" = 1'-0"

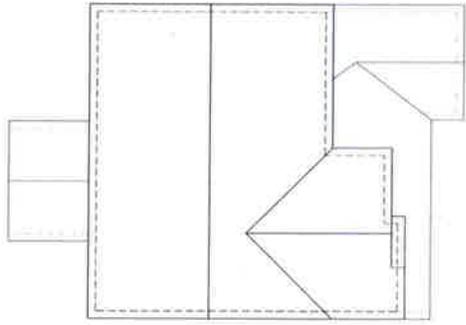
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PLAN 2 | FRONT ELEVATIONS

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FOLSOM, CALIFORNIA



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SCALE: 1/8" = 1'-0"

ROOF PLAN



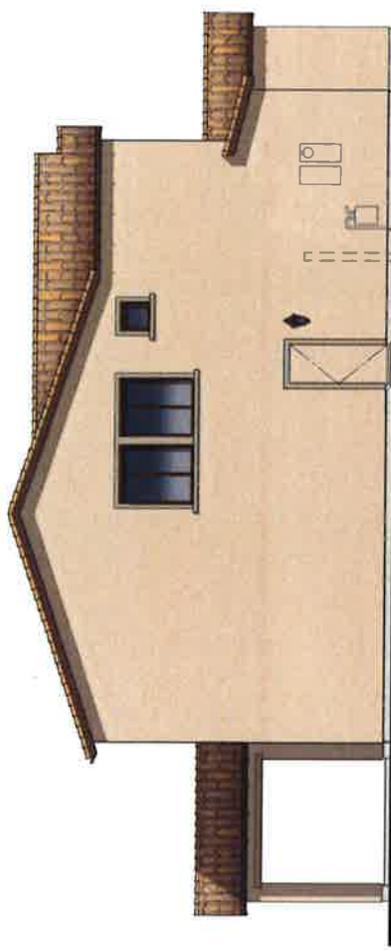
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RIGHT ELEVATION



SCALE: 1/4" = 1'-0"

REAR ELEVATION



SCALE: 1/4" = 1'-0"

LEFT ELEVATION

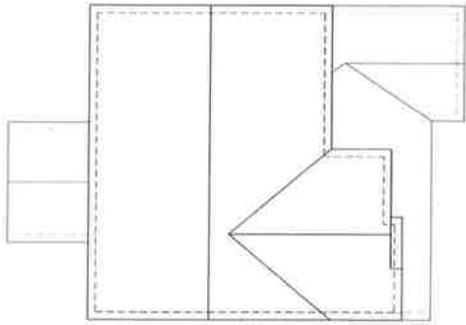
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PLAN 2 | ELEVATION 'A' | SPANISH ECLECTIC
RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY

FOLSOM, CALIFORNIA

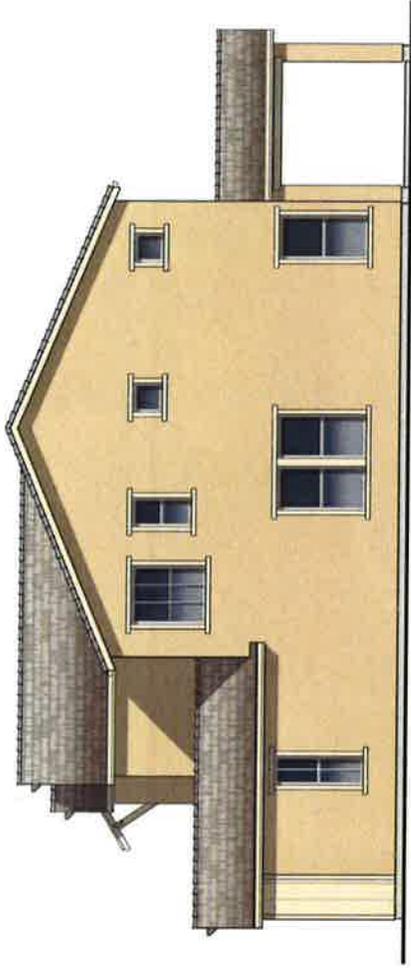


woodley architectural group, inc.
 6000 17311 Southpark Ct. Suite B
 Folsom, CA 95630
 California | 924.471.1100
 San Joaquin | 925.555.8919



ROOF PLAN

SCALE: 1/8" = 1'-0"



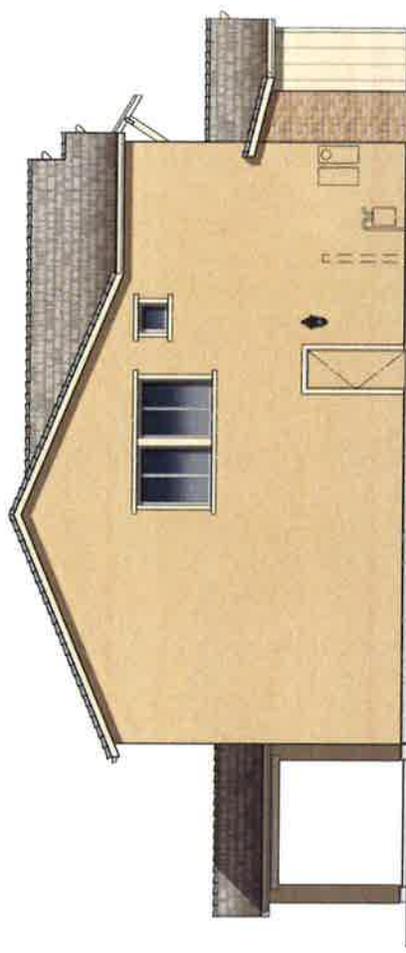
RIGHT ELEVATION

SCALE: 1/4" = 1'-0"



REAR ELEVATION

SCALE: 1/4" = 1'-0"



LEFT ELEVATION

SCALE: 1/4" = 1'-0"

09.21.18

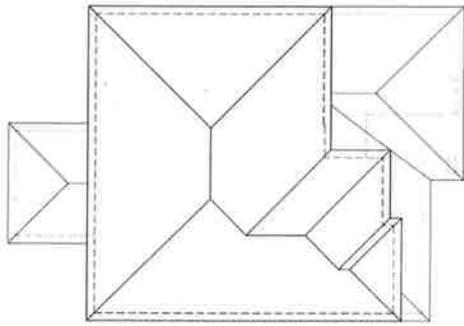
PLAN 2 | ELEVATION 'B' | MODERN FARMHOUSE
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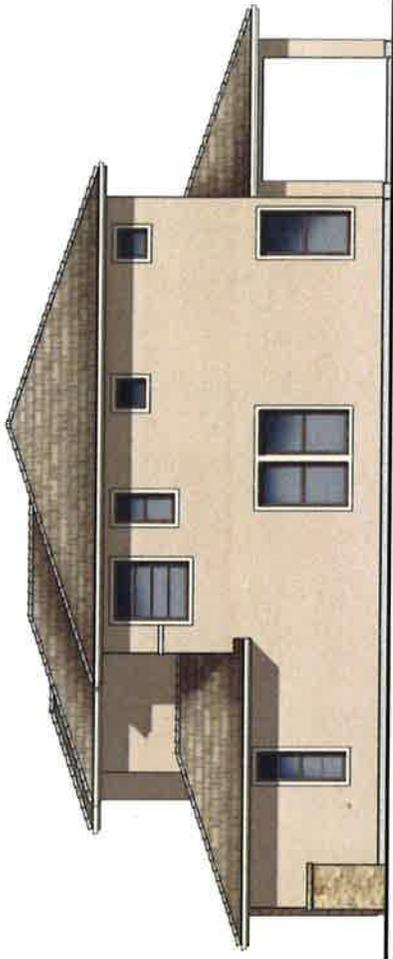
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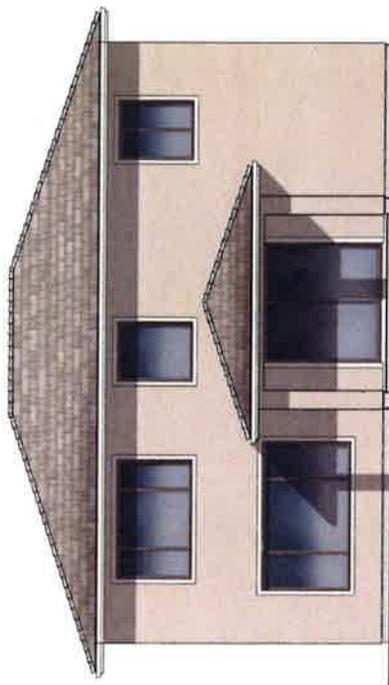
ROOF PLAN

SCALE: 1/8" = 1'-0"



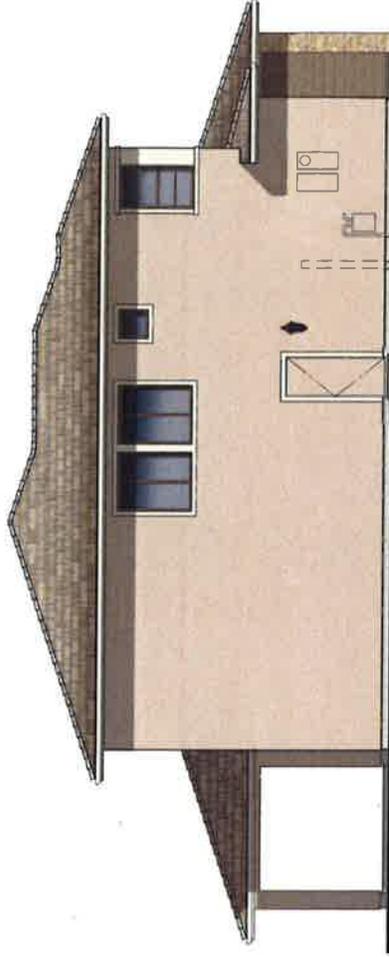
RIGHT ELEVATION

SCALE: 1/4" = 1'-0"



REAR ELEVATION

SCALE: 1/8" = 1'-0"



LEFT ELEVATION

SCALE: 1/4" = 1'-0"

09.21.18

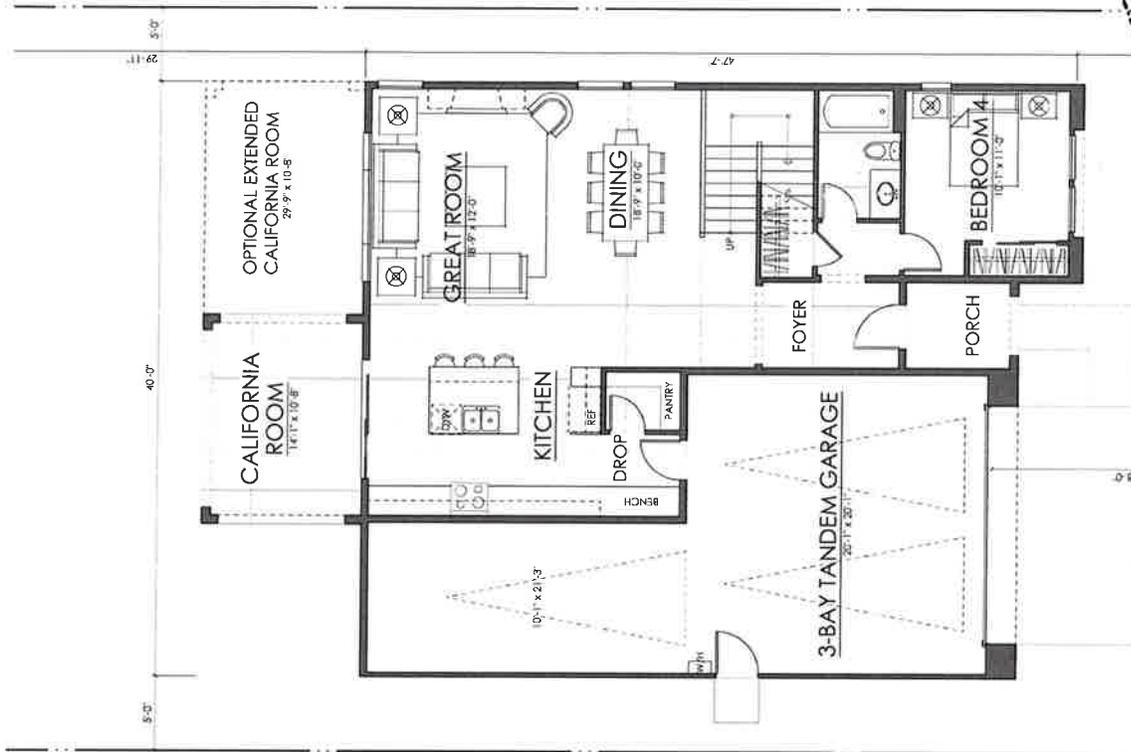
PLAN 2 | ELEVATION 'C' | CALIFORNIA PRAIRIE
RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA



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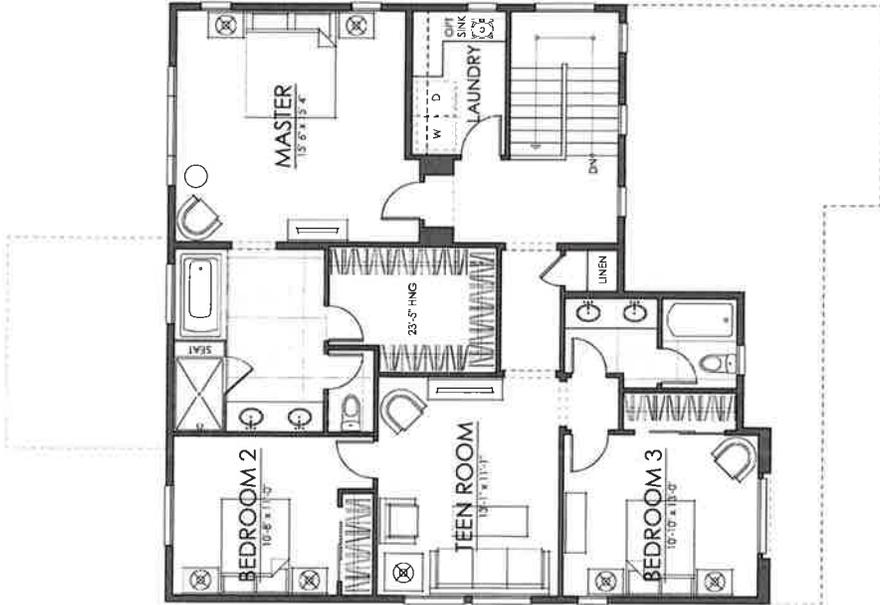


LOT SIZE 50'-0" X 90'-0"

FIRST FLOOR 1080 SQ. FT.
 SECOND FLOOR 1327 SQ. FT.
TOTAL LIVING 2407 SQ. FT.

LOT COVERAGE 1768 SQ. FT.
 MAX COVERAGE 2250 SQ. FT.

PLAN TWO | 2407 SQ. FT.



09.21.18

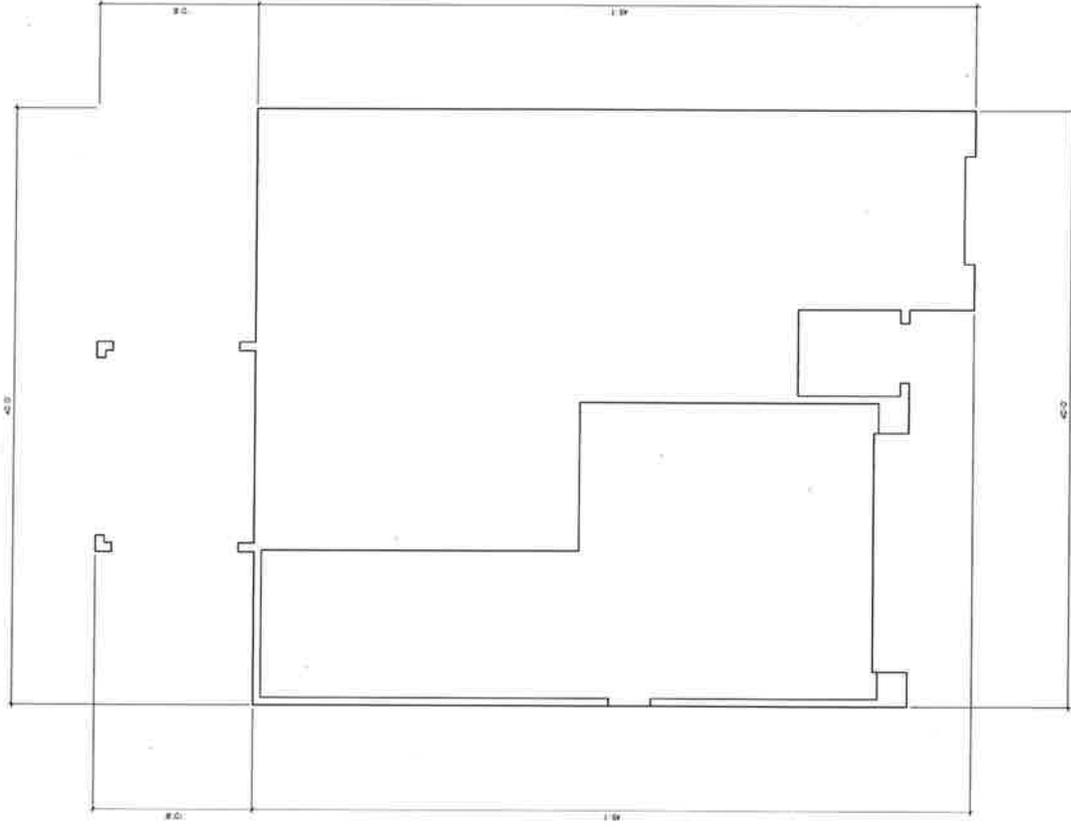
RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA

SWOODLEY
architectural
group inc

colorado 7721 southpark circle suite 8
 colorado 10000 north
 colorado 17243 colorado road
 colorado 97205 / 946.553.8919



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SLAB EDGE PLAN 'A'
1/4" = 1'-0"

SPANISH ECLECTIC

PLAN TWO

RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
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NOTE: SEE SHEET 100-1 FOR THE LOCATION OF THE SLAB EDGE PLAN 'A'.

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10000 S. 100th Ave., Suite B
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 415.533.8919
 10000 S. 100th Ave., Suite A
 Richmond, CA 94603
 415.533.8919



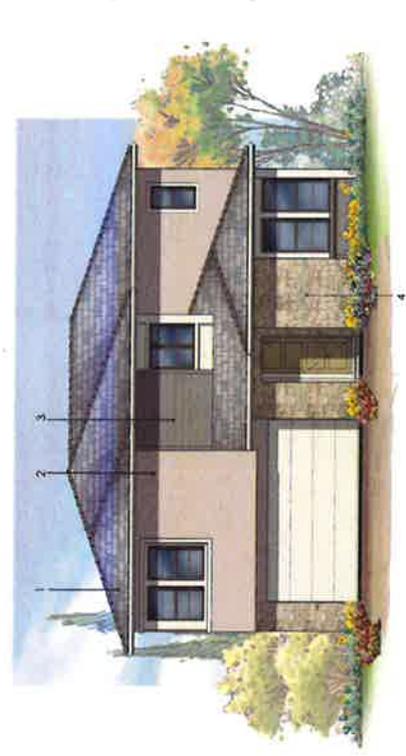
- MATERIAL NOTES**
1. 1" CONCRETE ROOF
 2. STUCCO
 3. CERAMIC SIDING
 4. CERAMIC TILE
 5. DECORATIVE SHUTTERS
 6. DISCO CERAMIC TILE

elevationA- SPANISH ECLECTIC



- MATERIAL NOTES**
1. CONCRETE FLAT ROOF
 2. STUCCO
 3. SIDING & SILL
 4. SHAPED CORBELS
 5. CERAMIC TILE
 6. OUTLINE
 7. DECORATIVE SHUTTERS

elevationB- MODERN FARMHOUSE



- MATERIAL NOTES**
1. CONCRETE FLAT ROOF
 2. STUCCO
 3. CERAMIC SIDING
 4. SPANISH TILE

elevationC- CALIFORNIA PRAIRIE

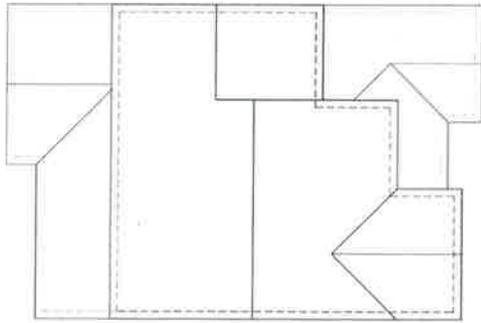
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PLAN 3 | FRONT ELEVATIONS

RUSSELL RANCH | 50 X 90 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA

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ROOF PLAN

SCALE: 1/8" = 1'-0"



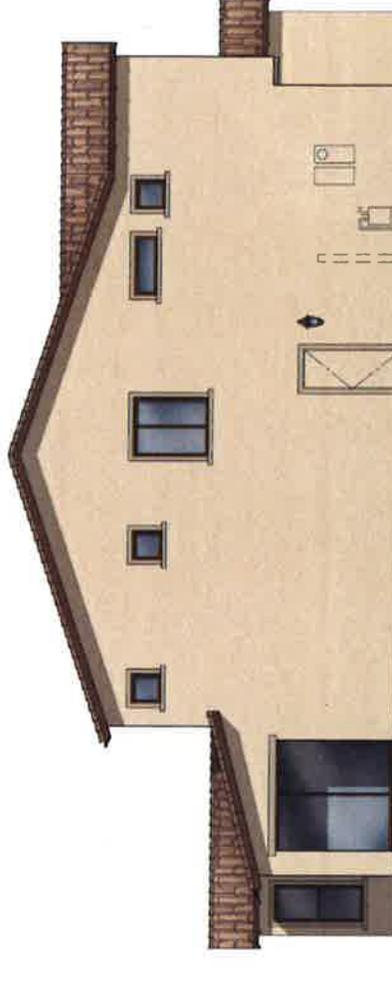
RIGHT ELEVATION

SCALE: 1/4" = 1'-0"



REAR ELEVATION

SCALE: 1/4" = 1'-0"



LEFT ELEVATION

SCALE: 1/4" = 1'-0"

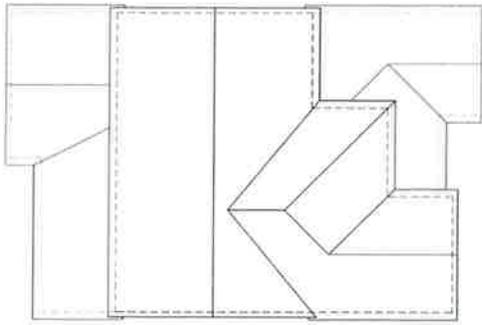
09.21.18

PLAN 3 | ELEVATION 'A' | SPANISH ECLECTIC
RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA



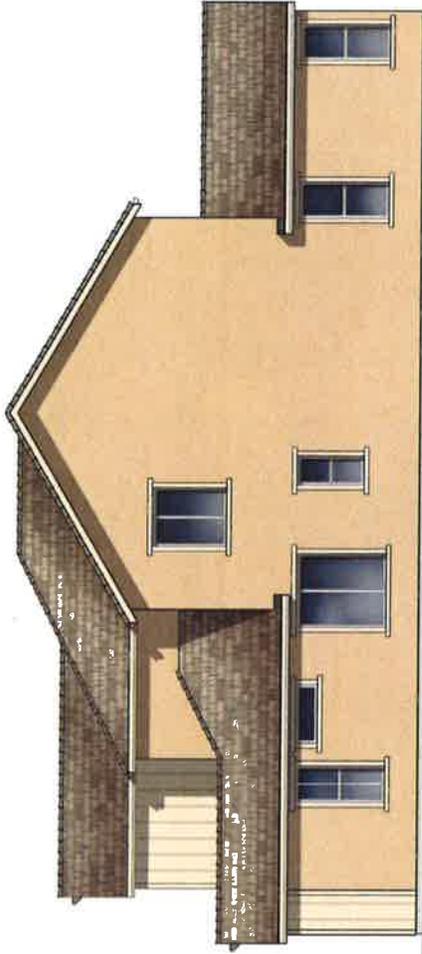
colorado // 731 southpark dr, suite 8
 littleton, co 80120 // 303.685.7231
 colorado // 10000 north
 santa ana, ca 92705 // 949.553.8919

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ROOF PLAN

SCALE: 1/8" = 1'-0"



RIGHT ELEVATION

SCALE: 1/4" = 1'-0"



REAR ELEVATION

SCALE: 1/4" = 1'-0"



LEFT ELEVATION

SCALE: 1/4" = 1'-0"

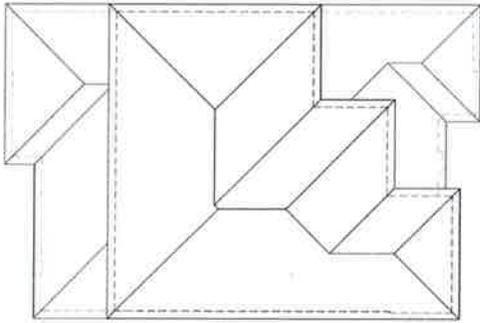
09.21.18

PLAN 3 | ELEVATION 'B' | MODERN FARMHOUSE
RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA



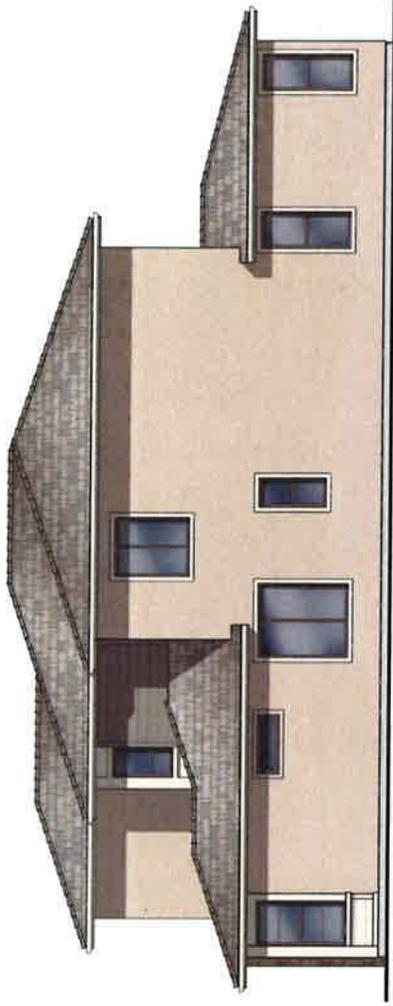
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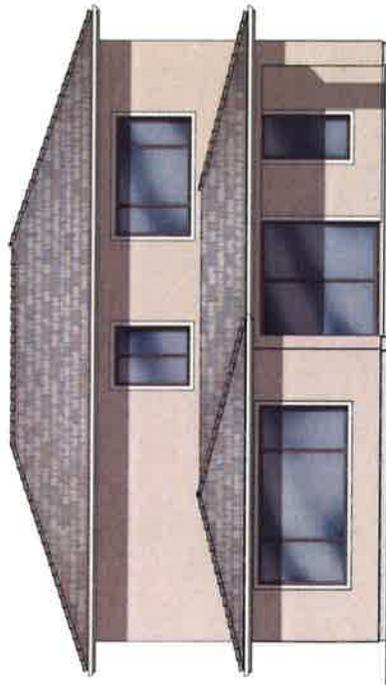
SCALE: 1/8" = 1'-0"

ROOF PLAN



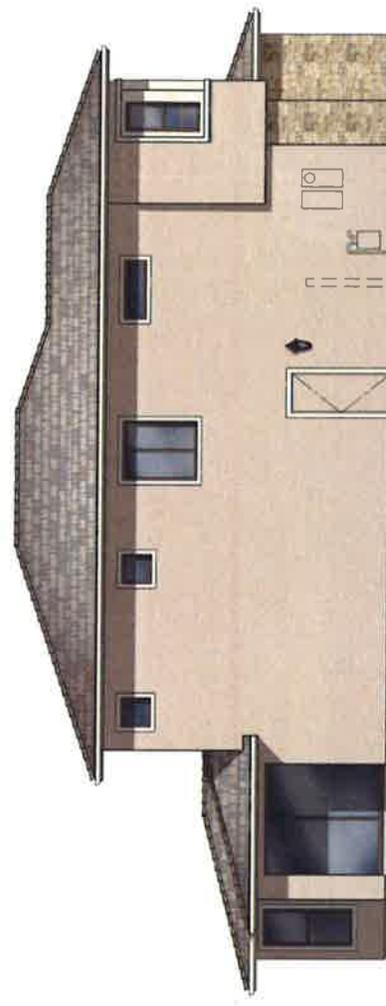
SCALE: 1/8" = 1'-0"

RIGHT ELEVATION



SCALE: 1/8" = 1'-0"

REAR ELEVATION



SCALE: 1/8" = 1'-0"

LEFT ELEVATION

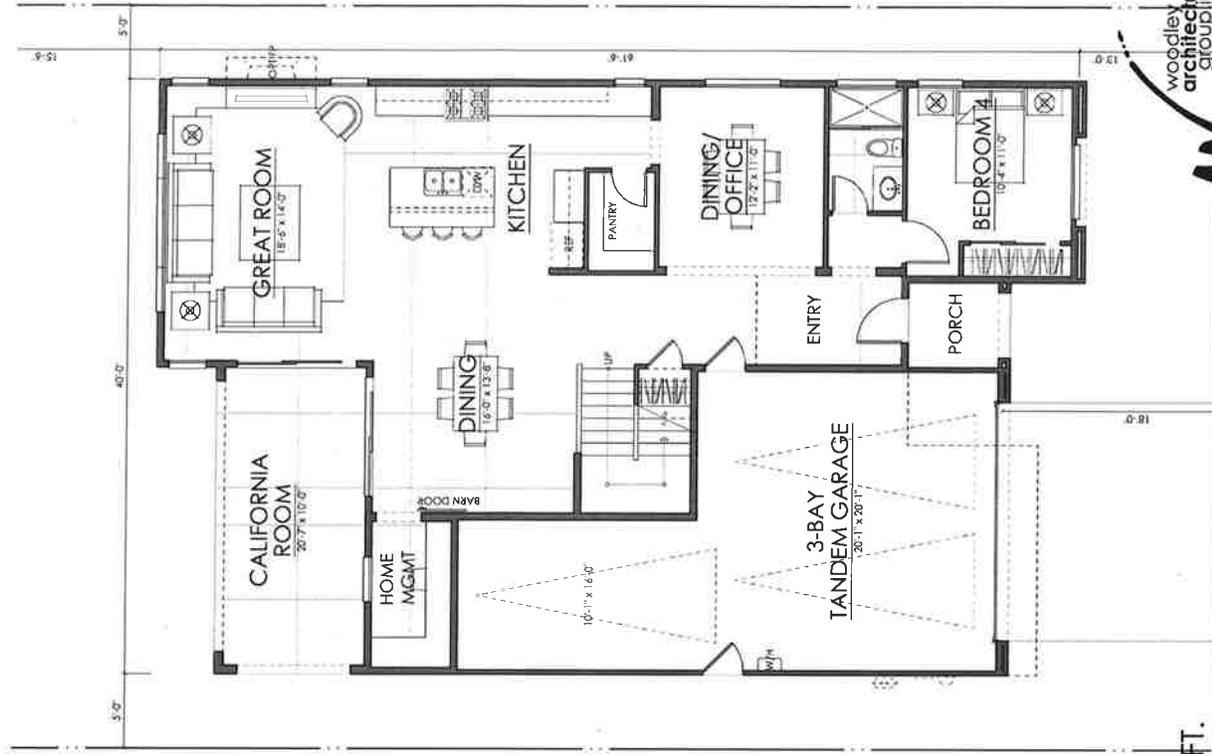
09.21.18

PLAN 3 | ELEVATION 'C' | CALIFORNIA PRAIRIE
RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA

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 california // 2843 palmdale bl. suite a
 san lita, ca 92732 / 949 533 8919



LOST SIZE 50'-0\"/>

FIRST FLOOR	1422 SQ. FT.
SECOND FLOOR	1308 SQ. FT.
TOTAL LIVING	2720 SQ. FT.
TARGET	2700 SQ. FT.
LOT COVERAGE	2040 SQ. FT.
MAX COVERAGE	2250 SQ. FT.

PLAN THREE | 2720 SQ. FT.

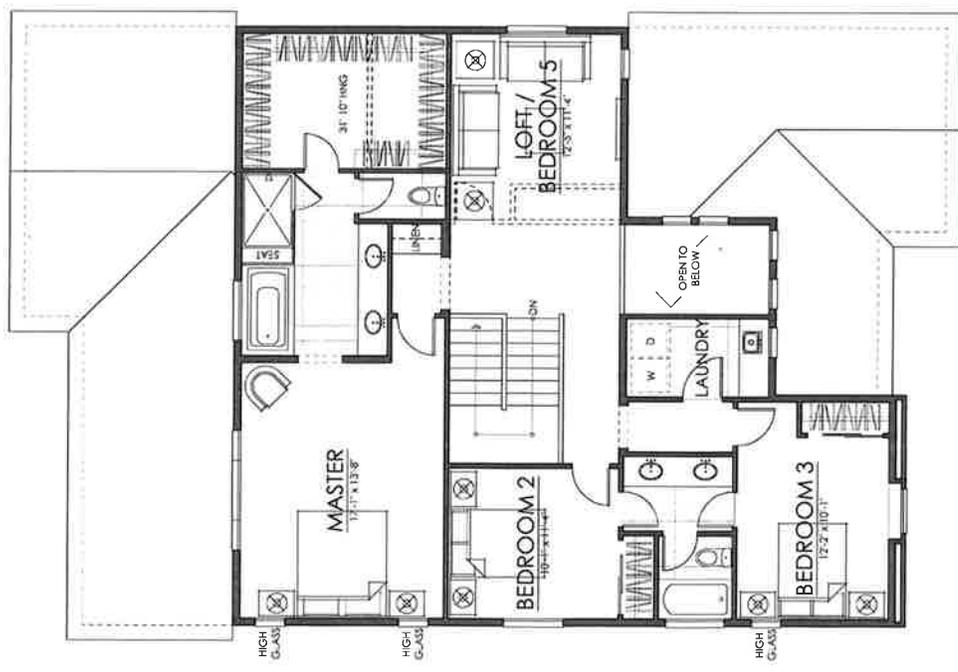
RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY

FOLSOM, CALIFORNIA

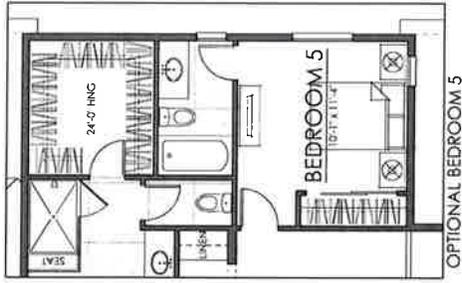
NOTE: SQUARE FOOTAGE MAY VARY BASED ON CALCULATION METHODS. CONTACT HOUSING ARCHITECTURE FOR FURTHER INFORMATION.



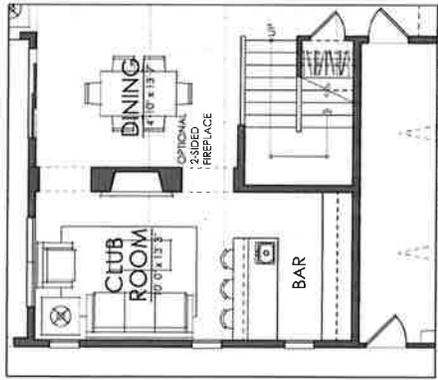
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 littleton, co 80120 // 303.683.7231
 california // 2943 pullman st. suite A
 santa ana, ca 92705 // 949.338.8919



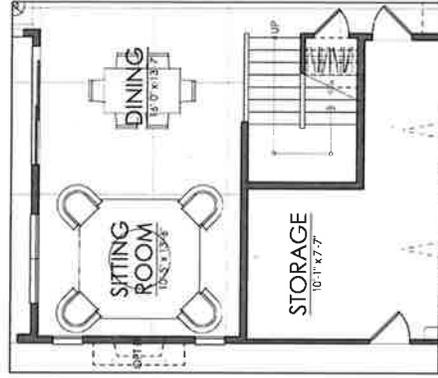
09.21.18



OPTIONAL BEDROOM 5



OPTIONAL CLUB ROOM +1.66 SQ. FT.



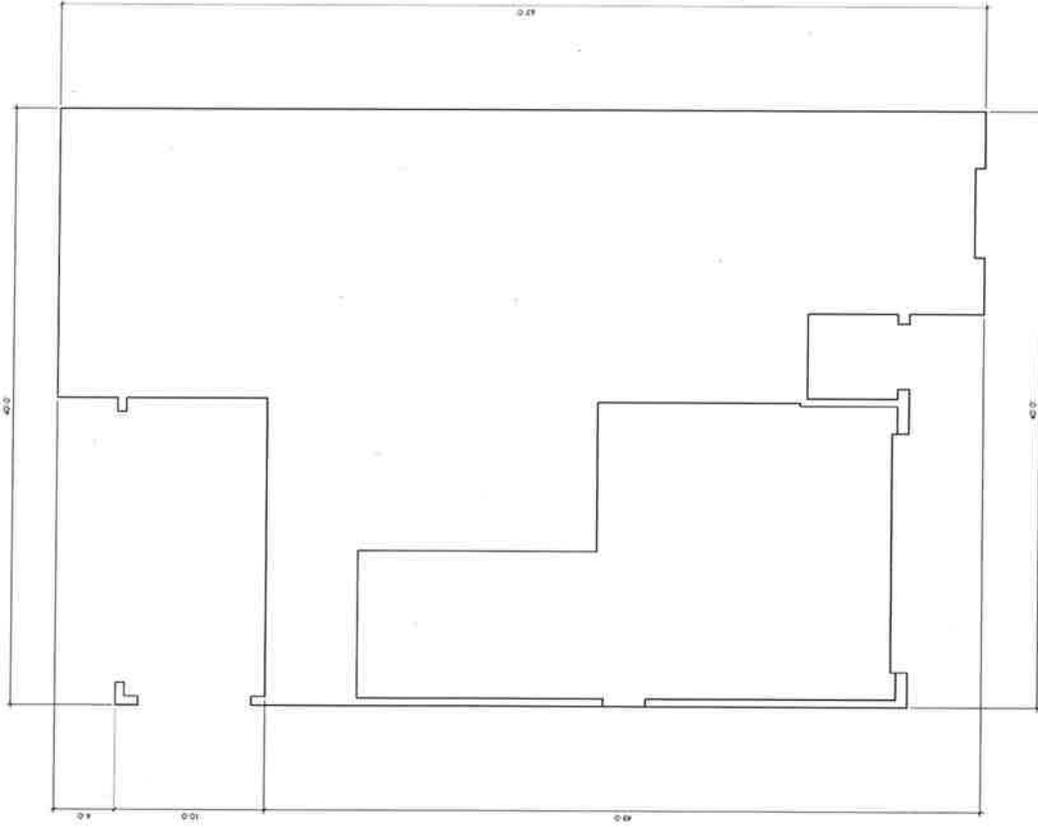
OPTIONAL SITTING ROOM +85 SQ. FT.

09.21.18

PLAN THREE | 2720 SQ. FT.

RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA

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SLAB EDGE PLAN 'A'

1/4" = 1'-0"

SPANSECTECTIC

PLAN THREE

RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA

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 Alhambra, CA 91803
 California / 714 293 0000 ext. 101
 sanjo ana, ca 92705 / 949 553 8919



- MATERIAL NOTES**
1. ST. CONCRETE ROOF
 2. ST. STUCCO
 3. CERAMIC TILE SIDING
 4. STONE SIDING
 5. ESCOBIADO SHUTTERS
 6. DECO CERAMIC TILE

SCALE 1/4" = 1'-0"

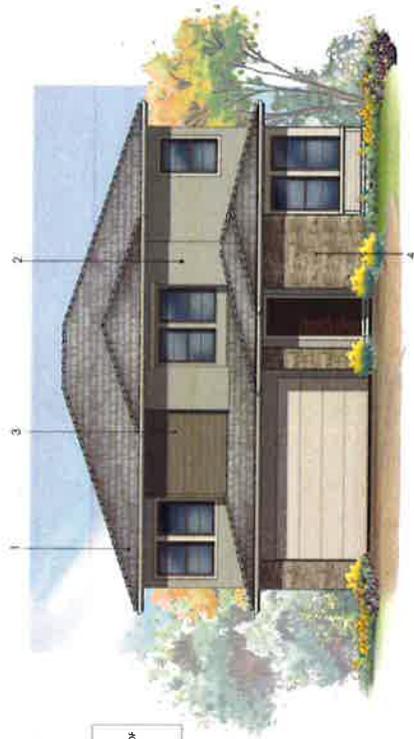
elevation A- SPANISH ECLECTIC



- MATERIAL NOTES**
1. CONCRETE FLAT ROOF
 2. STUCCO
 3. BOARD & BATT
 4. METAL ROOF
 5. STUCCO
 6. SHUTTERS
 7. DECORATIVE BRACKET

SCALE 1/4" = 1'-0"

elevation B- MODERN FARMHOUSE



- MATERIAL NOTES**
1. CONCRETE FLAT ROOF
 2. STUCCO
 3. HORIZONTAL SIDING
 4. STONE TOWER

SCALE 1/4" = 1'-0"

elevation C- CALIFORNIA PRAIRIE

09.21.18

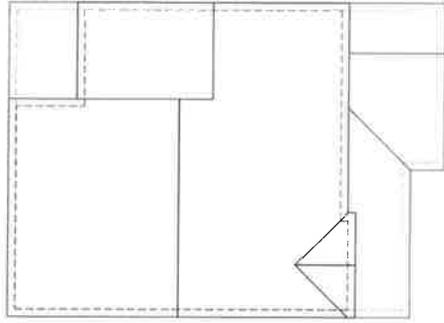


PLAN 4 | FRONT ELEVATIONS

RUSSELL RANCH | 50 X 90 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA

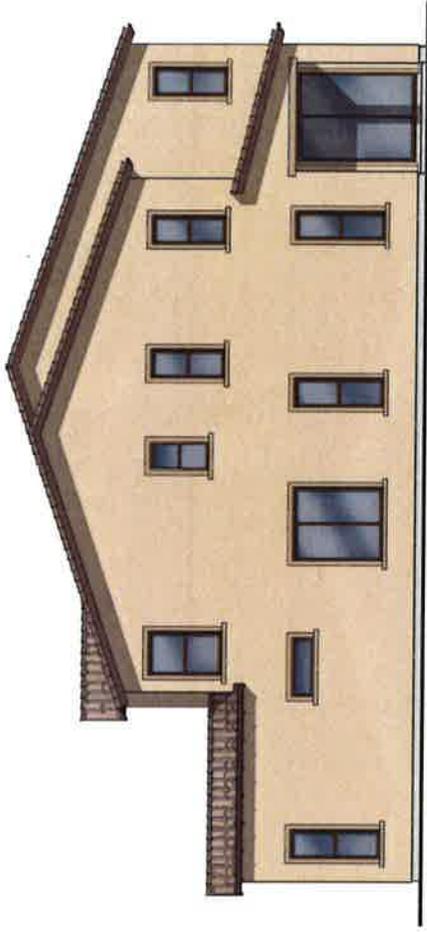
woodley architecture GROUP, INC.

17115 S. 176th St., Suite 3
 Tukwila, WA 98148
 California: 916.214.2000 | 916.214.2211
 woodleyarch.com



SCALE: 1/8" = 1'-0"

ROOF PLAN



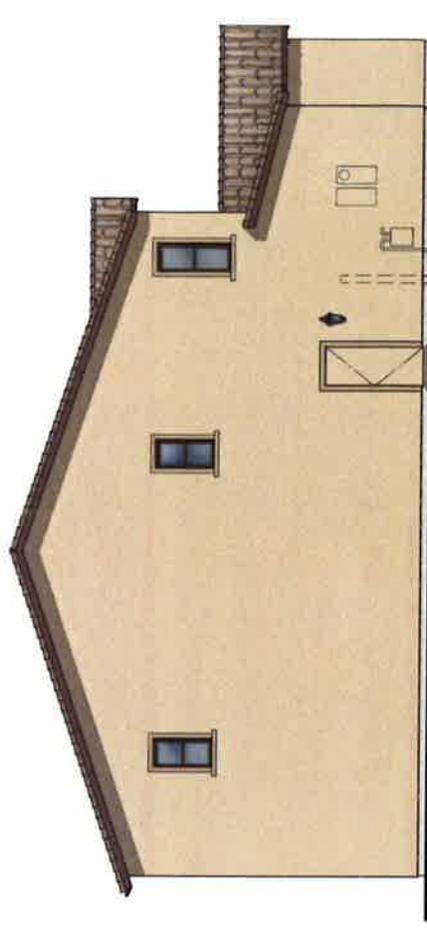
SCALE: 1/4" = 1'-0"

RIGHT ELEVATION



SCALE: 1/4" = 1'-0"

REAR ELEVATION



SCALE: 1/4" = 1'-0"

LEFT ELEVATION

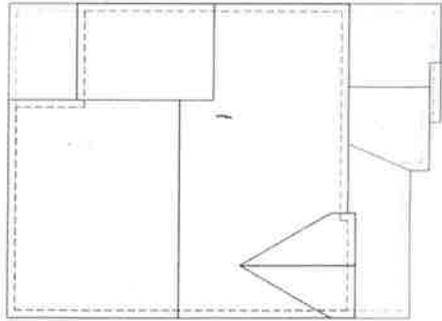
09.21.18

PLAN 4 | ELEVATION 'A' | SPANISH ECLECTIC
RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA



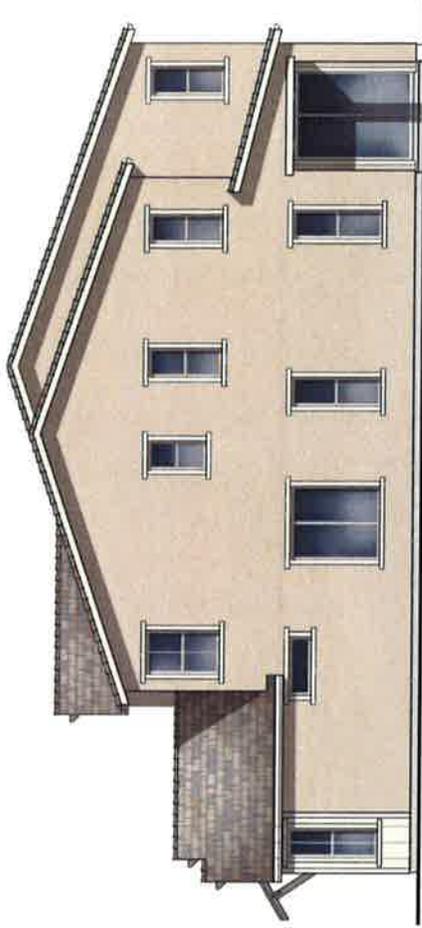
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 architectural
 group, inc.
 colorado // 731 southport dr. suite 3
 denver, colorado 80202
 california // 2940 calaverita rd. #100
 san joe, ca 95128 / 916.555.8919

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ROOF PLAN

SCALE: 1/8" = 1'-0"



RIGHT ELEVATION

SCALE: 1/4" = 1'-0"



REAR ELEVATION

SCALE: 1/4" = 1'-0"



LEFT ELEVATION

SCALE: 1/4" = 1'-0"

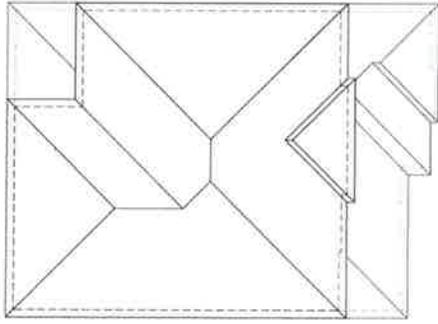
09.21.18

PLAN 4 | ELEVATION 'B' | MODERN FARMHOUSE
RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA



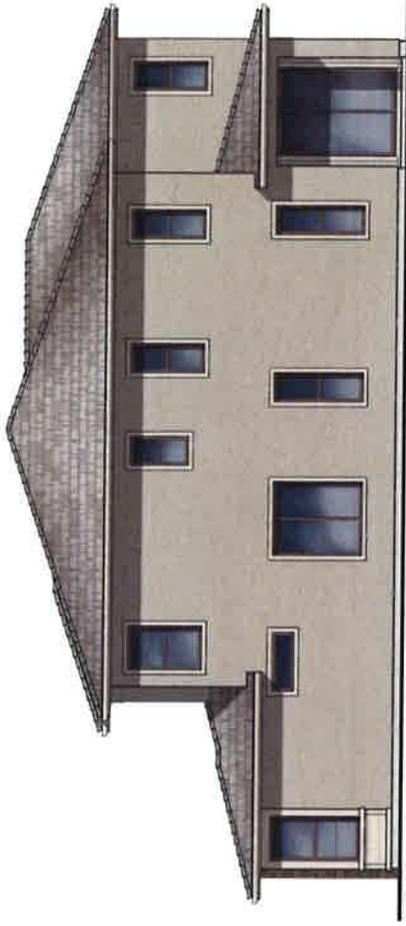
woodley architectural AS5 Group, Inc.
 15011 Pointe Vista Drive
 Suite A
 Folsom, CA 95762
 Phone: 916.977.5577

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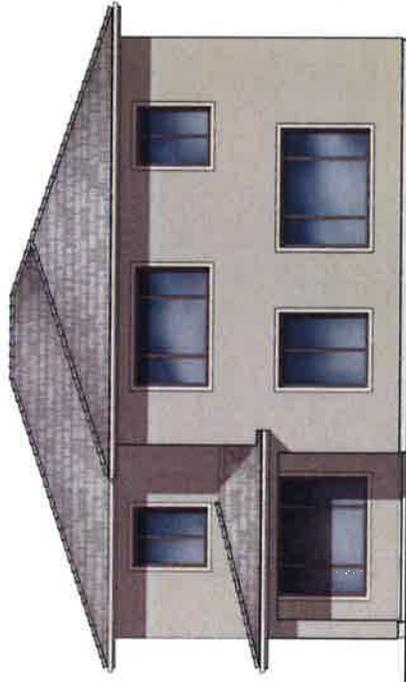
ROOF PLAN

SCALE: 1/8" = 1'-0"



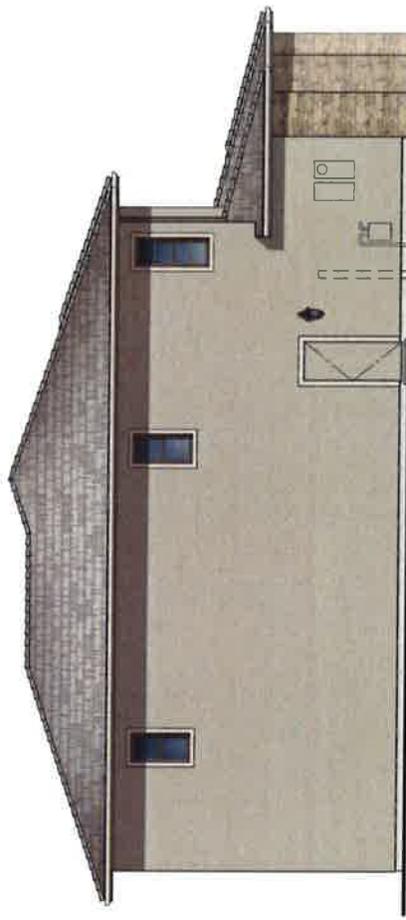
RIGHT ELEVATION

SCALE: 1/4" = 1'-0"



REAR ELEVATION

SCALE: 1/8" = 1'-0"



LEFT ELEVATION

SCALE: 1/4" = 1'-0"

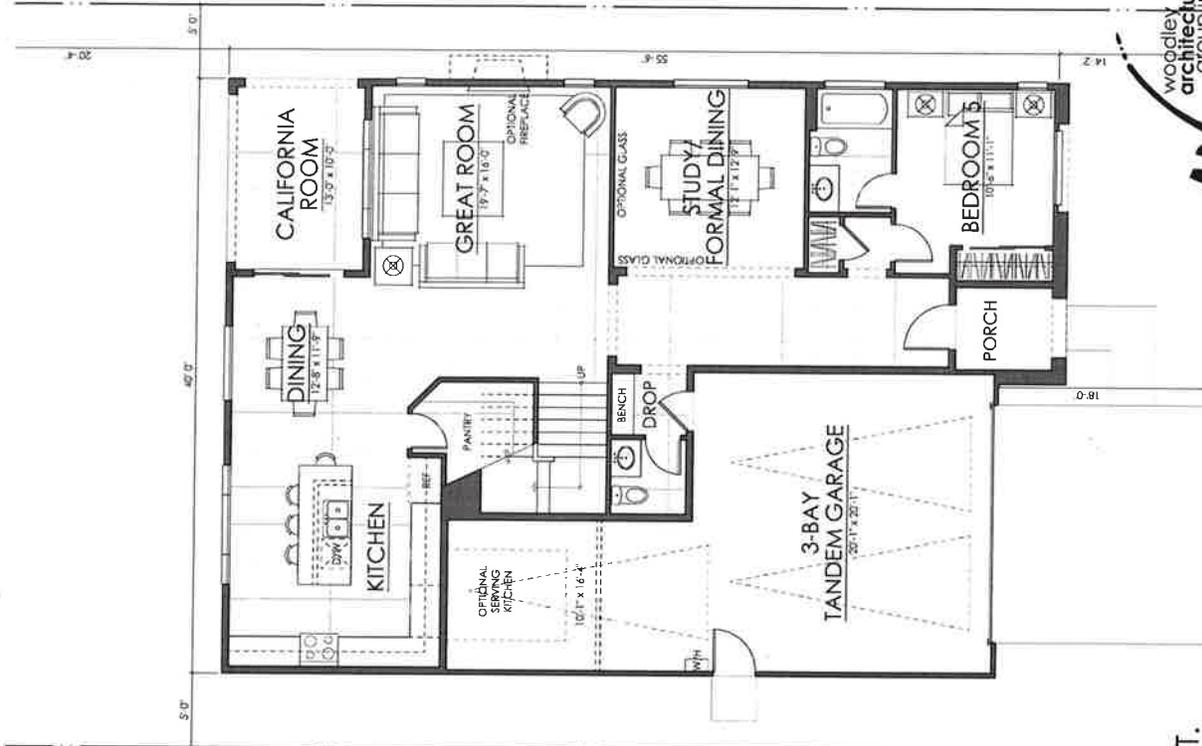
09.21.18

PLAN 4 | ELEVATION 'C' | CALIFORNIA PRAIRIE
RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA



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LOST SIZE 50'-0" X 90'-0"

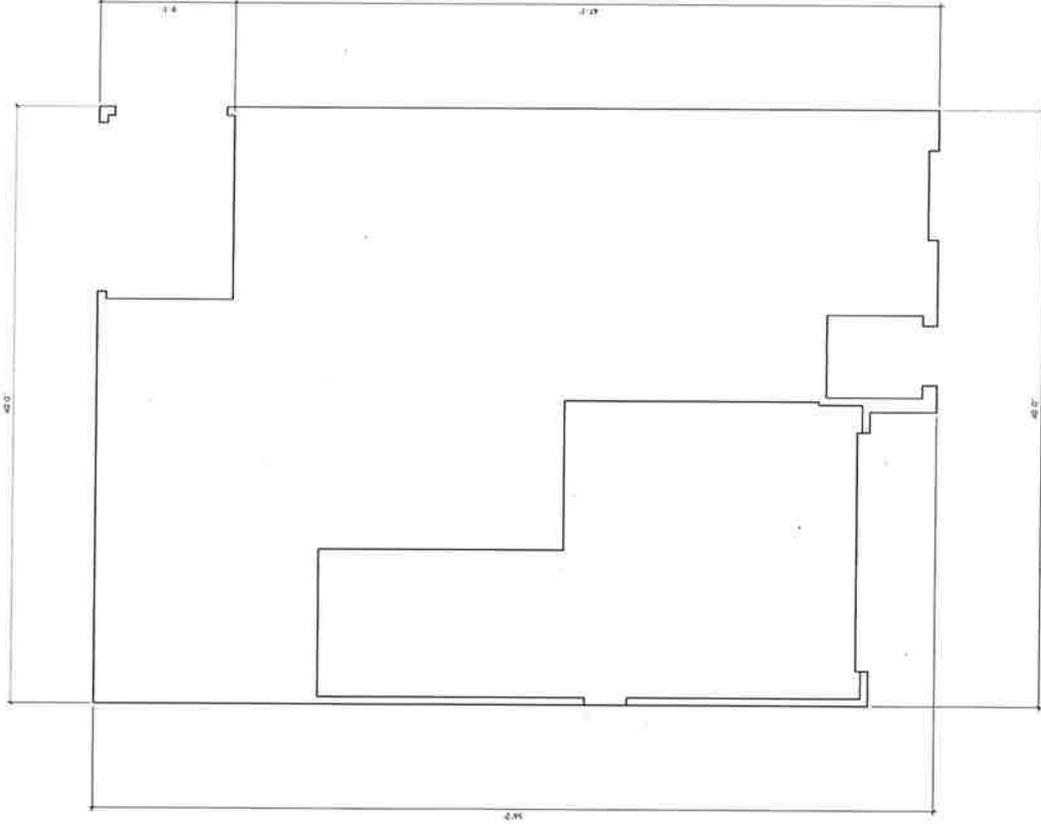
FIRST FLOOR 1403 SQ. FT.
 SECOND FLOOR 1550 SQ. FT.
TOTAL LIVING 2953 SQ. FT.
 TARGET 2900 SQ. FT.

LOT COVERAGE 2017 SQ. FT.
 MAX COVERAGE 2250 SQ. FT.

PLAN FOUR | 2953 SQ. FT.

**RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA**





SLAB EDGE PLAN 'A'
1/8" = 1'-0"

SPANS/ELECTRIC

09.21.18

PLAN FOUR
RUSSELL RANCH | 50x90 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA

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Spanish Eclectic (A) Elevations										
Color Scheme	Omega Integral Stucco Color 1/620 Sand Finish	Paint Match to Stucco Color Flat Finish	Sherwin Williams Paint Company		Accent Color Eggshell Finish	Andersen Windows 100 Series	Eagle S-Tile Profile Concrete Roofing	Eagle S-Tile Profile Concrete Roofing		
			Raised Foam Color Flat Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco				Andersen Windows 100 Series	Andersen Windows 100 Series	
SP1	TBD	SW 7008 Alabaster	SW 6149 Relaxed khaki	SW 6083 Sable	SW 6209 Ripe Olive	Dark Bronze	SCC 8830 Albuquerque Blend	3723		
SP2	TBD	SW 7541 Greecian Ivory	SW 7052 Gray Area	SW 9183 Dark Clove	SW 6213 Halcyon Green	Dark Bronze	Adobe Blend	3815		
SP3	TBD	SW 6148 Wool Skein	SW 6150 Universal Khaki	SW 7510 Chateau Brown	SW 7625 Mount Etna	Dark Bronze	Red Bluff Blend			
Modern Farmhouse (B) Elevations										
Color Scheme	Omega Integral Stucco Color 1/620 Sand Finish	Paint Match to Stucco Color Flat Finish	Sherwin Williams Paint Company		Accent Color Eggshell Finish	Eldorado Brick See notes section for mortar specifications	Andersen Windows 100 Series	Eagle Shake Profile Concrete Roofing	Eagle Shake Profile Concrete Roofing	
			Sliding Color Eggshell Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco					Eldorado Brick See notes section for mortar specifications	Andersen Windows 100 Series
FA1	TBD	SW 7071 Gray Screen	SW 7010 White Duck	SW 7010 White Duck	SW 2839 Roycroft Copper Red	Lafigo TundraBrick	White	SCP 8803 Arlington Blend		
FA2	TBD	SW 6141 Softer Tan	SW 7012 Creamy	SW 7012 Creamy	SW 9179 Anchors Aweigh	Lafigo TundraBrick	White	5810 Avondale Blend		
FA3	TBD	SW 9165 Gossamer Veil	SW 7006 Extra White	SW 7006 Extra White	SW 7059 Unusual Gray	Lafigo TundraBrick	White	SCP 8802 Nantucket Blend		
California Praline (C) Elevations										
Color Scheme	Omega Integral Stucco Color 1/620 Sand Finish	Paint Match to Stucco Color Flat Finish	Sherwin Williams Paint Company		Accent Color Eggshell Finish	Creative Mines Stone Dystacked	Andersen Windows 100 Series	Eagle Shake Profile Concrete Roofing	Eagle Shake Profile Concrete Roofing	
			Sliding Color Eggshell Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco					Creative Mines Stone Dystacked	Andersen Windows 100 Series
PR1	TBD	SW 7649 Silverplate	SW 6199 Rare Gray	SW 7012 Creamy	SW 7067 Cityscape	Greypearl Craft Split Modular	Terratone	4697 Slate Range		
PR2	TBD	SW 6071 Popular Gray	SW 7018 Dovetail	SW 7005 Pure White	SW 0042 Ruskin Room Green	Timberwolf Craft Split Modular	Terratone	SCB 8802 Nantucket Blend		
PR3	TBD	SW 6198 Sensible Hue	SW 7046 Anonymous	SW 9166 Drift of Mist	SW 2838 Polished Mahogany	Greenflea Craft Split Modular	Terratone	SCB 8805 Seattle Blend		

General Notes:

- All paint is to be Sherwin Williams Paint Company.
- All metal elements are to be painted Sherwin Williams SW 7020 "Black Fox", Eggshell Finish
- All non-decorative items such as meter doors, non-decorative vents etc. are to be painted the same color and finish as the surrounding field color.
- All non-decorative roof metal is to be painted to match the darkest color from the roofing blend.
- Metal Roofing is to be Custom-Bit Metals. Color: "Pre-Weathered Galvalume"
- Rain gutters and downspouts are TBD

Masonry Notes at B Elevation:

- Mortar is to be Orco Blended Products MAC Mortar. Color: "OBP Soft White"
- Mortar is to be flush with the face of the masonry with the faces remaining clean.

Attachment 6

Color and Materials Palette, dated March 16, 2018

Spanish Eclectic (A) Elevations						
Sherwin Williams Paint Company						
Color Scheme	Omega Integral Stucco Color 1 1/2" Sand Finish	Paint Match to Stucco Color Flat Finish	Raised Foam Color Flat Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Eagle S-Tile Profile Concrete Roofing
SP1	TBD	SW 7008 Alabaster	SW 6149 Relaxed khaki	SW 6083 Sable	SW 6209 Rise Olive	SCC 8830 Albuquerque Blend
SP2	TBD	SW 7541 Grecian Ivory	SW 7052 Gray Area	SW 9183 Dark Clove	SW 6213 Halcyon Green	3723 Adobe Blend
SP3	TBD	SW 6148 Wool Skein	SW 6150 Universal Khaki	SW 7510 Chateau Brown	SW 7625 Mount Elma	3815 Red Bluff Blend

Modern Farmhouse (B) Elevations						
Sherwin Williams Paint Company						
Color Scheme	Omega Integral Stucco Color 1 1/2" Sand Finish	Paint Match to Stucco Color Flat Finish	Siding Color Eggshell Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Eagle Shake Profile Concrete Roofing
FA1	TBD	SW 7071 Gray Screen	SW 7010 White Duck	SW 7010 White Duck	SW 2839 Roycroft Copper Red	White 100 Series
FA2	TBD	SW 6141 Softer Tan	SW 7012 Creamy	SW 7012 Creamy	SW 9179 Ancho's Aweigh	White 100 Series
FA3	TBD	SW 9165 Gossamer Veil	SW 7006 Extra White	SW 7006 Extra White	SW 7059 Unusual Gray	White 100 Series

California Prairie (C) Elevations						
Sherwin Williams Paint Company						
Color Scheme	Omega Integral Stucco Color 1 1/2" Sand Finish	Paint Match to Stucco Color Flat Finish	Siding Color Eggshell Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Eagle Shake Profile Concrete Roofing
PR1	TBD	SW 7649 Silverplate	SW 6199 Rare Gray	SW 7012 Creamy	SW 7067 Cityscape	White 100 Series
PR2	TBD	SW 6071 Popular Gray	SW 7018 Dovetail	SW 7005 Pure White	SW 0042 Ruskin Room Green	White 100 Series
PR3	TBD	SW 6198 Sensible Hue	SW 7046 Anonymous	SW 9166 Drift of Mist	SW 2838 Polished Mahogany	White 100 Series

General Notes:

- All paint is to be Sherwin Williams Paint Company.
- All metal elements are to be painted Sherwin Williams SW 7020 "Black Fox", Eggshell Finish
- All non-decorative items such as meter doors, non-decorative vents etc. are to be painted the same color and finish as the surrounding field color.
- All non-decorative roof metal is to be painted to match the darkest color from the roofing blend.
- Metal Roofing is to be Custom-Blit Metals. Color: "Pre-Weathered Galvalume"
- Rain gutters and downspouts are TBD

Masonry Notes at B Elevation:

- Mortar is to be Orco Blended Products MAC Mortar. Color: "OGP Soft White"
- Mortar is to be flush with the face of the masonry with the faces remaining clean.

Attachment 7

Russell Ranch Design Guidelines



RUSSELL RANCH
AND BROADSTONE ESTATES AT RUSSELL RANCH
PLANNED DEVELOPMENT DESIGN GUIDELINES

CHAPTER 4: ARCHITECTURE

“Home is a name, a word, it’s a strong one; stronger than magician ever spoke, or spirit ever answered to, in the strongest conjuration.”

-Charles Dickens

4.1 INTRODUCTION

By employing an authentic architectural palette and creative site planning techniques, Russell Ranch will be a neighborhood with a strong architectural identity and distinctive character within the City of Folsom.

Chapter 4 defines the design principles and development standards that apply to all residential development within Russell Ranch. These guidelines and standards articulate the lot characteristics, setbacks, garage type and orientation, and building massing.

Further, Chapter 4 includes a detailed architectural design guidelines section, which identifies, defines, and articulates the architectural styles appropriate for Russell Ranch.

4.2 DESIGN PRINCIPLES

4.2.1 Diversity of Streetscape

An elegant and diverse streetscape is a defining characteristic of enduring landmark neighborhoods. Simple and elegant planning and design elements can change the essence of a community while maintaining an overall unified theme. The intent of this section is to articulate the standards and unique defining elements by which Russell Ranch shall be built in order to create a cohesive and animated streetscape with a diverse character.

A. MASTER HOME PLAN REQUIREMENTS

To achieve streetscape variation, a master home plan series must comprise master home plans with a minimum of three (3) associated elevations per plan (each elevation must be a different architectural style) per each collection of home offerings, based upon the number of lots to be built upon by one builder as an individual project within the neighborhood. Further, a certain number of architectural styles is required, which will be applied based on appropriate massing (i.e., every floor plan will not have every architectural style applied. Certain home collections will benefit from a greater number of elevations per floor plan and architectural styles, whereas others may benefit from a more cohesive palette (such as the court homes, which provide an opportunity for a unified architectural statement based upon a singular style). This selective architectural style application will enhance the eclectic nature and variety of the streetscape. Master home plans are defined as unique floor plans with a distinct footprint with regard to placement and relationship of garage, front door, and building massing.

B. MASSING AND ROOF FORM

Proportion and placement of architectural forms and elements must be appropriately and authentically applied in a manner consistent with the historical architectural style being represented. Roof articulation in the form of proper roof pitches and forms also plays a significant role in the authenticity and diversity of the streetscape.

- o Massing must be appropriate and authentic to the architectural style (e.g., the Monterey style has a cantilevered second story balcony as a signature defining element; it would be inauthentic to design a single story Monterey home).
- o One out of every three homes must have a significantly different roof form than its neighbors (e.g., forward-facing gable versus side-facing gable).
- o Horizontal and vertical articulation is required on all homes, as appropriate to each architectural style, and can be achieved through differing roof forms, combinations of one and two story elements, architectural projections, porches, etc.
- o Front porches, when appropriate to the building style, must have a minimum depth of six (6) feet.

D. REPETITION

Avoiding repetition of identical floor plans or architectural styles is important to create a sense that a neighborhood has been built over time.

- o In the areas of the neighborhood with a traditional home-to-street relationship, the same floor plan with the same architectural style shall be no less than three (3) lots away in any direction (on the same side of the street as well as the opposite side of the street).

- o It is appropriate to exhibit a cohesive architectural theme within the court homes to create a village concept, as such, deviation to the repetition requirement is allowable within this enclave.

4.2.2 Four-Sided Architecture

The continuation of style-specific architectural elements from the front façade around to the side and rear elevations creates an authentic architectural statement. As defined in the Architectural Guidelines section found in Section 4.4, there is a minimum level of enhancement required on all homes based on architectural style.

Russell Ranch features single-loaded streets with highly visible front and rear elevations and side elevations that are less prominent. The approach should be a hierarchy of treatment based on location. Blank, unadorned building faces are never permitted; a certain minimum amount of detail is required to reflect a unified architectural treatment. The front elevation should be the most highly detailed; the rear elevation should exhibit a specific number of style-specific architectural elements; typical side elevations may exhibit fewer style-specific architectural elements, while corner lots will feature a consistent level of detail on both the side and rear elevations.

The following section identifies enhanced lot situations as well as the four-sided elements that are required on these lots.

Figure 4.1 identifies home sites that are visible from multiple angles, public ways, open space, community edges, and major arterials. Home sites identified as enhanced lots are subject to the requirements in the following section.

A. ENHANCED LOTS

Home sites that are highly visible warrant special attention to any visible building faces to present an authentic and cohesive appearance. The following standards apply to highly visible lots within the neighborhood in addition to the standard requirements and enhancements for all homes described in section 4.4.

- o All corner lots (those with two adjacent streets), identified in blue on Figure 4.1, must employ at least three enhancements from the enhanced elements portion of the corresponding architectural style matrix (found in Section 4.4) on all street adjacent building faces (in addition to the minimum enhancements required for all homes).
- o All other highly visible home sites (such as those adjacent to open space corridors), identified in red on Figure 4.1, must employ at least two enhancements from the enhanced elements portion of the corresponding architectural style matrix (found in Section 4.4) on all building faces adjacent to public ways, open space, community edges, and/or major arterials (in addition to the minimum enhancements required for all homes).



Figure 4.1: Enhanced Lots

This illustration is a conceptual plan intended to guide the determination of highly visible lots. Actual determination of enhanced lots will be based on final lotting of subdivisions.

4.2.3 Reciprocal Use Easements

Reciprocal use easements are an innovative way to increase the usable yard area for a small lot home (reciprocal use easements are permitted on The Courts Homes only). By allowing one home to utilize the side yard of an adjacent home, side yard space effectively doubles. When reciprocal use easements are used, the following factors apply:

- The resident of the home relinquishing its side yard has the right to access the adjacent home's side yard for home maintenance and painting.
- Reciprocal use easements are required to be detailed on individual plot plans as part of the project construction phasing. Traditional setbacks shall not apply to reciprocal use easement areas, for landscape related features.
- Landscape structures, such as fountains, pergolas, etc. are permitted within the use easement and must be 3' from the face of the adjacent structure, consistent with building and fire code.

4.2.4 Garages

Reducing garage dominance on the streetscape and bringing living space closer to the street creates streetscenes that are inviting and safe with an "eyes on the street" environment. Using design techniques that enhance a home's architectural style and relegating the garage to a less visible position promotes a more pedestrian-oriented neighborhood. The following section describes the permitted garage mitigation measures for Russell Ranch.

- Garages must be recessed a minimum of 5' from living space or porches when accessed from a traditional street configuration. Garages that are located along

alleys or motor courts shall not be required to meet the 5' requirement.

- Garages accommodating more than two cars are allowable only in a split or tandem configuration. Three car front-loaded garages are not permitted.

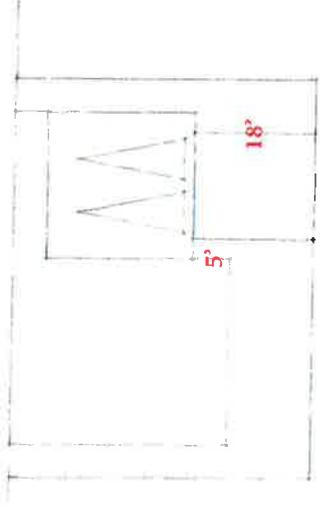


Figure 4.2: Garage Recess

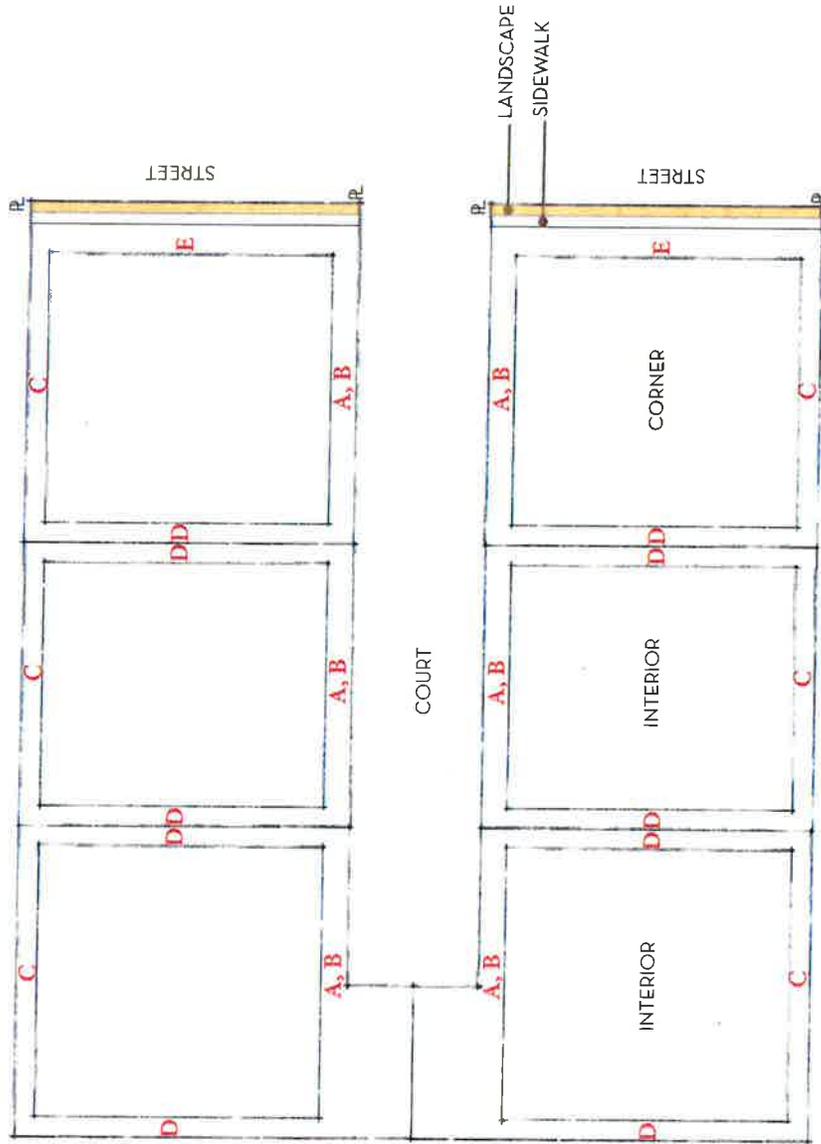


Figure 4.3: View Lots (Easement)

This illustration is a conceptual plan intended to guide the determination of highly visible lots. Actual determination of view lots will be based on final lotting of subdivisions.

4.3 DEVELOPMENT STANDARDS

4.3.1 The Court Homes

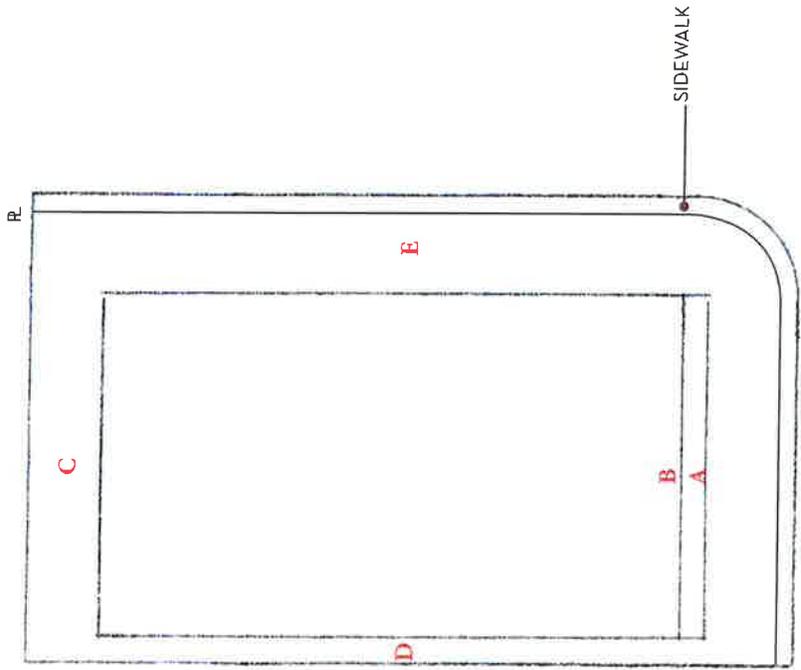


DESCRIPTION:	Court SFD	CATEGORY:	MLD
Minimum lot area:	2,150 Square Feet	A. Minimum front setback at court (to living or covered outdoor space);	4'
Minimum lot width:	43'	B. Minimum garage setback;	4'
Minimum lot depth:	50'	C. Minimum rear setback;	3'
Maximum lot coverage (single story / 2-story): ¹	60% / 60%	D. Minimum interior side setback;	3'
Maximum height (single story / 2-story):	28' / 35'	E. Minimum street side setback;	8'
Minimum parking requirement:	2 covered spaces/unit & 0.8 uncovered guest spaces/unit		

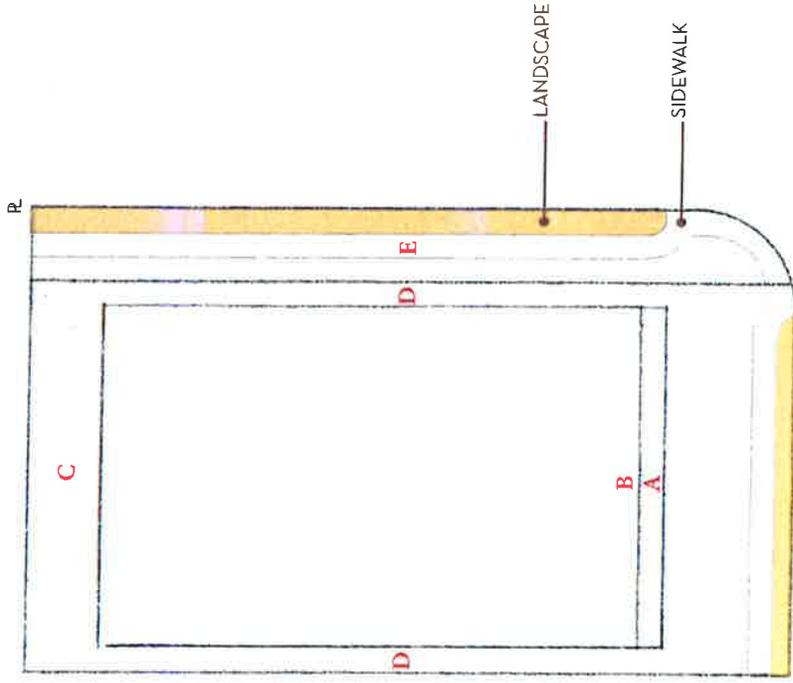
Note: Setbacks are measured from property line (P).

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4.3.2 Single Family High Density (SFHD)



Attached Sidewalk Condition



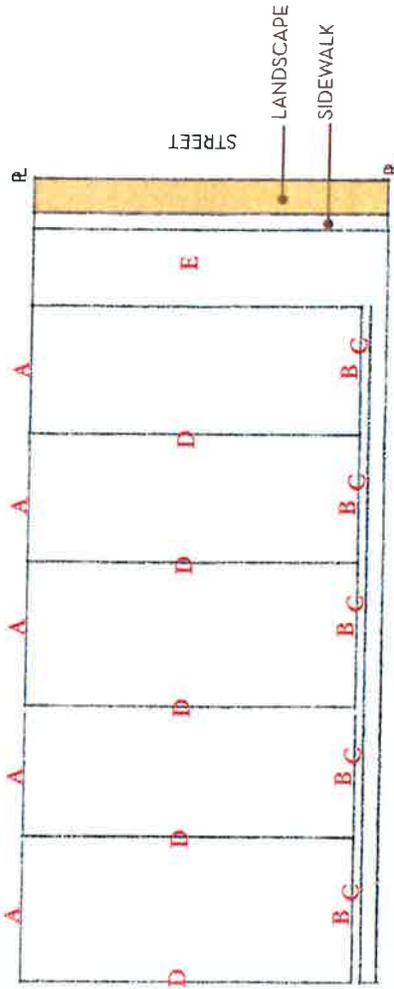
Detached Sidewalk Condition

DESCRIPTION:	SFD traditional lot	CATEGORY:	SFHD
Minimum lot area:	4,500 Square Feet	A. Minimum front setback to living space or covered outdoor space (attached sidewalk/detached sidewalk); ²	12.5'/24.5'
Minimum lot width:	50'	B. Minimum garage setback (attached sidewalk/detached sidewalk); ²	18'/30'
Minimum lot depth:	90'	C. Minimum rear setback; ^{2,3}	15'
Maximum lot coverage: ^{1,5}	50%	D. Minimum interior side setback; ²	5'
Maximum height (single story / 2-story):	28' / 35'	E. Minimum street side setback; ^{2,4}	10'

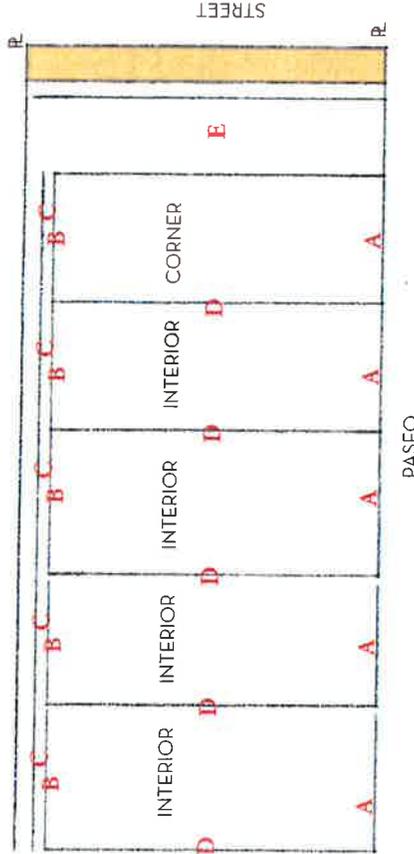
Note: Setbacks are measured from property line (R).

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4.3.3 Townhomes (MLD)



ALLEY



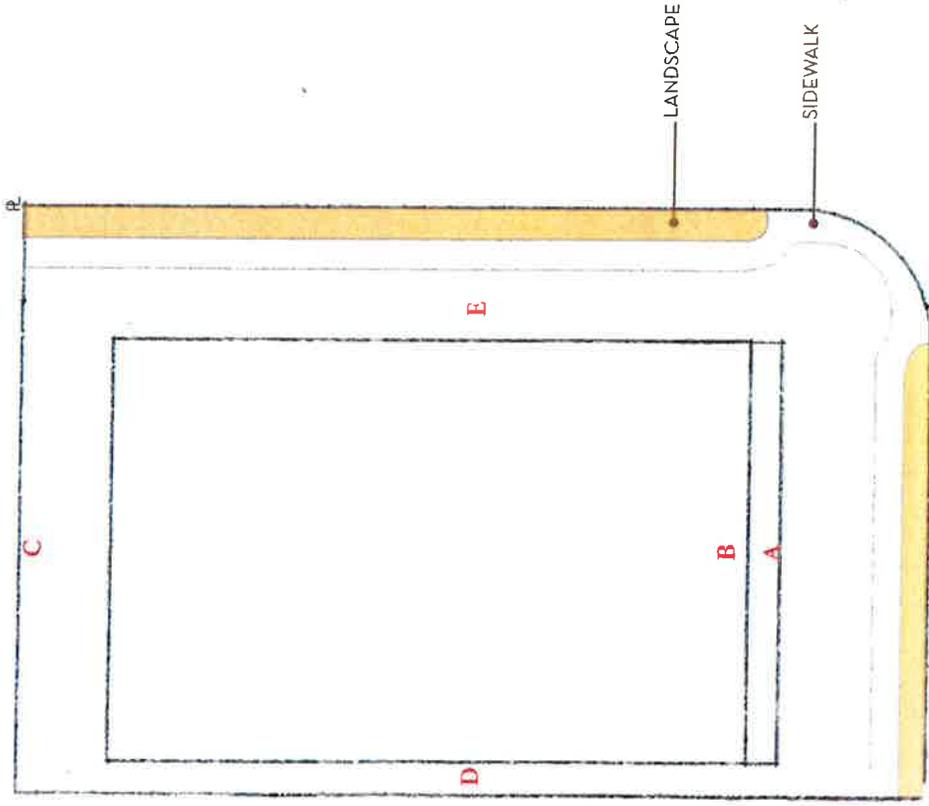
PASEO

DESCRIPTION:	MLD lot	CATEGORY:	SFHD
Minimum lot area:	1,188.8 Square Feet	A. Minimum front setback at landscape/paseo to living space or covered outdoor space: ²	0'
Minimum lot width:	21.42'	B. Minimum rear garage setback at alley: ²	5'
/ 3' Minimum lot depth:	55.5'	C. Minimum rear setback at alley (lower floor/upper floor): ^{2,3}	5'/3'
Maximum lot coverage: ^{1,6}	90%	D. Minimum interior side setback: ²	0'
Maximum height (single story / 2-story):	35'	E. Minimum street side setback: ^{2,4}	12.5'
Minimum parking requirement:	2 covered spaces / unit & 0.8 uncovered guest spaces / unit		

Note: Setbacks are measured from property line (R).

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4.3.4 Single Family (SF)



Detached Sidewalk Condition

DESCRIPTION:	SFD traditional lot	CATEGORY:	SF
Minimum lot area:	6,600 Square Feet	A. Minimum front setback to living space or covered outdoor space; ²	24.5'
Minimum lot width:	60'	B. Minimum garage setback; ²	30'
Minimum lot depth:	110'	C. Minimum rear setback; ^{2,3}	15'
Maximum lot coverage: ^{1,6}	50%	D. Minimum interior side setback; ²	5'
Maximum height (single story / 2-story):	28' / 35'	E. Minimum street side setback; ^{2,4}	22'

Note: Setbacks are measured from property line (R).

4.3.5 Development Standards Footnotes

1. MLD lot coverage shall be calculated as the percentage of lot area covered by the roof, including covered porches and patios. SFHD and SF lot coverage shall be calculated as the percentage of lot area covered by the roof, including covered porches but excluding outdoor covered unconditioned spaces (i.e., patios) and guest house/pool house/casita (i.e., detached habitable structure with only sleeping, living, and bathroom).
2. Accessory structures shall be consistent with the FPASP standards of 5' side yard (interior lot lines) and 5' rear yard setbacks; notwithstanding provision 3a below.
3. Minimum rear setback (view lots)
 - a. View lots, as identified in Figure 4.3, shall have a rear yard setback measured from the top of slope of the lot (as opposed to the downslope property line).
 - b. A View Lot Easement shall be recorded over the sloping portion of the lot (from the hinge point to the rear property line, see page 25, Rear Yard View Fencing section).
 - c. The easement area landscape shall conform to the slope area landscape treatments described in Chapter 3.
 - d. Monitoring and compliance with slope area requirements shall be the responsibility of Russell Ranch Homeowners Association. Any violations observed by the City of Folsom shall be referred to the Russell Ranch Homeowners Association.

4. Street side setback shall be 5' (measured from back of sidewalk) when adjacent to an open space lot 5' or greater in width.
5. A 5% lot coverage bonus for 3-sided outdoor covered unconditioned spaces (except with fireplace option, then 2-sided permitted) and/or guest house/pool house/casita (i.e., detached habitable structure with only sleeping, living and bathroom) shall be granted. In no case shall the total lot coverage exceed 55%.
6. A 10% lot coverage bonus for 3-sided outdoor covered unconditioned spaces (except with fireplace option, then 2-sided permitted) and/or guest house/pool house/casita (i.e., detached habitable structure with only sleeping, living and bathroom) shall be granted. In no case shall the total lot coverage exceed 60%.
7. For SF and SFHD lots, front yard setbacks shall be measured from property lines. Property lines shall be located as depicted in Figure 4.3. The Public Utility Easement (PUE) shall be located co-terminous with the front property line and extend into the lots as shown in yellow on Figure 4.3.

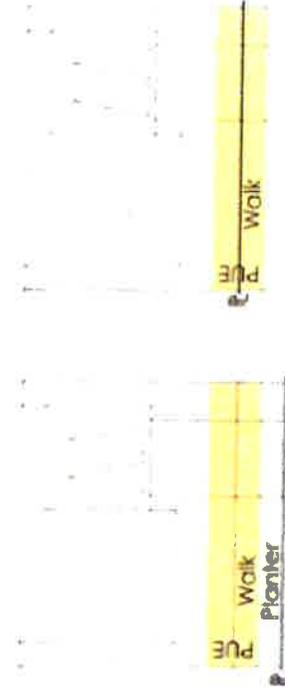


Figure 4.3. SF and SFHD PUE Location

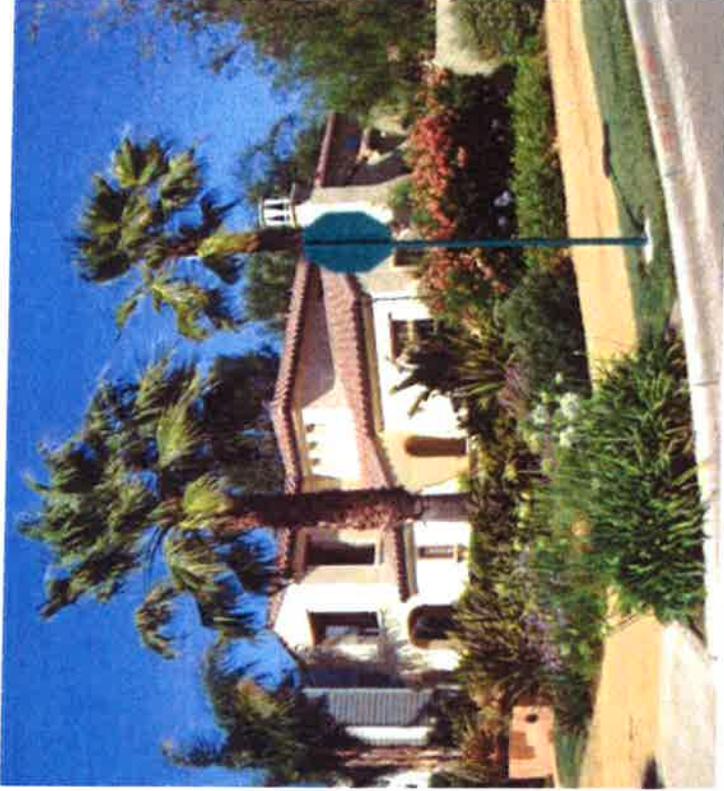
4.4 ARCHITECTURAL GUIDELINES

The concept, inspiration, and vision for Russell Ranch is one of a distinctively California village with a unique and compelling design character derived from successful landmark neighborhoods throughout California.

Russell Ranch presents a cohesive collection of nine architectural styles, which will create a diverse and eclectic streetscape through massing and form, material and color, and detailing. The variety of styles will animate and activate the streetscape, creating a dynamic and vibrant community palette. Additional architectural styles that are consistent with the neighborhood vision will be reviewed and approved by the RRDR on a case by case basis.

ARCHITECTURAL PALETTE

- o The California Wine Country
- o The Monterey
- o The Spanish Eclectic
- o The California Cottage
- o The Transitional Bungalow
- o The California Prairie
- o The Spanish Colonial Revival
- o The California Craftsman
- o The California Villa



Streetscape Example

HOW TO USE THESE GUIDELINES

The guidelines found in this section are formatted into individual style spreads, with each architectural style being defined and articulated over the course of three pages. The first page of the style spread defines the history and intent of the style; the second page provides a matrix that identifies the minimum elements for each style along with applicable enhanced elements and the third page identifies key style elements.

The core requirements for home design within the Russell Ranch neighborhood are concentrated in each style matrix (the second page of each spread), with the remaining pages intended as supporting and inspirational material. The sketches, key style elements, and details are intended to be an overview of the defining characteristics of the style and are not intended to be exhaustive, nor to represent all of the appropriate details inherent to each style.

With the intent of creating authentic representations of these architectural styles, all of the minimum elements outlined on the style specific matrix are required along with three enhanced elements.

To further define and emphasize the architecture of Russell Ranch, the following statements apply to all styles:

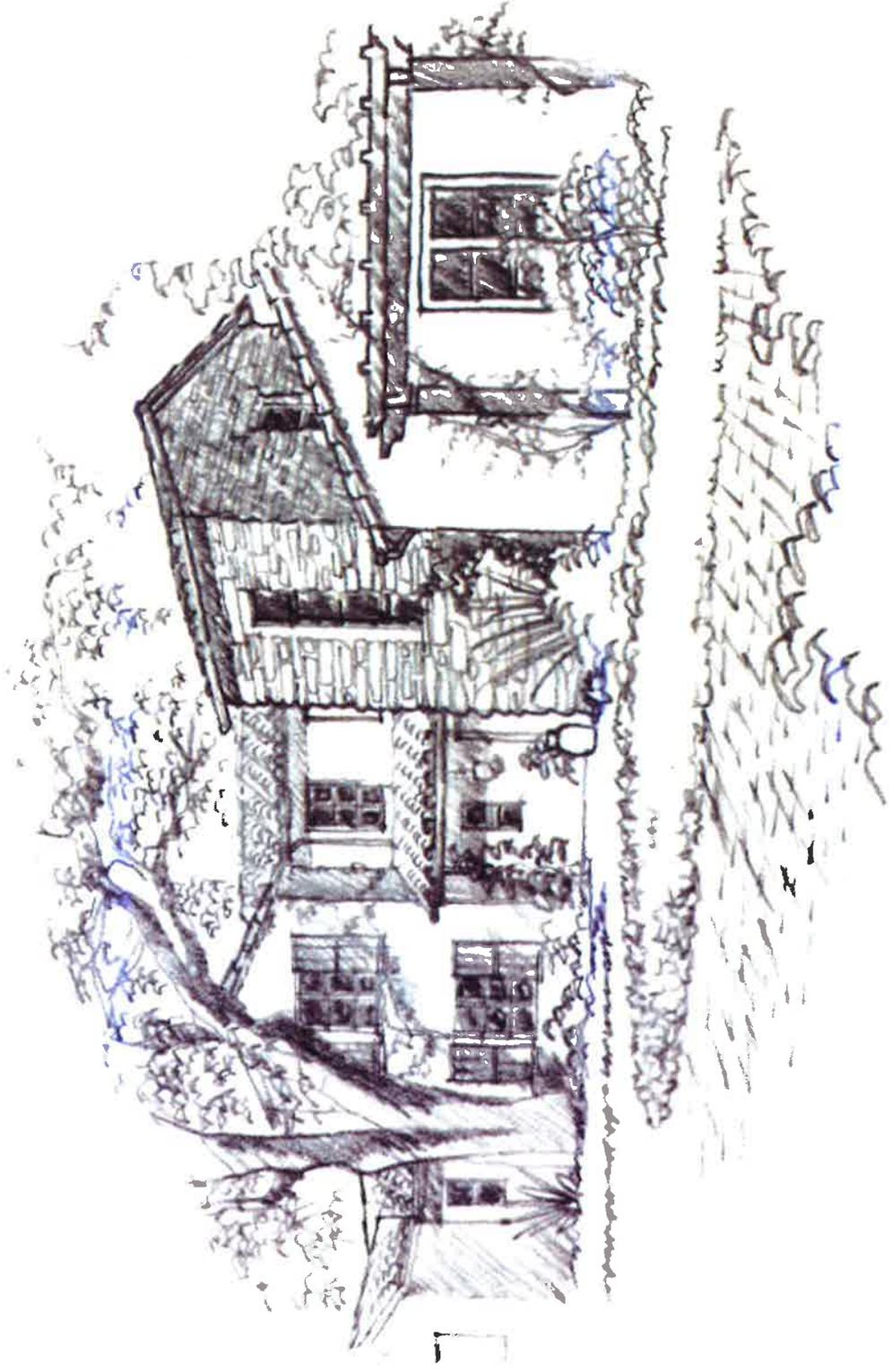
- Masonry must be applied authentically, wrapping outside corners and terminating at inside corners.
- Stone or brick scattered over stucco to mimic building age is not appropriate.
- Heavy knock down or "Spanish Lace" stucco is not permitted. Stucco finish options may include light lace, sand, smooth, imperfect smooth, cat face, or similar.

- All material changes must occur at an inside corner or other defined terminus (i.e., a fence line).
- No fascia gutter (gutter that serves as fascia) is permitted.
- No concrete rake tiles are permitted.
- Where wood is specified, cementitious material is acceptable to promote longevity and ease of maintenance.
- Grooved plywood siding and vinyl siding are not permitted.
- Garage doors shall complement the architectural style.
- House lights shall complement the architectural style.
- When shutters are used, each shutter shall be sized to one-half of the entire adjacent window width, such that if the shutters were closed, they would completely cover the window.

THE CALIFORNIA WINE COUNTRY

Reflective of the rolling terrain of the California Wine Country, this casual and rustic architectural style borrows European accents and reinterprets them to be authentically California. Asymmetrical forms are layered to create casual massing and the use of rustic accent materials, such as overgrouted stone or brick, wrought iron, and heavy beams add to the charm of this style.

The California Wine Country style will add a rustic element to the streetscape, offering a casual and lower profile complement to the more formal vertical styles.



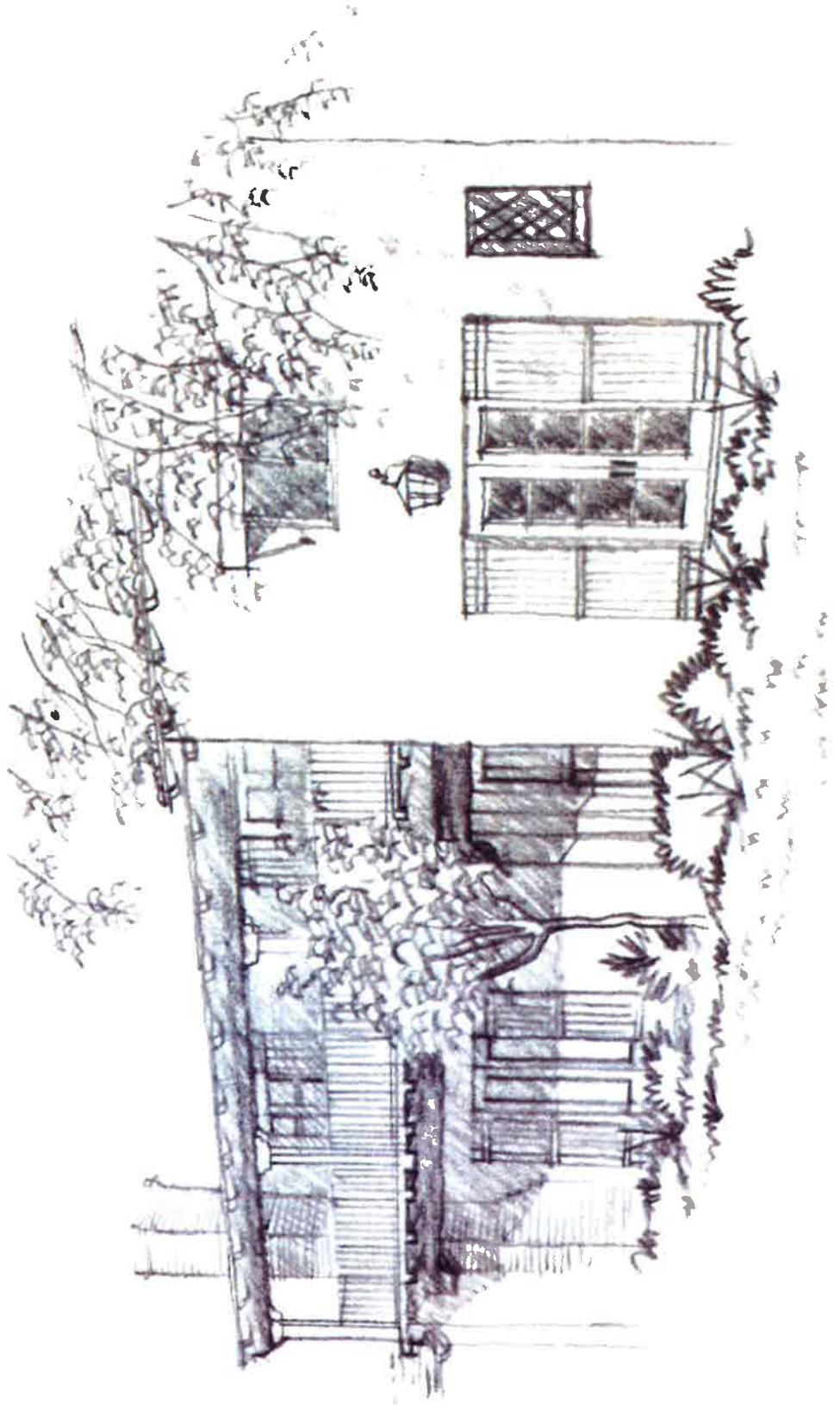
Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1,2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> Simple rectangular massing in one, one-and-a-half, full two-story, or a combination thereof. 	
Roof	<ul style="list-style-type: none"> Low-pitched gabled primary roofs (3:12 to 5:12). Shed porches. Tight rake at gables (0" to 6"). 12" to 16" eaves. Barrel or S-tile roof. 	<ul style="list-style-type: none"> Exposed rafter tails. Boosted roof tiles.
Walls	<ul style="list-style-type: none"> Stucco is the primary wall material. 	<ul style="list-style-type: none"> Overgrouted stone or brick as entire massing element. Smooth or imperfect smooth stucco finish.
Windows & Doors	<ul style="list-style-type: none"> Head and sill window trim or full window surrounds. Simple wood panel doors with vertical panels. Divided light windows. 	<ul style="list-style-type: none"> Front elevation with all windows inset 2" or one feature window recessed 12". Shutters (plank or diagonal brace). Colored window frames (e.g. cranberry, sage green, dark brown, etc.)
Details	<ul style="list-style-type: none"> Rustic wood railing and column posts. 	<ul style="list-style-type: none"> Massive chimney (may be battered or tapered) clad in stucco, stone, or brick. Wood trellises, applied sheds over windows, or Bermuda shutters. Carriage style garage doors with hardware. Typical downspouts replaced with "rain chains" or round metal downspouts.

1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.
2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE MONTEREY

The historic Monterey style blends the Spanish cultures of original California residents with the influences brought to the west by the first European settlers. These homes are a juxtaposition of local indigenous materials with colonial detailing applied. The dominant feature of the Monterey style is the always-present upper-story balcony element, which is contained within the principal roof form and cantilevered. The balcony is of heavy timbered construction, defining the structure.

The Monterey style is a direct link to the colonial heritage of California. Only appropriate for two-story homes, this style will enhance the animation of the streetscape, both vertically and horizontally, through height, recess, and significant shadow play.



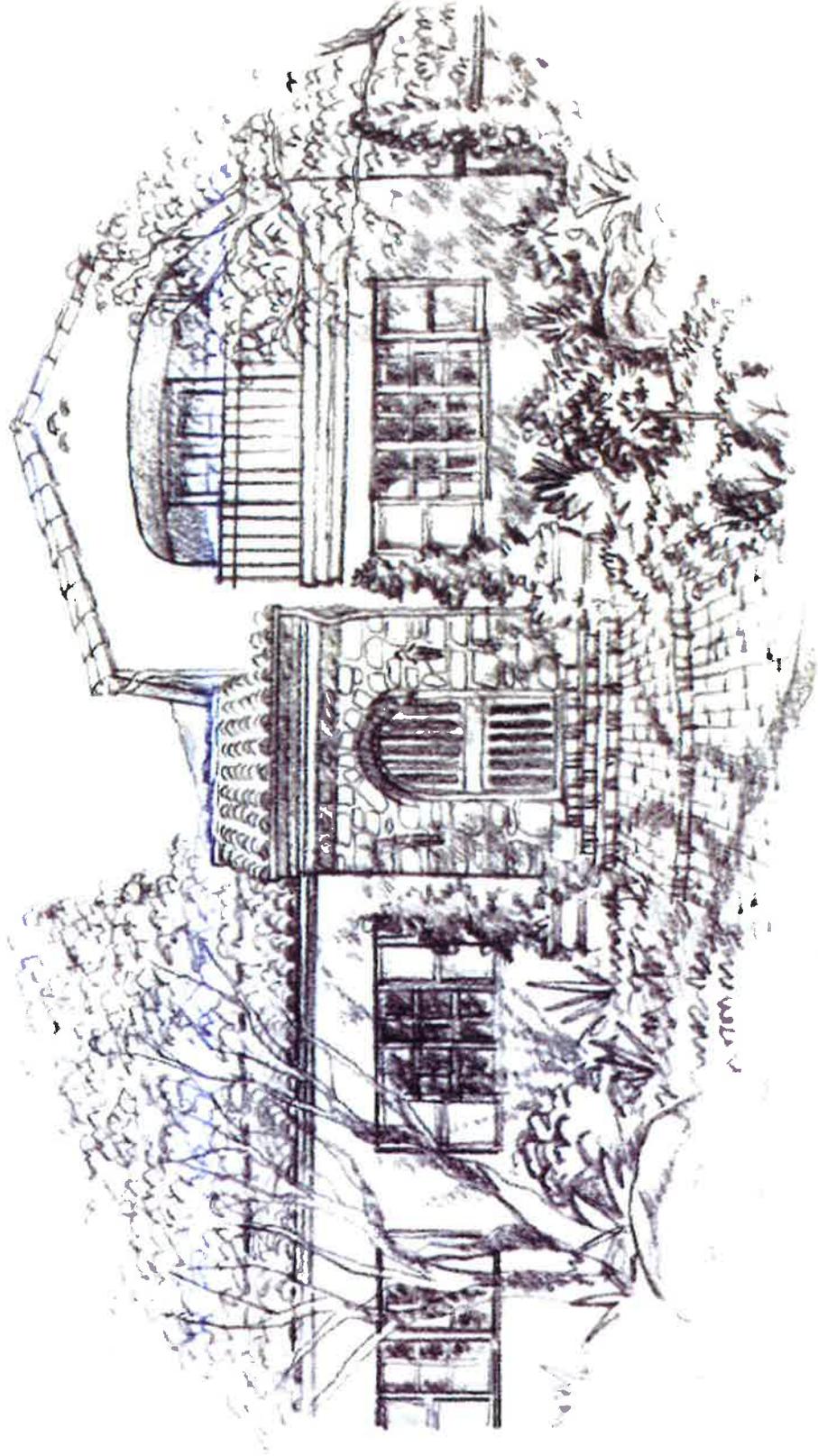
Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1,2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • Two-story, rectangular form. • Principal side gabled roof. • Cantilevered second story balcony covered by principal roof. 	<ul style="list-style-type: none"> • L-shaped form with front facing cross gable. • Hipped roof elements.
Roof	<ul style="list-style-type: none"> • Low-pitched gabled roofs (4:12 to 5:12). • Flat tile roof with barrel ridge and hip tiles. • 12" to 16" overhangs. • Thin eaves with either a half-round or agee gutter. 	<ul style="list-style-type: none"> • Full S-tile roof. • Applied shed roof elements over windows • Exposed rafter tails.
Walls	<ul style="list-style-type: none"> • Stucco is the dominant exterior finish. 	<ul style="list-style-type: none"> • Brick at first floor, which may be painted. • Horizontal wood siding at upper floor. • Thickened walls.
Windows & Doors	<ul style="list-style-type: none"> • Paired windows in groups of two or three. • Tall vertical windows. • Rustic plank entry door. 	<ul style="list-style-type: none"> • First floor arched picture window at cross gable. • Bermuda shutters. • At least one pair of French doors accessing the balcony.
Details	<ul style="list-style-type: none"> • Fixed panel or louvered wood shutters. • Wood railing at balcony to match posts and beams. 	<ul style="list-style-type: none"> • Exposed decorative wood elements at balconies. • Ornate wrought iron railing at balcony.

1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.
2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE SPANISH ECLECTIC

The Spanish Eclectic architectural style in California gained in popularity and sophistication in surface design after the 1915 Panama-California Exposition in San Diego. The Spanish Eclectic house is popular in many historic neighborhoods due to its adaptability of formal and casual, playful character. Historic precedence can be drawn from a wide and diverse range of influences; region, chronology, and function all contribute to the evolution of the Spanish Eclectic style. Truly one of the most diverse architectural styles, the Spanish Eclectic can vary from playful to exotic, bungalow to hacienda.

The Spanish Eclectic style will add to the intrinsic character and rich diversity of the streetscape. Offering an opportunity for bold color statement and whimsical forms, the style will add a playful element to the neighborhood.



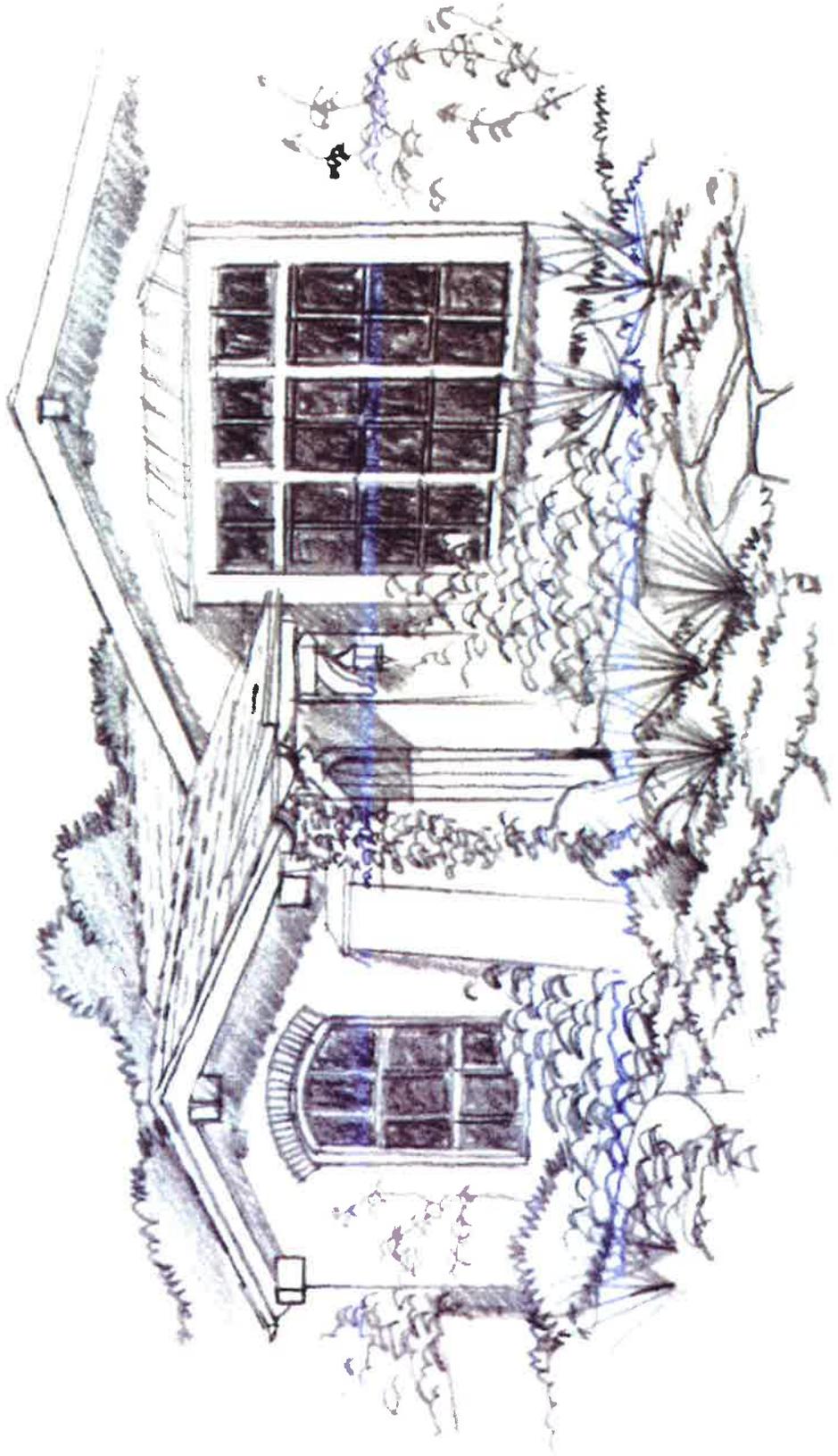
Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1, 2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • One, one-and-a-half (with strong one-story element and stepped back second story), or full two-story massing. (The form is not rigidly defined; this style can be applied to a wide variety of asymmetrical building mass configurations.) • Roof form is predominately pitched, hipped or gabled, but may also be parapeted. 	<ul style="list-style-type: none"> • Massive chimney with buttressed form and elaborate cap with arched opening and small tiled roof. • Massive battered (tapered) chimney with finial chimney cap. • Recessed arcade along front elevation. • Arcaded wing wall. • Balconies.
Roof	<ul style="list-style-type: none"> • Low pitched roof (3:12 to 5:12). • Simple flat, hip, or gable roof with one intersecting gable roof. • Overhangs are typically tight, but can be up to 18". • Fascia is either tight to the building (6" max) or nonexistent with rake tile providing the transition from wall to roof. • S-shaped concrete tiles. • Thin eaves with either a half-round or ogee gutter. 	<ul style="list-style-type: none"> • "Boosted" or raised tiles (may be all or a percentage of the roof field). • Applied shed roof elements over windows. • Exposed rafter tails.
Walls	<ul style="list-style-type: none"> • Stucco is the dominant exterior finish. 	<ul style="list-style-type: none"> • Overgrouted stone, brick, or adobe expressed as a single massing element.
Windows & Doors	<ul style="list-style-type: none"> • Feature recessed arched picture window or three grouped arched windows. • Vertical multi-paned windows or inserts at front elevations. • Window head and jamb trim is absent. • Modest (4" max) window sill trim. 	<ul style="list-style-type: none"> • Accent beveled glass recessed window. • Single or grouped arched windows. • Decorative precast concrete door and window surrounds. • Heavy wood head trim at windows. • Thickened walls. • Juliet balconies. • Bermuda shutters.
Details	<ul style="list-style-type: none"> • Masonry vents. • Canales. 	<ul style="list-style-type: none"> • Shaped rafter tails at feature areas. • Wrought iron balconies and accent details. • Arched stucco column porches. • Vibrant and colorful glazed Spanish tile accents.

1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.
2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE CALIFORNIA COTTAGE

The California Cottage introduces a quaint and picturesque element to the eclectic Russell Ranch streetscape. The Cottage typically has a steeply pitched roof, with the principal roof being side gabled, and multiple asymmetric cross gables. The Cottage is a popular style throughout historic California neighborhoods, with French and English European influences.

Homes may have applied half-timbering, and may feature face brick, which can be painted. Gabled dormers are common, with only modest eave extension. Details such as eyebrow dormers, bell-cast eaves, and arched windows add charm and whimsy to the storybook architectural style. Windows tend to be vertically oriented, often with casements, and often with square gridded or diamond-pane leaded muntins. Cottage houses generally have prominent chimneys.



Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1,2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • One, one-and-a-half, or full two-story massing. • Asymmetrical massing and proportions. • Gable roof form (either front-to-back, side-to-side, or cross-gable). 	<ul style="list-style-type: none"> • Single eyebrow dormer. • Multiple (two or three) gable dormers. • Massive chimney, usually integrated with the dominant gable.
Roof	<ul style="list-style-type: none"> • Low pitched main roof (3:12 to 6:12). • Asphalt composition shingles preferred, concrete tile allowed with raised bargeboard. • Rake at gables up to 12". 	<ul style="list-style-type: none"> • Steeply pitched accent gable (6:12 to 9:12). • Standing seam metal accent roof at dormers or bay windows. • Up to 16" eaves.
Walls	<ul style="list-style-type: none"> • Stucco, lap siding, masonry/brick, stone, or any combination thereof. 	<ul style="list-style-type: none"> • Smooth or imperfect smooth stucco. • Stone or brick wainscot. • Horizontal siding accents at gables and single massing elements. • Masonry as an entire massing element (e.g., chimney, gable end, etc.) • Painted brick.
Windows & Doors	<ul style="list-style-type: none"> • Divided lights common on all windows. • Vertical windows in groupings of two and three. • Head and sill window trim or full window surrounds. • Entry doors accented by trim surrounds. 	<ul style="list-style-type: none"> • Front elevation with all windows inset 2" or one feature window recessed 12". • Round accent window or arched accent window flanked with arched shutters. • Soft arch or radius top windows. • Casement windows. • Mullied window groupings. • Arched entry door. • Brick or stone window and/or door surrounds at key locations. • Bay window. • Deep recessed entry door.
Details	<ul style="list-style-type: none"> • Shutters 	<ul style="list-style-type: none"> • Exposed accent wood timbers and beams. • Cast concrete door surrounds, window trim accents, and/or lentils. • Balcony or windows with decorative metal railings and French doors. • Recessed gable vents. • Leader heads at downspouts. • Brick window and/or door headers at key locations.

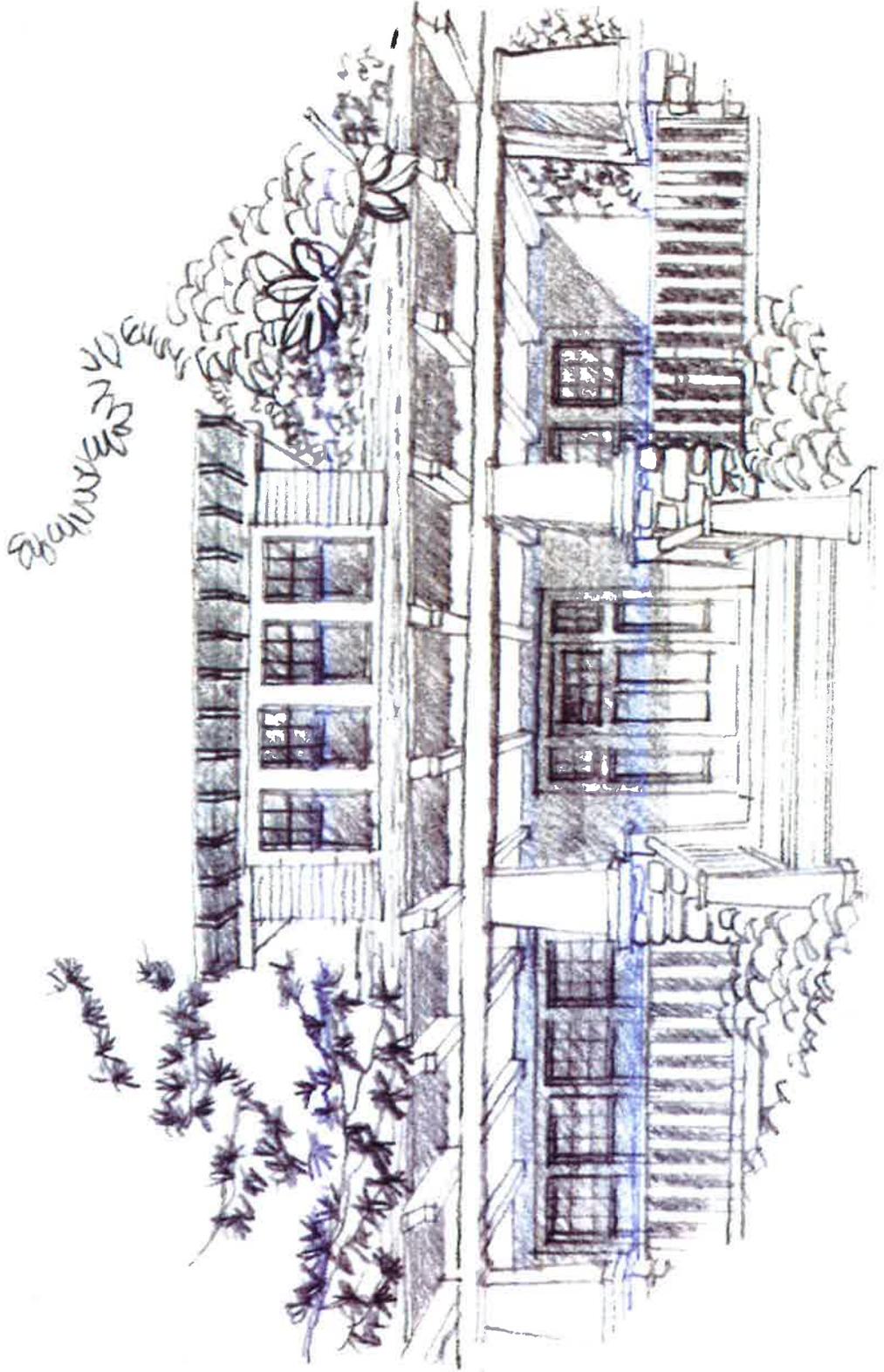
1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.

2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE TRANSITIONAL BUNGALOW

The Craftsman bungalow was the first truly American vernacular style and became a very popular and beloved addition to historic California streetscapes. The Craftsman plan form broke with earlier formal Victorian spatial arrangements and changed the way that families lived in and related to their houses.

Russell Ranch's Transitional Bungalow reinterprets the traditional Craftsman style through a reduction in ornamentation and delicacy, bold material placement emphasizing major architectural elements, and prominent strong architectural forms. Focused on the blending of structure with nature, window walls and clerestory forms effectively bring the outdoors in to the home.



Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1, 2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • Simple massing, front or side gabled, typically one- to one-and-a-half-story massing. • Symmetrical or asymmetrical form. • Deep front entry porch. • Stylized column and beam detailing at porches 	<ul style="list-style-type: none"> • Cross-gabled massing. • Two stories with a combination of one and two-story elements. • 6' minimum full width, deep porch at entry.
Roof	<ul style="list-style-type: none"> • Low-pitched roofs with large over-hanging eaves, emphasizing horizontal planes. • 4:12 to 6:12 roof pitch. • 12" to 18" overhangs. • Asphalt composition shingles 	<ul style="list-style-type: none"> • Concrete roof tile with raised bargeboard. • Varied porch roofs; shed or gabled. • Metal roof at porch (standing seam or corrugated). • Cascading (multiple) gables. • Single large shed dormer. • 18" to 36" overhangs. • Extended and shaped barge rafters. • Exposed rafter tails at eaves.
Walls	<ul style="list-style-type: none"> • Exterior wall materials with combinations of materials, such as wood shingles, horizontal siding, board and batten, and stucco. 	<ul style="list-style-type: none"> • Brick chimneys. • Battered (tapered) stone foundation or wainscot. • Foundation or wainscot using brick.
Windows & Doors	<ul style="list-style-type: none"> • Single hung windows at front elevations. • Use windows individually or in groups (typically two or three). • Doors with full surrounds. • Windows with full surrounds and a projected sill/apron. 	<ul style="list-style-type: none"> • Casement windows. • Three or more windows in a "ribbon." • Grouped windows with a high transom. • Large feature picture window flanked by two narrow vertical windows. • Wide wood entry door with integrated glass. • Wood door and window surrounds.
Details	<ul style="list-style-type: none"> • Entry porches with columns resting on larger piers or bases. • Porch rails of repeated vertical elements. 	<ul style="list-style-type: none"> • Entry porch columns consisting of single or multiple wood posts with rectangular or battered brick or stone piers or bases. • Wood brackets or knee braces. • Porch rails comprised of decoratively cut boards that create a horizontal pattern. • Typical downspouts replaced with "rain chains" or round metal downspouts. • Open eave overhangs with plumb or square cut rafter tails. • Exposed square cut ridge beams, outlookers, and purlins. • Porte-cochère, pergola, or trellis that continues, or is integrated with, the front porch.

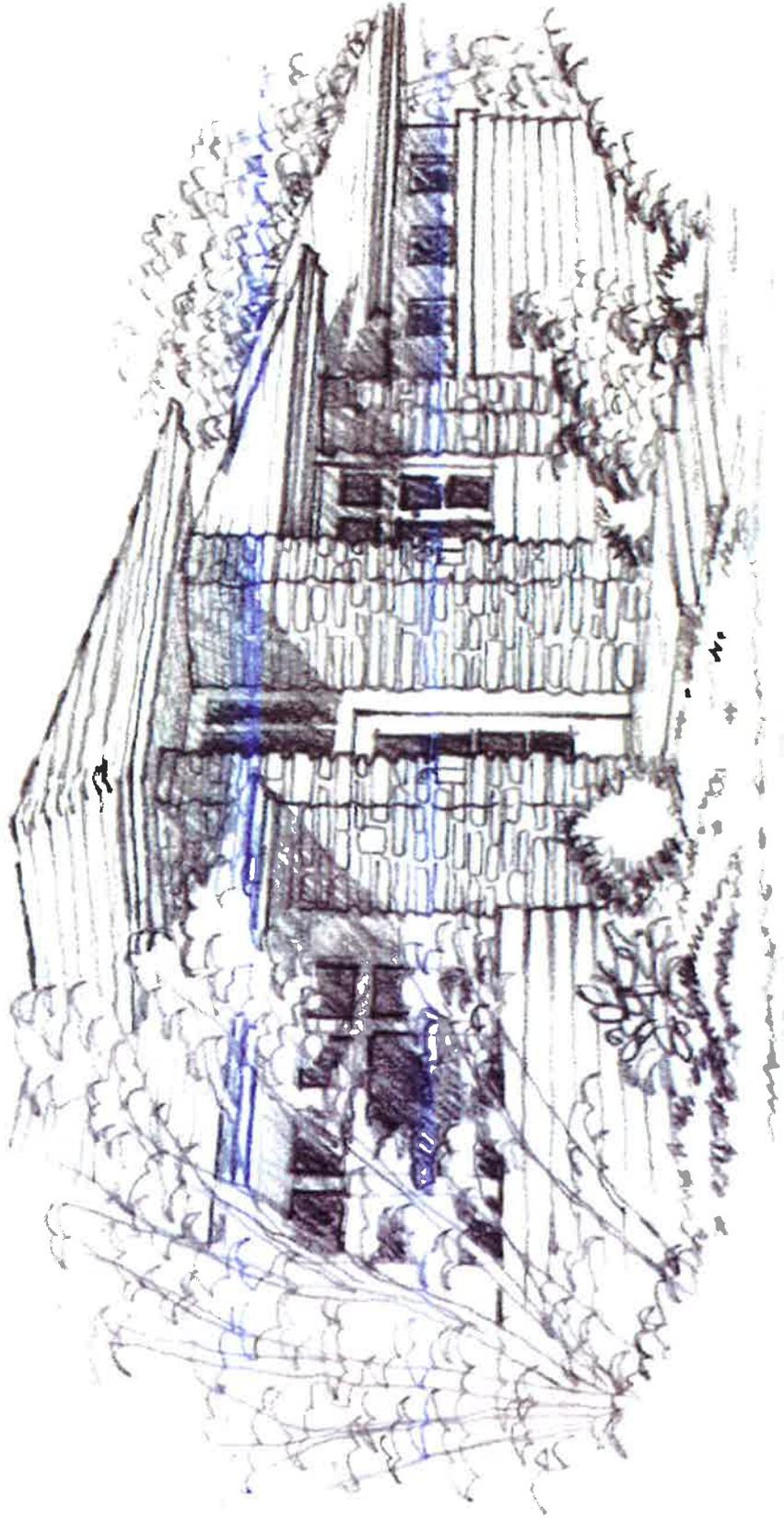
1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.

2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE CALIFORNIA PRAIRIE

The Prairie style was borne of the Chicago Prairie School movement. The style is organic in nature, integrated with the land, using natural materials and abstracted nature forms. The Prairie emphasizes the integration of indoor and outdoor spaces with its trademark wide overhangs, appropriate for the regional climate, identifying the style. Although not as prevalent in the area as the Craftsman style, Prairie homes are very distinctive and add a strong horizontal presence to the community.

The California Prairie at Russell Ranch is a slightly abstracted version of the traditional Prairie style with simplicity in form and detail. A commitment to strong orthogonal forms and linear arrangements tie the California Prairie to its Midwestern roots.



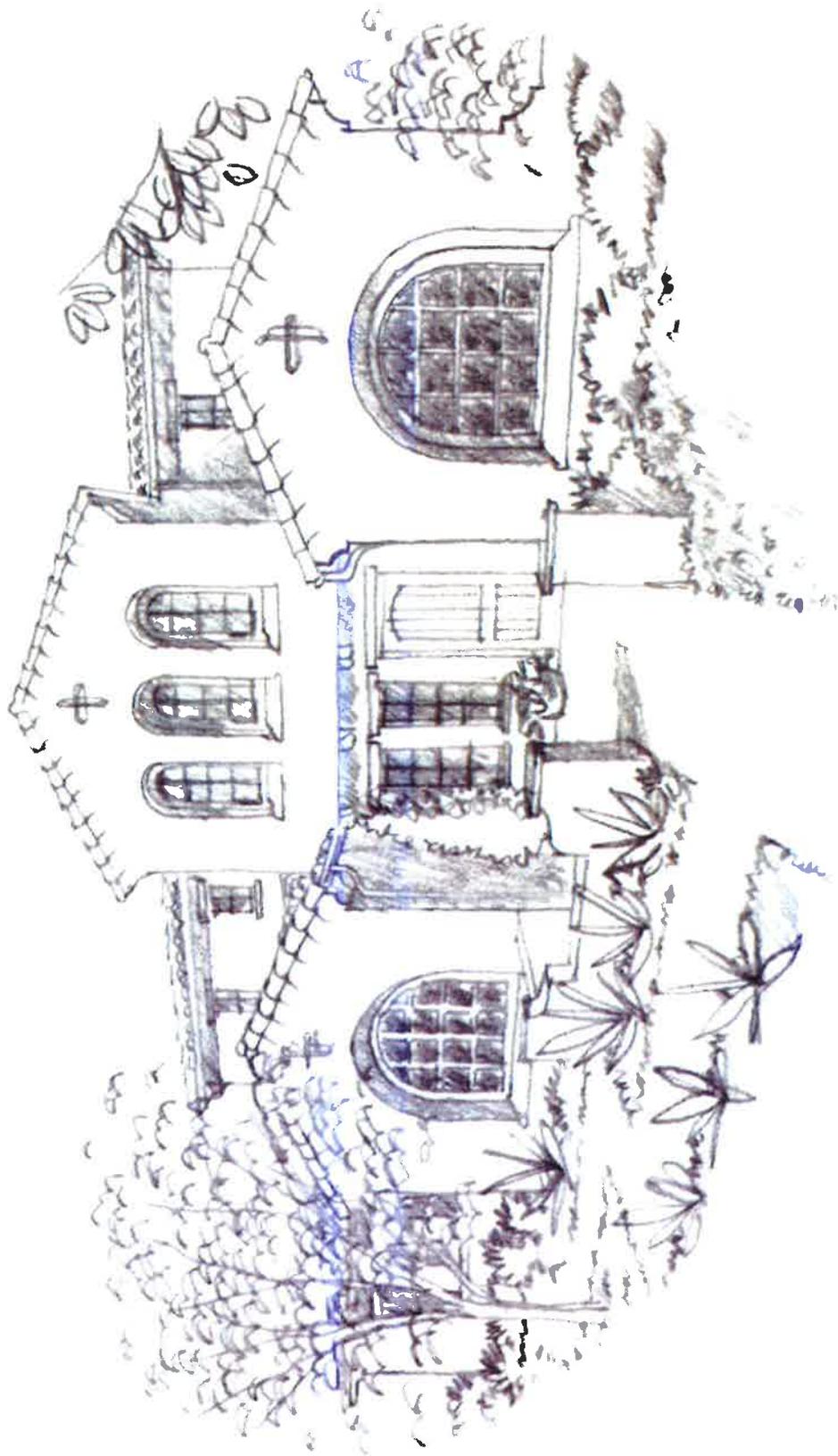
Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1, 2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • One or two-story with strongly horizontal massing. • Secondary masses perpendicular to the primary forms. 	<ul style="list-style-type: none"> • Porte-cocheres (where applicable) and raised porches extending out from the entry of the home. • Accentuated horizontal base extending out as a site or planter wall.
Roof	<ul style="list-style-type: none"> • Long, horizontal low-pitched hip roofs with large overhanging eaves, emphasizing horizontal planes (3.5:12 to 4:12 roof pitch). • 36" minimum overhangs. • Gable roof forms are also appropriate. • Flat concrete tile with a shingle appearance or asphalt composition shingles. 	<ul style="list-style-type: none"> • Terraces covered by primary roof form with massive rectilinear stone piers for roof support. • 12" - 18" overhangs
Walls	<ul style="list-style-type: none"> • Stucco in combination with ledge stone or masonry wainscot base. • Ledge stone used as post bases and fireplaces only. 	<ul style="list-style-type: none"> • Extensive use of brick or ledge stone, used to emphasize the horizontal planes, with struck horizontal grout joints. • Cement plank lap siding is found in some examples.
Windows & Doors	<ul style="list-style-type: none"> • Square or rectangular windows • Grouping and arrangement of windows should emphasize the geometry of the elevation. • Ribbons of windows under deep roof overhangs. • Wood window and door trim. 	<ul style="list-style-type: none"> • Clerestory windows. • Leaded glass inserts at entry. • Accent colored window frames. • Style-specific divided lights
Details	<ul style="list-style-type: none"> • Ornamental railings and gates. • Wood beams and brackets. 	<ul style="list-style-type: none"> • Metal or wood fascia. • Carpenter detailing³. • Style-specific unique lighting fixtures. • Low garden walls to enclose and frame outdoor living spaces. • Massive chimney forms, wrapped in stone or brick.

1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.
2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.
3. Wood trim details (e.g., at doors, windows, eaves, terraces and pergola style porte-cocheres).

THE SPANISH COLONIAL REVIVAL

The 1915 Panama-California Exposition, designed by Spanish Colonial architecture expert Bertram Grosvenor Goodhue, ended the precedent of free adaptations of the Mission style and brought the emergence of precise imitations of more elaborate Spanish prototypes. Goodhue wanted to go beyond the then prevalent Mission interpretations and emphasize the richness of Spanish precedents found throughout Latin America. Fashionable architects of the time began to look directly to Spain for source material, finding a long and rich sequence of architectural traditions, which became melded into a style that became the Spanish Colonial Revival.

The Spanish Colonial Revival introduces a more formal and symmetrical counterpart to the playful Spanish Eclectic style, adding diversity to the Russell Ranch streetscape.



Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1,2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> One, one-and-a-half (with strong one-story elements), or full two-story massing. Roof form is predominantly pitched, hipped or gabled, but may also have a parapeted accent, which may be shaped in a traditional mission form. Primarily symmetrical form. 	<ul style="list-style-type: none"> Massive chimney with buttressed form and elaborate cap with arched opening and small tiled roof. Massive battered (tapered) chimney with finial chimney cap. Recessed arcade along front elevation. Arcaded wing wall. Balconies. Cantilevered second story elements with brackets.
Roof	<ul style="list-style-type: none"> Low pitched roof (3:12 to 5:12). Overhangs are typically tight, but can be up to 18". Fascia is either tight to the building (6" max) or nonexistent with rake tile providing the transition from wall to roof. S-shaped concrete tiles. Thin eaves with either a half-round or ogee gutter. 	<ul style="list-style-type: none"> "Boosted" or raised tiles (may be all or a percentage of the roof field). Exposed rafter tails.
Walls	<ul style="list-style-type: none"> Stucco is the dominant exterior finish. 	
Windows & Doors	<ul style="list-style-type: none"> Feature recessed arched picture windows or three grouped arched windows. Vertical multi-paned windows or inserts at front elevations. Window head and jamb trim is absent. Modest (4" max) window sill trim. Oversized wood entry door. 	<ul style="list-style-type: none"> Multiple feature recessed arched windows. Accent beveled glass recessed window. Single or grouped arched windows. Decorative precast concrete door and window surrounds. Heavy wood head trim at windows. Thickened walls. Juliet balconies. Bermuda shutters.
Details	<ul style="list-style-type: none"> Masonry vents. Canales. 	<ul style="list-style-type: none"> Scalloped eaves. Shaped rafter tails at feature areas. Wrought iron balconies and accent details. Arched stucco column porches. Vibrant and colorful glazed Spanish tile accents.

1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.

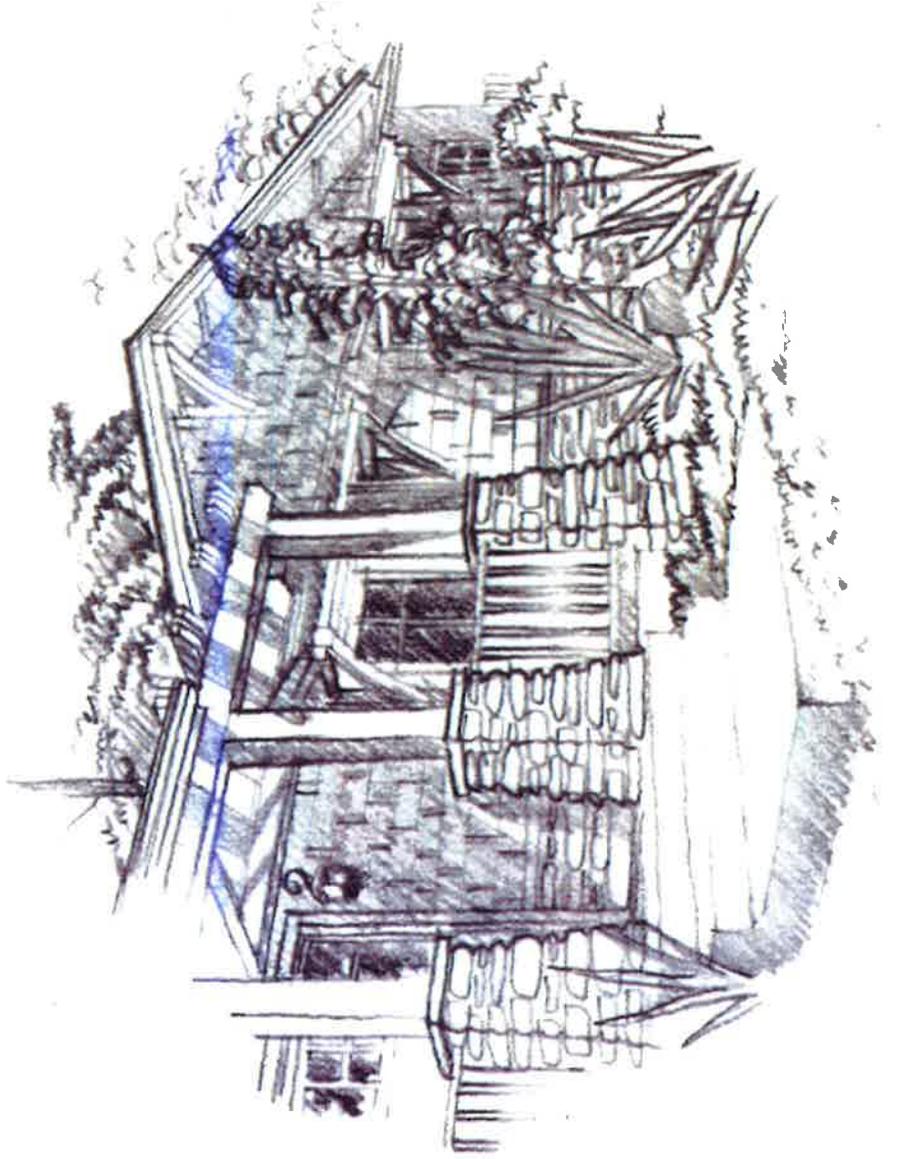
2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE CALIFORNIA CRAFTSMAN

The Craftsman bungalow was an enormously influential form and style of architecture in America between 1906 and 1918, the first truly American vernacular style. The Craftsman plan form broke with earlier formal Victorian spatial arrangements and changed the way that families lived in and related to their houses.

Architecturally, the Craftsman bungalow was designed to achieve harmony between the house and its landscape, to get as close as possible to nature. A Craftsman bungalow has many of the hallmarks of the Arts and Crafts aesthetic: clinker brick, carved rafter tails, a mixture of cladding (brick, clapboard, tile, and shingle), and oversized eave brackets painted in colors of nature.

The California Craftsman recalls the comfortable and welcoming nature of the much-loved historic Craftsman bungalows with a uniquely California flair with large windows and an emphasis on the blending of indoor and outdoor spaces. These homes reflect a sense of permanence that only artisanship and careful design can convey.



Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1,2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> Simple massing, front or side gabled, typically one- to one-and-a-half-story massing. Symmetrical or asymmetrical form. 6' minimum deep front entry porch. Stylized column and beam detailing at porches 	<ul style="list-style-type: none"> Cross-gabled massing. Two stories with a combination of one and two-story elements. Full width, deep porch at entry.
Roof	<ul style="list-style-type: none"> Low-pitched roofs with large over-hanging eaves, emphasizing horizontal planes. 4:12 to 6:12 roof pitch. 12" to 18" overhangs. Asphalt composition shingles preferred. 	<ul style="list-style-type: none"> Concrete roof tile with raised bargeboard. Varied porch roofs; shed or gabled. Cascading (multiple) gables. Roof dormers (shed or gable form). 18" to 36" overhangs. Extended and shaped barge rafters. Exposed rafter tails at eaves.
Walls	<ul style="list-style-type: none"> Exterior wall materials with combinations of materials, such as wood shingles, horizontal siding, board and batten, and stucco. 	<ul style="list-style-type: none"> Stone, brick or combination chimneys. Eliminate stucco as a wall treatment. Foundation or wainscot using brick, stone, or a combination. Battered (tapered) stone foundation or wainscot.
Windows & Doors	<ul style="list-style-type: none"> Single hung windows at front elevations. Use windows individually or in groups (typically two or three). Doors with full surrounds. Windows with full surrounds and a projected sill/apron. 	<ul style="list-style-type: none"> Casement windows. Divided light windows. Three or more windows in a "ribbon." Grouped windows with a high transom. Wide wood entry door with integrated glass. Wood door and window surrounds.
Details	<ul style="list-style-type: none"> Entry porches with columns resting on larger piers or bases. Porch rails of repeated vertical elements. 	<ul style="list-style-type: none"> Wood brackets or knee braces. Entry porch columns consisting of single or multiple wood posts with rectangular or battered brick or stone piers or bases. Porch rails comprised of decoratively cut boards that create a pattern. Additional "stick-work" in gable ends. Typical downspouts replaced with "rain chains." Open eave overhangs with shaped rafter tails. Decorative ridge beams, outlookers, and purlins. Porte-cochère, pergola, or trellis that continues, or is integrated with, the front porch.

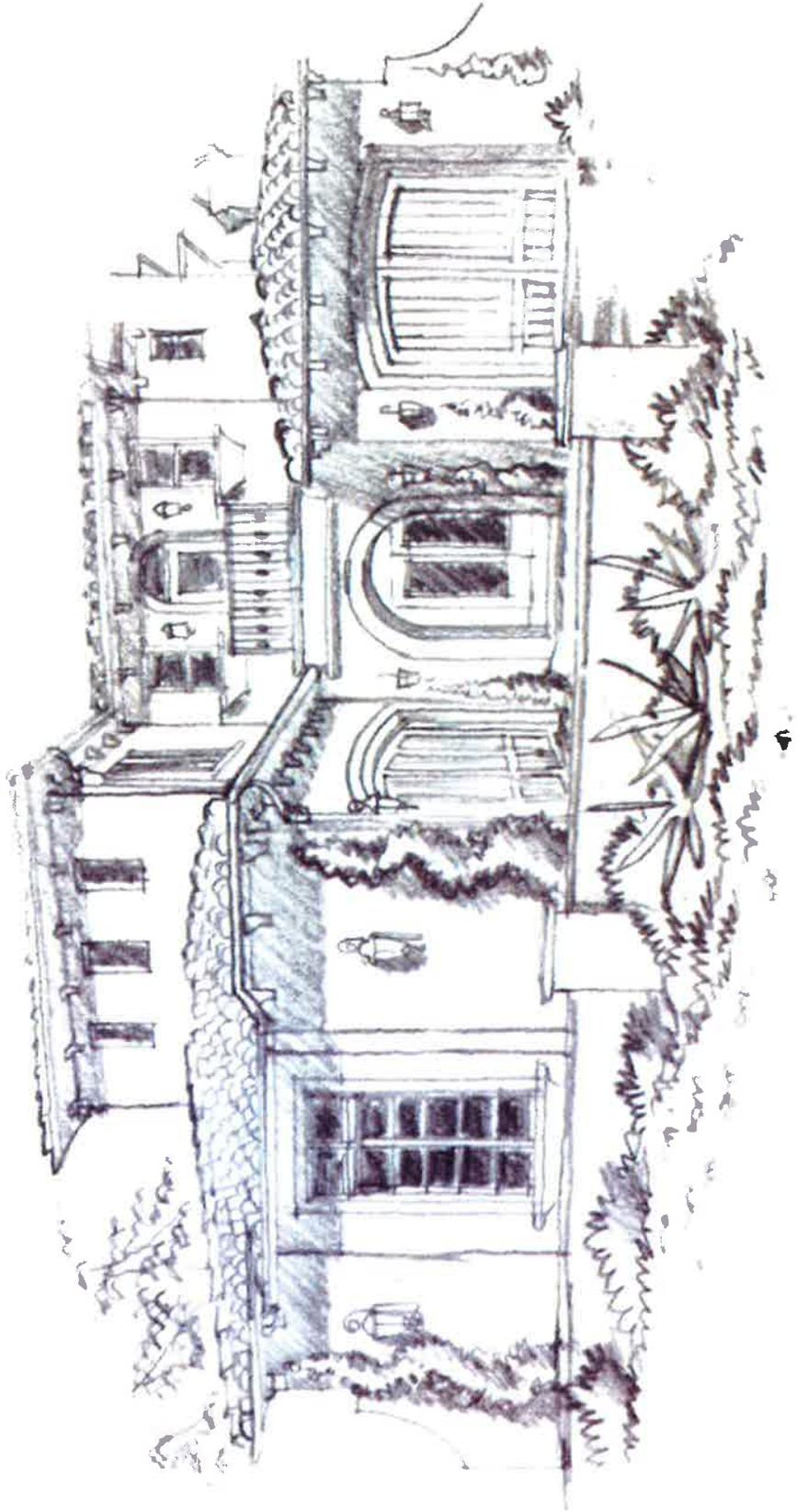
1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.

2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE CALIFORNIA VILLA

The Villa pays homage to Italian Renaissance architecture while introducing elements of California style. Simple, symmetrical, unornamented facades with decorative elements focused to high-visibility areas, such as doors and windows. Small details concentrated in high impact locations work to create a highly ordered style with classical elegance and restrained formality.

Being more formal and vertical in nature, the Villa adds an attractive contrast to some of the casual and eclectic styles within the neighborhood.



Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1,2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • Two-story form. • Simple hipped roof with a flat, symmetrical facade. 	<ul style="list-style-type: none"> • Full-width loggia with a formal and elegantly detailed colonnade.
Roof	<ul style="list-style-type: none"> • Low pitched roof (4:12 to 5:12). • Simple hipped roof. • Broadly overhanging (24" min) boxed eaves. • Barrel or S-shaped concrete tiles. 	<ul style="list-style-type: none"> • Decorative brackets at eaves. • Hipped roof with single-story projecting wings (i.e., porte-cochere or sunroom). • Decorative frieze.
Walls	<ul style="list-style-type: none"> • Stucco is the dominant exterior finish. 	<ul style="list-style-type: none"> • Yellow brick walls. • Horizontal rusticated base of stone or masonry.
Windows & Doors	<ul style="list-style-type: none"> • Formal window arrangement across full facade. • Symmetrical placement of windows. • Smaller windows on upper floors. • Classical door surrounds. 	<ul style="list-style-type: none"> • Full-length first-story windows with arches above. • Palladian window arrangements. • Precast concrete door and window surrounds. • Pedimented door surround with columns. • Arched entry door. • Pedimented windows.
Details	<ul style="list-style-type: none"> • Belt-course to accent second floor plate or window sills. 	<ul style="list-style-type: none"> • Roof-line balustrades. • Molded cornices. • Bracketed window cornices. • Molded precast concrete belt-course to accent second floor plate or window sills. • Shutters (louvered).

1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.

2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

4.5 ACTIVE ADULT

Russell Ranch will feature an active adult neighborhood within the community for residents seeking a living experience designed specifically for residents 55 and better. The active adult neighborhood will feature a private, gated entrance, its own private clubhouse amenity and will share access to the Russell Ranch trail system.

The Active Adult neighborhood will comply with the SFHD guidelines set forth in the Architecture section of this document.

4.6 THE TOWNHOMES

The Townhomes provide an opportunity to create an eclectic urban micro-neighborhood environment within Russell Ranch and there are certain defining elements that the Townhomes must exhibit. When designing townhomes, which are typically distinguished as being narrow in nature, the quantity, scale, and placement of architectural detail must be judicious to not overwhelm the scale of the building.

- The architectural style must be selected from the permitted architectural styles described in this chapter.
- To avoid dominant unbroken planes and create shadow lines, The Townhomes must provide vertical articulation at the front elevation.
- Varied setbacks for different portions of the home, such as garages, second floors, balconies, etc., are encouraged.
- Massing of forms must be established using the fundamental characteristics of the selected architectural style.

- Contemporary interpretations of the architectural styles permitted for The Townhomes are encouraged through the use of eclectic materials, such as metals (must be anti-reflective) and variations on traditional siding, asymmetrical roof pitches, and playful massing and use of color.



Townhome Concept Imagery

4.7 THE RECREATION CENTER

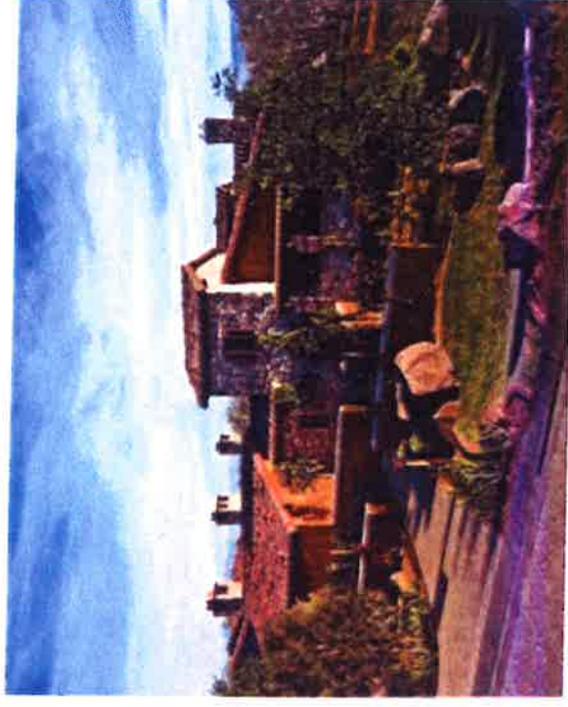
Russell Ranch will feature two recreation centers: one for the multigenerational portion of the neighborhood and one for the active adult portion of the neighborhood. The recreation centers will provide an opportunity for residents of the neighborhood to gather and enjoy a beautiful indoor-outdoor environment. The Recreation Centers will feature architecture that blends historic with contemporary to create a truly iconic neighborhood building that residents and visitors alike will admire. The design intent is to convey the impression of an historic structure with a contemporary addition.

The following defining characteristics are intended as a kit of parts and set of rules to influence the design of the Recreation Centers. A combination of these elements should be used to inform the design of the building (all of the elements are not required).

- Casual, asymmetrical form with a combination of gable and shed roof forms.

- o Overgrouted stone as a significant wall material, utilized to present full massing elements. For example, an entire central mass of the structure may feature stone, with wings of a complementary material, such as stucco. This design solution gives the appearance of an original structure with wings added on over time.
- o A clerestory roof form in the central portion of the structure is appropriate.
- o Wall materials may be overgrouted stone or brick, smooth or imperfect smooth stucco or plaster, and glass.
- o Accent materials include heavy beams, rusted metal, wrought iron, copper, and precast concrete.
- o The primary roof should be barrel or S-tile, which may be boosted.
- o The contemporary portion of the building may feature a metal standing seam roof.
- o Rain chains should be used in place of traditional downspouts.
- o Windows may be recessed and void of trim, or feature full window surrounds, which may be precast concrete or wood.
- o Arched windows are appropriate as feature or accent windows.
- o Exposed beams and structural connections should be celebrated as part of the design statement.
- o Site structures, such as restrooms, cabanas, pool equipment rooms, and trash enclosures must complement the design of the primary structure.

- o Mechanical and pool equipment must be screened from sight through integrated walls, landscaping, or a combination of both.



Recreation Center Concept Imagery



4.8 THE SCHOOL

Although it is recognized that school architecture is governed by functionality and a specialized kit of parts and set of rules, the elementary school located at Russell Ranch is encouraged to complement the surrounding neighborhood in its design. The following recommendations are encouraged for consideration in design of the school:

- o The school is encouraged to draw inspiration from the approved architectural styles found in these Guidelines.
- o A simplified interpretation of the selected architectural style is appropriate.
- o Earth tone colors are encouraged in rich hues.
- o Recommended wall materials include stucco, brick, horizontal siding, and board and batten siding.
- o Standing seam metal is the recommended material for the roof in dark earth tone colors.



School Concept Imagery

PLANNING COMMISSION STAFF REPORT

PROJECT TITLE	Russell Ranch Phase 1, Villages 3, 5, and 7 Residential Design Review
PROPOSAL	Request for Residential Design Review Approval for 108 single-family residential units located within Phase 1, Villages 3, 5, and 7 of the Russell Ranch Subdivision
RECOMMENDED ACTION	Approve, based upon findings and subject to conditions
OWNER/APPLICANT	The New Home Company
LOCATION	The three project sites (10.5, 12.5, and 21.2-acres respectively) are generally located east of Placerville Road and north of Grand Prairie Road within Phase 1 of the Russell Ranch Subdivision
SITE CHARACTERISTICS	The project sites are currently in the process of being rough-graded, with grading work expected to be completed by early December
GENERAL PLAN DESIGNATION	SF (Single Family) and SFHD (Single Family High Density)
ZONING DESIGNATION	SF-PD (Single Family, Planned Development District) and SFHD-PD (Single Family High Density, Planned Development District)
ADJACENT LAND USES/ZONING	North: Undeveloped Residential Property (SF PD) with Open Space and Alder Creek Parkway Beyond South: Grand Prairie Road with Undeveloped Residential Property (SF PD) Beyond East: Grand Prairie Road with Undeveloped Residential Property (SF PD) Beyond

West: Placerville Road with Undeveloped Residential Property (SFHD PD) Beyond

PREVIOUS ACTION

City Council approval of a General Plan Amendment, Specific Plan Amendment, Large-Lot Vesting Tentative Subdivision Map, Small-Lot Vesting Tentative Subdivision Map, Planned Development Permit, Design Guidelines, Inclusionary Housing Plan, and Amended and Restated Development Agreement Amendment for development of an 879-unit single-family residential subdivision (Russell Ranch Subdivision) on May 15, 2015

City Council approval of an Amended Large-Lot Vesting Tentative Subdivision Map and Small-Lot Vesting Tentative Subdivision Map for development of an 852-unit single-family residential subdivision (Russell Ranch Subdivision) on June 28, 2016

City Council approval of a General Plan Amendment, Specific Plan Amendment, Design Guidelines Amendment, Large-Lot Vesting Tentative Subdivision Map, Small-Lot Vesting Tentative Subdivision Map, and Development Agreement Amendment for the development of a 389 unit residential subdivision (Russell Ranch Lots 24-32 Subdivision) on March 27, 2018

Planning Commission approval of a Design Review Application for 114 single-family residential units located within Phase 1, Village 4 (Courts at Folsom Ranch) of the Russell Ranch Subdivision on October 17, 2018

FUTURE ACTION

Approval of a Final Map and Issuance of Building Permits

APPLICABLE CODES

FPASP (Folsom Plan Area Specific Plan)
Russell Ranch Design Guidelines
FMC 17.06, Design Review

ENVIRONMENTAL REVIEW

An Environmental Impact Report has previously been certified for the Russell Ranch Subdivision project on May 15, 2015 by the City Council in accordance with the requirements of the California Environmental Quality Act (CEQA) and the CEQA Guidelines

ATTACHED REFERENCE MATERIAL

1. Vicinity Map
2. Russell Ranch Phase 1 Lot Mix Exhibit and Site Plan
4. Typical Lot Layout, dated September, 2018
5. Color Street Scene
6. Building Elevations and Floor Plans, dated September 21, 2018
7. Color and Material Palette, dated March 16, 2018
8. Russell Ranch Design Guidelines

PROJECT PLANNER

Steve Banks, Principal Planner

BACKGROUND

On May 15, 2018, the City Council approved a General Plan Amendment, Specific Plan Amendment, Large-Lot Vesting Tentative Subdivision Map, Small-Lot Vesting Tentative Subdivision Map, Planned Development Permit, Design Guidelines, Inclusionary Housing Plan, and Amended and Restated Development Agreement Amendment for development of an 879-unit single-family residential subdivision known as the Russell Ranch Subdivision within the eastern portion of the Folsom Plan Area. As part of the aforementioned approvals, the Russell Ranch Design Guidelines were established to act as an implementation tool for residential development within the Russell Ranch Subdivision.

On June 28, 2016, the City Council approved an Amended Large-Lot Vesting Tentative Subdivision Map and an Amended Small-Lot Vesting Tentative Subdivision Map for development of an 852-unit single-family residential subdivision (Russell Ranch Subdivision). On March 27, 2018, the City Council approval of a General Plan Amendment, Specific Plan Amendment, Design Guidelines Amendment, Large-Lot Vesting Tentative Subdivision Map, Small-Lot Vesting Tentative Subdivision Map, and Development Agreement Amendment for the development of an 389-unit residential subdivision (Russell Ranch Lots 24-32 Subdivision). The Design Guidelines Amendment provided additional direction in terms of the architecture and design of the active-adult community, the community center, and the townhome portions of the Russell Ranch Subdivision.

On October 17, 2018, the Planning Commission approved a Design Review Application for 114 single-family “court-style” (Village 4 features a court-style design in which groups of six homes are located on private courts) residential units located within Phase 1, Village 4 (Courts at Russell Ranch) of the previously approved Russell Ranch Subdivision project. The design review approval for Village 4 included three individual master plans with three distinct California heritage-themed architectural styles (Bungalow, California Cottage, and Craftsman) and nine color and material alternatives.

APPLICANT'S PROPOSAL

The applicant, The New Home Company, is requesting residential design review approval for 108 traditional single-family residential units located within Phase 1, Villages 3, 5, and 7 of the previously approved Russell Ranch Subdivision project. Specifically, the applicant is requesting design review approval for four (4) individual master plans within Villages 3, 5, and 7. The master plans include three (3) distinct California heritage-themed architectural styles (California Prairie, California Villa, and California Wine Country) and nine (9) color and material alternatives. The proposed master plans, which feature three, two-story models and one, single-story model, range in size from 2,626 to 3,500 square feet (3BR/3.5BA to 5BR/4.5BA) and include an attached two-car garage. The three classic design themes are characterized by a variety of unique architectural elements including varied roof shapes and forms, covered entries, distinct window design, and enhanced decorative elements. Proposed building materials include stucco, cement plank lap siding, ledgerstone, stone veneer, wrought iron details, exposed rafter tails, overgrouted stone, form window trim, multi-paned windows, themed garage doors, decorative light fixtures, and concrete roof tiles. In addition, there are 9 distinct color and material alternatives available for each of the master plans resulting in 108 different visual expressions.

Architecture/Design

As described previously, the proposed project features three distinct architectural themes that have been chosen from the traditional heritage of California home styles including California Prairie, California Villa, and California Wine Country. The following is a description of each of the aforementioned architectural styles proposed for Villages 3, 5, and 7 within the Russell Ranch Subdivision:

California Prairie

The California Prairie design, also known as Modern Prairie, is a late 19th and early 20th century architectural style that has its roots in the City of Chicago. Frank Lloyd Wright, the most famous proponent of this architectural style, promoted the idea of "organic architecture", the primary principal of which was that a structure should look as if it belongs on the site as if it naturally grew there. California Prairie is a distinctly American style defined by simple structured massing, horizontal lines, and flat or hipped roofs with broad overhanging eaves. The California Prairie style is also recognized for its use of brick, stone, and stucco exteriors with restrained application of ornamental materials.

California Villa

Artfully combining Spanish Colonial and California Hacienda vernaculars, the California Villa style translates these quintessential Early California aesthetics into one style, more refined and clean in its execution than its predecessors. The style echoes required elements of both Spanish Colonial and Early California Ranch aesthetics as depicted by the Russell Ranch Design Guidelines. The form is inherently asymmetrical, simplistic in its massing, and is articulated by low-pitched gable roofs and expressed entries. Comprised primarily of stucco, the facade is accentuated by barrel-tiled roofs, arched windows, wood detailing, and minimal wrought iron. Entries are emphasized with arches, single-story elements, and porches.

California Wine Country

Present in more rural, agrarian parts of California and influenced by the vineyard estates scattered throughout California's wine country, the California Wine Country style has a built-over-time feel, expressed through its rustic material palette and building forms. Comparable to elements of

Western Farmhouse and Early California Ranch, as defined by the Russell Ranch Design Guidelines, the form is humble and simple in its appearance. Gable roofs dominate and exhibit steep roof pitches, while roof tails further distinguish the style. Windows appear as structured compositional elements, emphasized by wrapping foam trim and overgrouted stone. A rich material palette of stucco, stone veneer, and textured concrete roof tile contributes to feeling that the structure was developed over time, while the clean execution of its form and materials provides a distinctly contemporary impression.

The proposed project, which is located within the eastern portion of the Folsom Plan Area, is subject to the Russell Ranch Design Guidelines, which were originally approved by the City Council in 2015 and modified on March 27, 2018. The Design Guidelines are a complementary document to the Folsom Plan Area Specific Plan and the Folsom Plan Area Specific Plan Community Guidelines. The Design Guidelines, which are intended to act as an implementation tool for residential development within the Russell Ranch Subdivision, provide the design framework for architecture, streetscene, and landscaping to convey a master plan identity. The Design Guidelines also establish the pattern and intensity of development for the Russell Ranch Subdivision to ensure a high quality and aesthetically cohesive environment. While these Design Guidelines establish the quality of architectural and landscape development for the overall subdivision, they are not intended to prevent alternative designs and/or concepts that are compatible with the overall project theme.

As a regulatory tool, the Design Guidelines are intended to assist applicants in creating single-family residential neighborhoods that reflect the City's rich history, reinforce the sense of community, and utilize sustainable best practices. The Design Guidelines also provide the framework for design review approval of residential projects within the Russell Ranch Subdivision. In addition, the Design Guidelines are intended to be used by builders and developers when designing their Master Plot Plans. Any development project that is submitted to the City must be reviewed for consistency with these Design Guidelines. The following are the general architectural principles intended to guide the design of the Russell Ranch Subdivision to ensure quality development:

- Master Home Plan series must include a minimum of three unique building elevations
- Provide an elegant and diverse streetscape
- Building massing must be appropriate and authentic to the architectural style
- Horizontal and vertical articulation is required
- Include varied roof shapes and forms
- Avoid repetition of identical architectural styles
- Ensure four-sided architecture is provided
- Highly visible lots require additional architectural enhancements

In addition to the general architectural principles referenced above, the Design Guidelines also provide specific direction regarding a number of architectural situations and features including: building forms, roof details, wall materials, window and door designs, and decorative details. Each of the architectural styles is required to include a specific set of style elements (form, roof, walls, windows/doors, and details) based on the particular design theme. Based on the highly visible nature of the residential lots within the Russell Ranch Subdivision, the Design Guidelines also require that corner lots and lots adjacent to open space provide additional enhanced elements above and beyond the minimum required style elements for the building elevations facing the street(s) and open space areas respectively. The following are examples of required and enhanced style elements that are relevant to the proposed project:

- Provide symmetrical or asymmetrical form
- Utilize simple massing, front or side gabled
- Include a deep front entry porch
- Provide low-pitched roofs with large over-hanging eaves
- Wall materials should include stucco, lap siding, wood shingles, and masonry/brick
- Use windows individually or in groups
- Provide head and sill window trim or full window surrounds
- Utilize two stories with combination of one and two-story elements (enhanced element)
- Include steep pitched accent gable (enhanced element)
- Provide smooth or imperfect smooth stucco (enhanced element)
- Utilize casement windows (enhanced element)
- Feature entry porch columns with single or multiple posts (enhanced element)
- Include exposed rafter tails or eaves (enhanced element)

In reviewing the architecture and design of the project, staff determined that the design of the four proposed master plans (which also include 3 elevation plans, 9 color and material alternatives, and 108 architectural and visual expressions) accurately reflect the level and type of high quality design features recommended by the Russell Ranch Design Guidelines. Specifically, the master plans are responsive to views on all four building elevations and include a variety of unique architectural elements that create an interesting streetscape scene including: varied roof shapes and forms, gable roof forms, covered entries, distinct window design, themed garage doors, and enhanced decorative elements. In addition, each of the proposed master plans meet or exceed the minimum and enhanced style element requirements as articulated in the Design Guidelines.

The proposed building materials (stucco, cement plank lap siding, ledgerstone, stone veneer, wrought iron details, exposed rafter tails, overgrouted stone, form window trim, multi-paned

windows, themed garage doors, decorative light fixtures, and concrete roof tiles) are consistent with the materials recommended by the Russell Ranch Design Guidelines. In addition, the proposed project includes distinct color schemes, which are consistent with each of the architectural styles, thus enhancing the visual interest of each of the master plans. Taking into consideration the aforementioned architectural details, materials, and colors, staff has determined that the master plans are consistent with the design principles established by the Russell Ranch Design Guidelines. Staff forwards the following design recommendations to the Commission for consideration:

1. This approval is for four, one and two-story master plans (3 building elevations with 9 color and material options and 108 visual expressions) for the Russell Ranch Villages 3, 5, and 7 Subdivision. The applicant shall submit building plans that comply with this approval, the attached building elevations dated September 21, 2018.
2. The design, materials, and colors of the proposed Russell Ranch Villages 3, 5, and 7 Subdivision single-family residential units shall be consistent with the submitted building elevations, materials samples, and color scheme to the satisfaction of the Community Development Department.
3. The Community Development Department shall approve the individual lot permits to assure no duplication or repetition of the same house, same roof-line, same elevation style, side-by-side, or across the street from each other.
4. All mechanical equipment shall be ground-mounted and concealed from view of public streets, neighboring properties and nearby higher buildings. For lots abutting the open space areas, mechanical equipment shall be located out of view from open space areas.
5. Lighting shall be designed to be directed downward onto the project site and away from adjacent properties and public rights-of-way. In addition, final exterior building and site lighting plans shall be submitted for review and approval by Community Development Department for aesthetics, level

These recommendations listed above are included in the conditions of approval presented for consideration by the Planning Commission (Condition No. 11).

ENERGY CONSERVATION

To reduce impacts in terms of energy and water consumption, the proposed project is required to meet the 2017 Title 24 Building Envelope Energy Efficiency Standards. The project will be allowed to achieve this performance standard through a combination of measures to reduce energy use for heating, cooling, water heating and ventilation. Because energy use for each different system type (i.e., heating, cooling, water heating, and ventilation) as well as appliances is defined, this method will also easily allow for application of individual measures aimed at reducing the energy use of these devices in a prescriptive manner.

ENVIRONMENTAL REVIEW

An Environmental Impact Report has previously been certified for the Russell Ranch Subdivision project on May 15, 2015 by the City Council in accordance with the requirements of the California Environmental Quality Act (CEQA) and the CEQA Guidelines. Staff has determined that no new

impacts will result from development of the subject project that were not already considered with the previous approval. No further environmental review is required.

RECOMMENDATION/PLANNING COMMISSION ACTION

MOVE TO APPROVE THE DESIGN REVIEW APPLICATION FOR 108 SINGLE-FAMILY RESIDENTIAL UNITS AS ILLUSTRATED ON ATTACHMENTS 2 THROUGH 6 FOR THE RUSSELL RANCH PHASE 1, VILLAGES 3, 5, AND 7 SUBDIVISION PROJECT WITH THE FOLLOWING FINDINGS AND CONDITIONS (NO. 1-13).

GENERAL FINDINGS

- A. NOTICE OF HEARING HAS BEEN GIVEN AT THE TIME AND IN THE MANNER REQUIRED BY STATE LAW AND CITY CODE.
- B. THE PROJECT IS CONSISTENT WITH THE GENERAL PLAN, THE FOLSOM PLAN AREA SPECIFIC PLAN, AND THE RUSSELL RANCH SUBDIVISION DESIGN GUIDELINES.

CEQA FINDING

- C. AN ENVIRONMENTAL IMPACT REPORT HAS PREVIOUSLY BEEN CERTIFIED FOR THE RUSSELL RANCH SUBDIVISION PROJECT ON MAY 15, 2015 BY THE CITY COUNCIL IN ACCORDANCE WITH THE REQUIREMENTS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA).
- D. NO NEW IMPACTS WILL RESULT FROM DEVELOPMENT OF THE SUBJECT PROJECT THAT WERE NOT ALREADY CONSIDERED WITH THE PREVIOUS APPROVAL.
- E. NO FURTHER ENVIRONMENTAL REVIEW IS REQUIRED.

DESIGN REVIEW FINDING

- F. THE PROJECT IS IN COMPLIANCE WITH THE GENERAL PLAN, THE FOLSOM PLAN AREA SPECIFIC PLAN, AND THE APPLICABLE ZONING ORDINANCES.
- G. THE PROJECT IS IN CONFORMANCE WITH THE RUSSELL RANCH DESIGN GUIDELINES.
- H. THE BUILDING MATERIALS, TEXTURES, AND COLORS OF THE PROJECT WILL BE COMPATIBLE WITH SURROUNDING DEVELOPMENT AND CONSISTENT WITH THE GENERAL DESIGN THEME OF THE NEIGHBORHOOD.

Submitted:



PAM JOHNS

Community Development Director

CONDITIONS

See attached tables of conditions for which the following legend applies.

RESPONSIBLE DEPARTMENT		WHEN REQUIRED	
CD	Community Development	I	Prior to approval of Improvement Plans
(P)	Planning Division	M	Prior to approval of Final Map
(E)	Engineering Division	B	Prior to issuance of first Building Permit
(B)	Building Division	O	Prior to approval of Occupancy Permit
(F)	Fire Division	G	Prior to issuance of Grading Permit
PW	Public Works Department	DC	During construction
PR	Park and Recreation Department	OG	On-going requirement
PD	Police Department		

**CONDITIONS OF APPROVAL FOR THE RUSSELL RANCH VILLAGES 3, 5, and 7 SUBDIVISION PROJECT (PN 18-349)
EAST OF PLACERVILLE ROAD AND NORTH OF GRAND PRAIRIE ROAD
RESIDENTIAL DESIGN REVIEW**

	Mitigation Measure	Condition/Mitigation Measure	When Required	Responsible Department
1.		<p>The applicant shall submit final site development plans to the Community Development Department that shall substantially conform to the exhibits referenced below:</p> <ul style="list-style-type: none"> • Russell Ranch Phase 1 Lot Mix Exhibit and Site Plan • Typical Lot Layout, dated September, 2018 • Color Street Scene • Building Elevations and Floor Plans, dated September 21, 2018 • Color and Material Palette, dated March 16, 2018 <p>This project approval is for Russell Ranch Villages 3, 5, and 7 Subdivision Residential Design Review, which includes architectural and design details for 108 single-family residential units situated within Villages 3, 5, and 7 of the Russell Ranch Subdivision project. Implementation of the project shall be consistent with the above-referenced items as modified by these conditions of approval.</p>	B	CD (P)(E)
2.		<p>Building plans shall be submitted to the Community Development Department for review and approval to ensure conformance with this approval and with relevant codes, policies, standards and other requirements of the City of Folsom.</p>	B	CD (P)(E)(B)
3.		<p>The project approvals granted under this staff report (Residential Design Review) shall remain in effect for two years from final date of approval (November 7, 2020). Failure to obtain the relevant building (or other) permits within this time period, without the subsequent extension of this approval, shall result in the termination of this approval.</p>	B	CD (P)

**CONDITIONS OF APPROVAL FOR THE RUSSELL RANCH VILLAGES 3, 5, and 7 SUBDIVISION PROJECT (PN 18-349)
 EAST OF PLACERVILLE ROAD AND NORTH OF GRAND PRAIRIE ROAD
 RESIDENTIAL DESIGN REVIEW**

Mitigation Measure	Condition/Mitigation Measure	When Required	Responsible Department
4.	<p>The owner/applicant shall defend, indemnify, and hold harmless the City and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void, or annul any approval by the City or any of its agencies, departments, commissions, agents, officers, employees, or legislative body concerning the project. The City will promptly notify the owner/applicant of any such claim, action or proceeding, and will cooperate fully in the defense. The City may, within its unlimited discretion, participate in the defense of any such claim, action or proceeding if both of the following occur:</p> <ul style="list-style-type: none"> • The City bears its own attorney's fees and costs; and • The City defends the claim, action or proceeding in good faith <p>The owner/applicant shall not be required to pay or perform any settlement of such claim, action or proceeding unless the settlement is approved by the owner/applicant.</p>	OG	CD (P)(E)(B) PW, PR, FD, PD, NS
DEVELOPMENT COSTS AND FEE REQUIREMENTS			
5.	The owner/applicant shall pay all applicable taxes, fees and charges at the rate and amount in effect at the time such taxes, fees and charges become due and payable.	B	CD (P)(E)
6.	If applicable, the owner/applicant shall pay off any existing assessments against the property, or file necessary segregation request and pay applicable fees.	B	CD (E)
7.	The City, at its sole discretion, may utilize the services of outside legal counsel to assist in the implementation of this project, including, but not limited to, drafting, reviewing and/or revising agreements and/or other documentation for the project. If the City utilizes the services of such outside legal counsel, the applicant shall reimburse the City for all outside legal fees and costs incurred by the City for such services. The applicant may be required, at the sole discretion of the City Attorney, to submit a deposit to the City for these services prior to initiation of the services. The applicant shall be responsible for reimbursement to the City for the services regardless of whether a deposit is required.	B	CD (P)(E)

**CONDITIONS OF APPROVAL FOR THE RUSSELL RANCH VILLAGES 3, 5, and 7 SUBDIVISION PROJECT (PN 18-349)
EAST OF PLACERVILLE ROAD AND NORTH OF GRAND PRAIRIE ROAD
RESIDENTIAL DESIGN REVIEW**

Mitigation Measure	Condition/Mitigation Measure	When Required	Responsible Department
8.	<p>If the City utilizes the services of consultants to prepare special studies or provide specialized design review or inspection services for the project, the applicant shall reimburse the City for actual costs it incurs in utilizing these services, including administrative costs for City personnel. A deposit for these services shall be provided prior to initiating review of the Final Map, improvement plans, or beginning inspection, whichever is applicable.</p>	B	CD (P)(E)
9.	<p>This project shall be subject to all City-wide development impact fees, unless exempt by previous agreement. This project shall be subject to all City-wide development impact fees in effect at such time that a building permit is issued. These fees may include, but are not limited to, fees for fire protection, park facilities, park equipment, Humbug-Willow Creek Parkway, Light Rail, TSM, capital facilities and traffic impacts. The 90-day protest period for all fees, dedications, reservations or other exactions imposed on this project will begin on the date of final approval (November 7, 2018). The fees shall be calculated at the fee rate in effect at the time of building permit issuance.</p>	B	CD (P)(E), PW, PK
10.	<p>The owner/applicant agrees to pay to the Folsom-Cordova Unified School District the maximum fee authorized by law for the construction and/or reconstruction of school facilities. The applicable fee shall be the fee established by the School District that is in effect at the time of the issuance of a building permit. Specifically, the owner/applicant agrees to pay any and all fees and charges and comply with any and all dedications or other requirements authorized under Section 17620 of the Education Code; Chapter 4.7 (commencing with Section 65970) of the Government Code; and Sections 65995, 65995.5 and 65995.7 of the Government Code.</p>	B	CD (P)

ARCHITECTURE/SITE DESIGN REQUIREMENTS

The project shall comply with the following architecture and design requirements:

1. This approval is for four, one and two-story master plans (3 building elevations with 9 color and material options and 108 visual expressions) for the Russell Ranch Villages 3, 5, and 7 Subdivision. The applicant shall submit building plans that comply with this approval, the attached building elevations dated September 21, 2018.
2. The design, materials, and colors of the proposed Russell Ranch Villages 3, 5, and 7 Subdivision single-family residential units shall be consistent with the submitted building elevations, materials samples, and color scheme to the satisfaction of the Community Development Department.
3. The Community Development Department shall approve the individual lot permits to assure no duplication or repetition of the same house, same roof-line, same elevation style, side-by-side, or across the street from each other.
4. All mechanical equipment shall be ground-mounted and concealed from view of public streets, neighboring properties and nearby higher buildings. For lots abutting the open space areas, mechanical equipment shall be located out of view from open space areas.
5. Lighting shall be designed to be directed downward onto the project site and away from adjacent properties and public rights-of-way. In addition, final exterior building and site lighting plans shall be submitted for review and approval by Community Development Department for aesthetics, level of illumination, glare and trespass prior to the issuance of any building permits.

B

CD (P) (B)

11.

FIRE DEPARTMENT REQUIREMENT

12.	The building shall have illuminated addresses visible from the street or drive fronting the property. Size and location of address identification shall be reviewed and improved by the Fire Marshal.	B	FD
-----	---	---	----

POLICE/SECURITY REQUIREMENT

13.	<p>The owner/applicant shall consult with the Police Department in order to incorporate all reasonable crime prevention measures. The following security/safety measures shall be required:</p> <ul style="list-style-type: none"> • A security guard shall be on-duty at all times at the site or another approved security measure shall be in place including but not limited to a six-foot security fence shall be constructed around the perimeter of construction areas. (This requirement shall be included on the approved construction drawings). • Security measures for the safety of all construction equipment and unit appliances shall be employed. • Landscaping shall not cover exterior doors or windows, block line-of-sight at intersections or screen overhead lighting. 	B	PD
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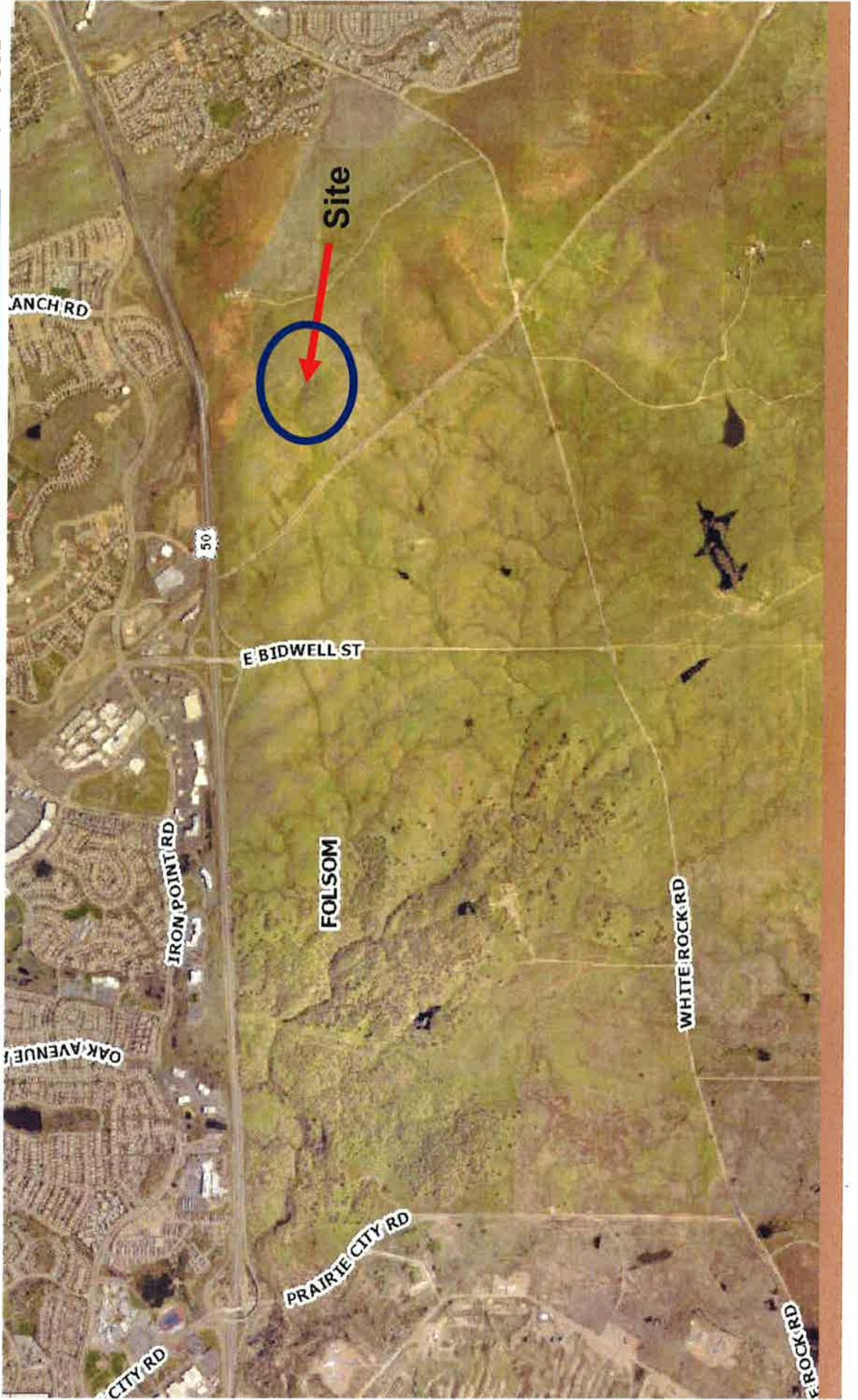
Attachment 1

Vicinity Map



CITY OF
FOLSOM

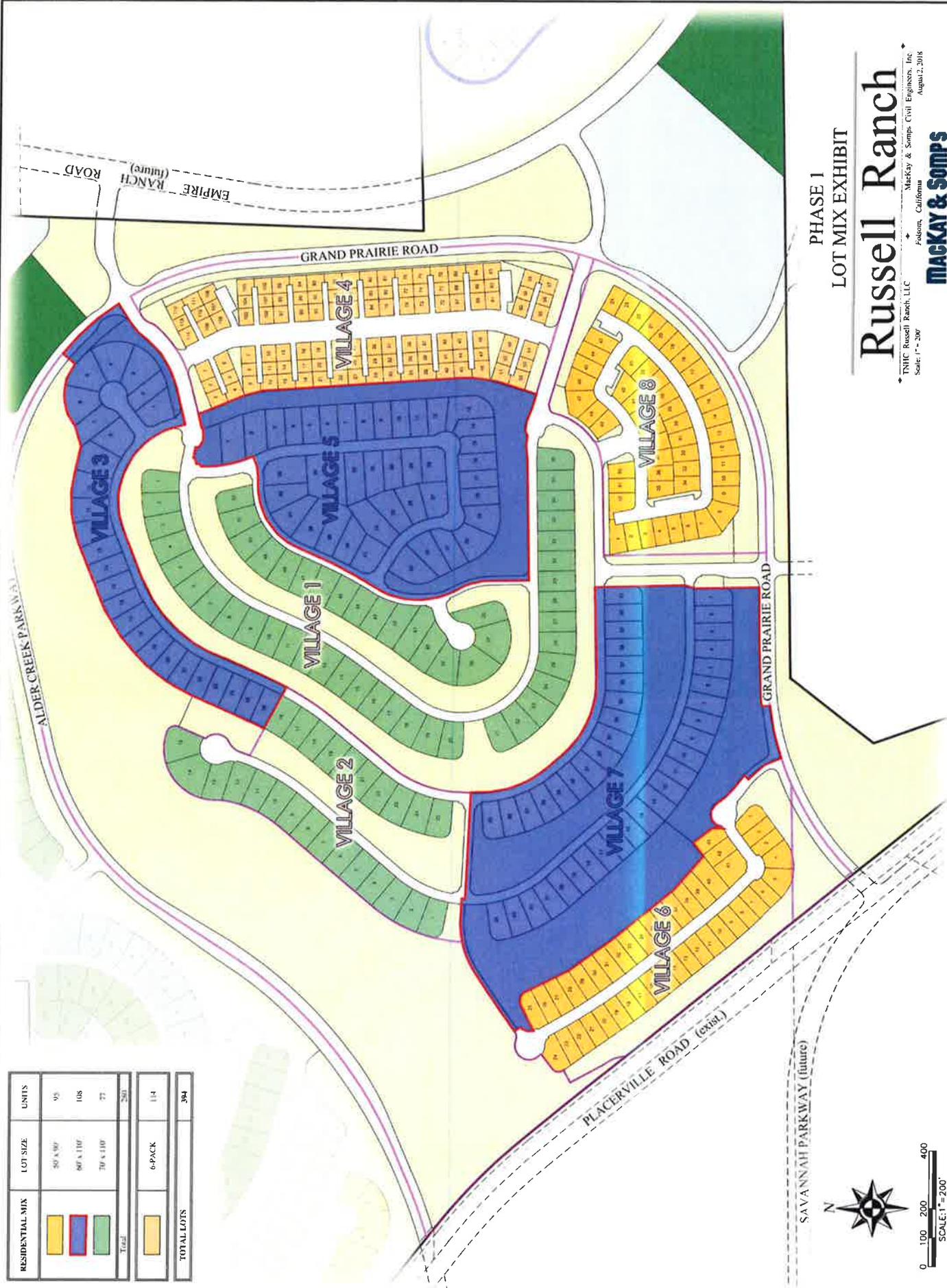
Vicinity Map



Attachment 2

Russell Ranch Phase 1 Lot Mix Exhibit and Site Plan

RESIDENTIAL MIX	LOT SIZE	UNITS
	30' x 90'	95
	60' x 110'	108
	70' x 110'	77
Total		280
	6-PACK	14
TOTAL LOTS		294



PHASE 1
LOT MIX EXHIBIT

Russell Ranch

TNHC Russell Ranch, LLC
Mackay & Samps Civil Engineers, Inc.
Folsom, California
August 2, 2018
Scale: 1" = 200'

MACKAY & SAMPS
ENGINEERS PLANNERS SURVEYORS



Attachment 3

Typical Lot Layout, dated September, 2018

Attachment 4
Color Street Scene

RUSSELL RANCH 60 X 110

Folsom, California



PLAN THREE | CALIFORNIA WINE COUNTRY

PLAN FOUR | CALIFORNIA PRAIRIE

PLAN ONE | CALIFORNIA WINE COUNTRY

PLAN TWO | CALIFORNIA VILLA

SHEET INDEX

- L1 SITE PLAN
- L2 PRELIMINARY LANDSCAPE PLAN
- A1 CALIFORNIA VILLA STYLE SHEET
- A2 CALIFORNIA WINE COUNTRY STYLE SHEET
- A3 CALIFORNIA PRAIRIE STYLE SHEET
- A4 PLAN 1 ELEVATIONS
- A5 PLAN 1 "A" ELEVATIONS
- A6 PLAN 1 "B" ELEVATIONS
- A7 PLAN 1 "C" ELEVATIONS
- A8 FLOOR PLAN 1
- A9 SLAB PLAN 1
- A10 PLAN 2 ELEVATIONS
- A11 PLAN 2 "A" ELEVATIONS
- A12 PLAN 2 "B" ELEVATIONS
- A13 PLAN 2 "C" ELEVATIONS

- A14 FLOOR PLAN 2
- A15 SLAB PLAN 2
- A16 PLAN 3 ELEVATIONS
- A17 PLAN 3 "A" ELEVATIONS
- A18 PLAN 3 "B" ELEVATIONS
- A19 PLAN 3 "C" ELEVATIONS
- A20 FLOOR PLAN 3
- A21 SLAB PLAN 3
- A22 PLAN 4 ELEVATIONS
- A23 PLAN 4 "A" ELEVATIONS
- A24 PLAN 4 "B" ELEVATIONS
- A25 PLAN 4 "C" ELEVATIONS
- A26 FLOOR PLAN 4
- A27 SLAB PLAN 4

PROPOSED GENERAL STATISTICS

CONSTRUCTION TYPE: 1-STORY AND 2-STORY DETACHED SINGLE FAMILY HOMES
 HOME COUNT: 108 SINGLE FAMILY HOMES
 PLAN 1: 3BR + OPT 4 @ STUDY 3.5 BA 2626 S.F.
 PLAN 2: 4BR + OPT 5 @ DINING 3.5 + OPT 4 BA 2995 S.F.
 PLAN 3: 5BR + TEEN ROOM 4.5BA 3309 S.F.
 PLAN 4: 5BR + BONUS ROOM 4.5BA 3500 S.F.



BUILDER:
 THE NEW HOME COMPANY
 2220 DOUGLAS BLVD., SUITE 240
 ROSEVILLE, CA 95661
 OFFICE: 916-740-3921

ARCHITECT:
 WOODLEY ARCHITECTURAL GROUP
 2943 PULLMAN STREET, SUITE A
 SANTA ANA, CA 92705
 OFFICE: 949-553-8919

Attachment 5

**Building Elevations and Floor Plans,
Dated September 21, 2018**

CALIFORNIA VILLA

Characterized by simple forms and restrained detailing, the California Villa style is defined by symmetry and elegance.

DESIGN ELEMENTS

- TWO-STORY MASSING
- STUCCO EXTERIOR FINISH
- 3' SHAPED CONCRETE TILE
- FORMAL WINDOW ARRANGEMENT
- LOW PITCHED ROOF

ENHANCED DESIGN ELEMENTS

- ARCHED ENTRY
- SHAPED STUCCO EAVE
- ARCHED WINDOWS
- LOUVERED SHUTTERS
- WROUGHT IRON DETAILS

EAGLE CONCRETE ROOFING
COLOR: REFER TO COLOR SCHEDULE



09.21.18

CALIFORNIA VILLA | ARCHITECTURAL STYLE

RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA

THIS DRAWING IS THE PROPERTY OF RUSSELL RANCH AND SHALL NOT BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM. ALL DIMENSIONS AND FINISHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED. © 2018 THE NEW HOME COMPANY, INC.

AI
woodley
architectural
group, inc.

colorado // 751 southport dr suite 5
denver, co 80120 / 303 483 7231
california // 2943 pullman st suite A
san diego, ca 92123 / 949 433 8919

CALIFORNIA WINE COUNTRY

The California Wine Country style is a uniquely Californian interpretation of European details and rustic elements.

DESIGN ELEMENTS

- RECTANGULAR MASSING
- LOW PITCHED GABLED ROOF
- 5' ROOF TILE
- STUCCO EXTERIOR FINISH
- DIVIDED LIGHT WINDOWS

ENHANCED DESIGN ELEMENTS

- IMPERFECT SMOOTH STUCCO
- PLANK SHUTTERS
- OVERGROUTED STONE
- EXPOSED RAFTER TAILS

EAGLE CONCRETE ROOFING
COLOR: REFER TO COLOR SCHEDULE



CREATIVE MINES STONE VENEER
PROFILE: CRAFT ORCHARD LIMESTONE
COLOR: REFER TO COLOR SCHEDULE

09.21.18

CALIFORNIA WINE COUNTRY | ARCHITECTURAL STYLE

RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY

FOLSOM, CALIFORNIA

NOTE: SQUARE FOOTAGE MAY VARY BASED ON EXCEL-A-COUNT METHODOLOGY.



colorado // 731 southport dr suite 8
littleton, co 80120 / 303.603.7231
california // 2945 pullman st suite A
san joe, ca 92070 / 949.333.8919

CALIFORNIA PRAIRIE

The Modern Prairie is a distinctly American style defined by simple, structured massing and horizontal lines. It is recognized by its low hipped roofs and generous roof overhangs.

DESIGN ELEMENTS

- TWO-STORY HORIZONTAL MASSING
- LOW-PITCHED ROOF WITH 36" EAVE OVERHANGS
- FLAT CONCRETE ROOF TILE
- STUCCO AND LEDGE STONE BODY
- RECTANGULAR WINDOWS EMPHASIZING THE BUILDING'S GEOMETRY

ENHANCED DESIGN ELEMENTS

- 18" OVERHANG
- CEMENT PLANK LAP SIDING
- LEDGE STONE TO EMPHASIZE THE HORIZONTAL PLANE
- ACCENTED COLORED WINDOW FRAME
- WOOD FASCIA
- STYLE SPECIFIC DIVIDED LIGHTS



09.21.18

CALIFORNIA PRAIRIE | ARCHITECTURAL STYLE

RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA

THIS RENDITION AND SCHEDULE IS SUBJECT TO CHANGE WITHOUT NOTICE. THIS RENDITION AND SCHEDULE IS FOR INFORMATION ONLY AND DOES NOT REPRESENT A CONTRACT. FOR MORE INFORMATION, CONTACT THE ARCHITECTURAL GROUP, INC. AT 925.333.8919.

A3
woodley
architectural
group, inc.

colorado // 703 southpark dr suite 8
littleton, co 80120 / 303.663.7231
california // 2943 pullman st suite A
san joaquin, ca 92205 / 949.333.8919



SCALE 1/4" = 1'-0"

elevationA- CALIFORNIA VILLA



SCALE 1/4" = 1'-0"

elevationB- CALIFORNIA WINE COUNTRY



SCALE 1/4" = 1'-0"

elevationC- CALIFORNIA PRAIRIE

09.21.18

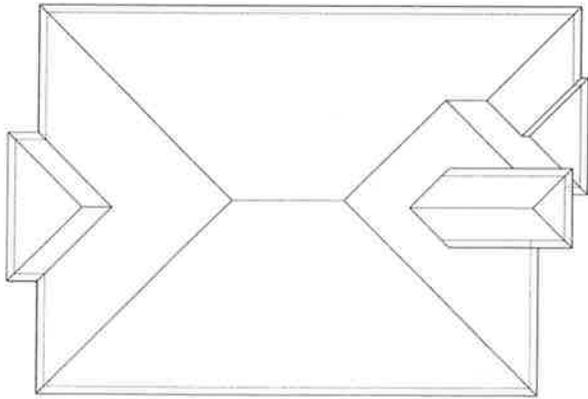
PLAN ONE | FRONT ELEVATIONS

RUSSELL RANCH | 60 X 110 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA



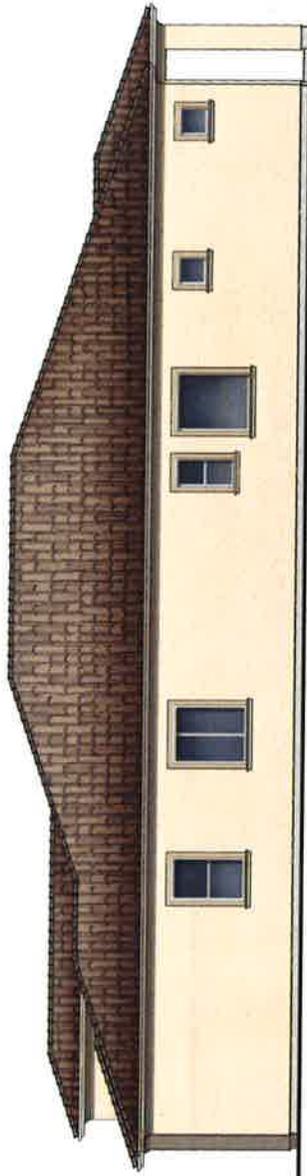
WOODLEY
ARCHITECTURE
GROUP, INC.
17700 Woodley Blvd., Suite 100
Folsom, CA 95630 | 916.438.2200
www.woodleyarch.com | 916.438.2200

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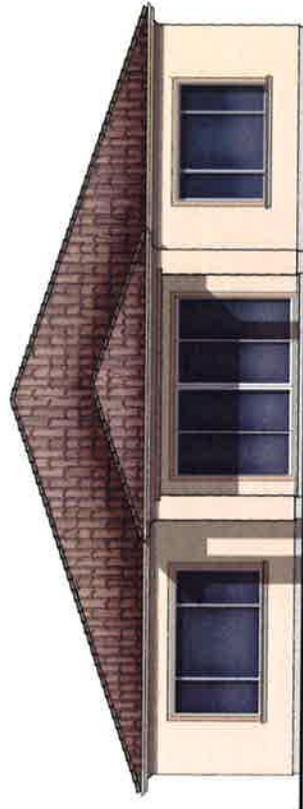
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ROOF PLAN



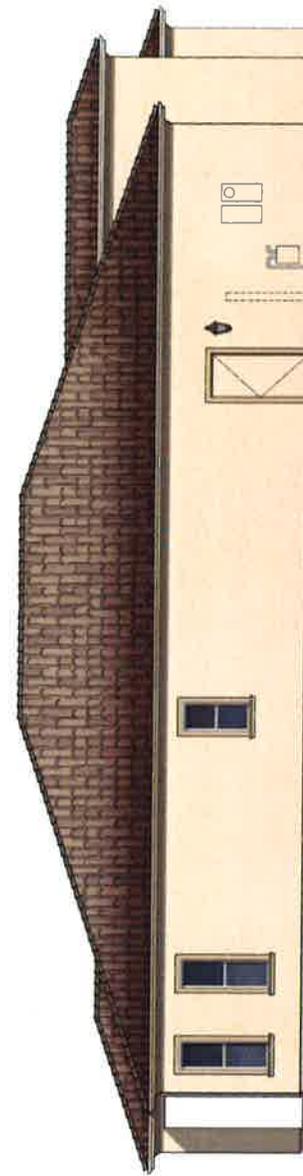
RIGHT ELEVATION

SCALE: 1/8" = 1'-0"



REAR ELEVATION

SCALE: 1/8" = 1'-0"



LEFT ELEVATION

SCALE: 1/8" = 1'-0"

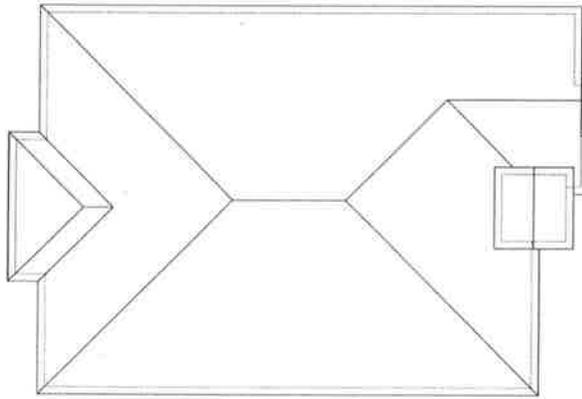
09.21.18

PLAN ONE | ELEVATION 'A' | CALIFORNIA VILLA
RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA

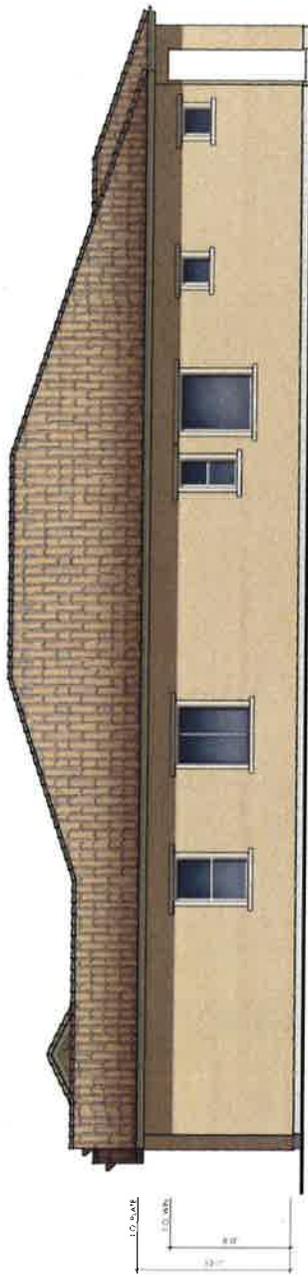
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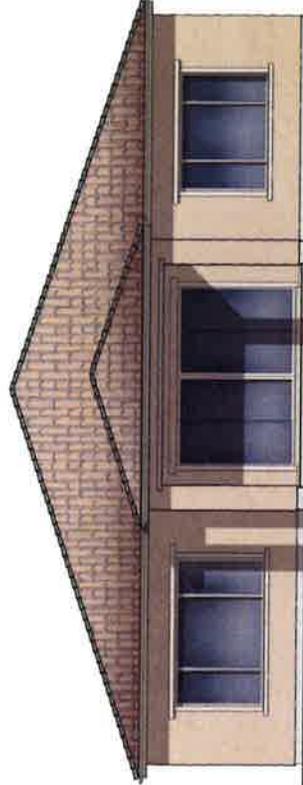
AS
woodley
architectural
group, inc.
 colorado // 731 southpark circle suite 3
 denver, co 80120 // 303.683.7211
 california // 10000 woodley way suite 100
 san diego, ca 92120 // 619.503.8914



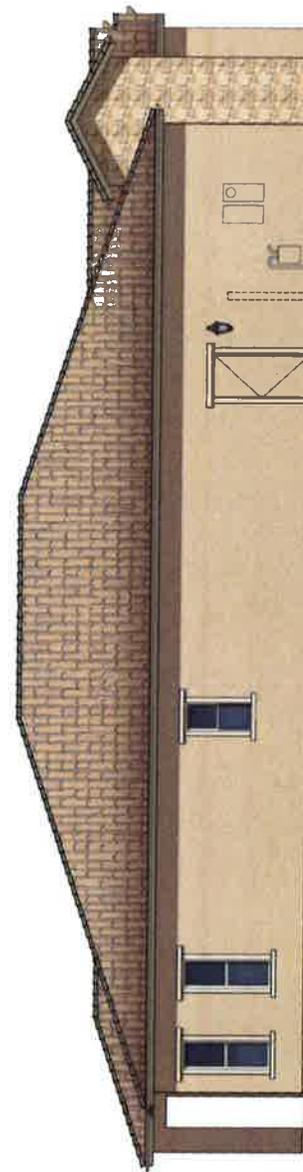
ROOF PLAN SCALE: 1/8" = 1'-0"



RIGHT ELEVATION SCALE: 1/8" = 1'-0"



REAR ELEVATION SCALE: 1/8" = 1'-0"



LEFT ELEVATION SCALE: 1/8" = 1'-0"

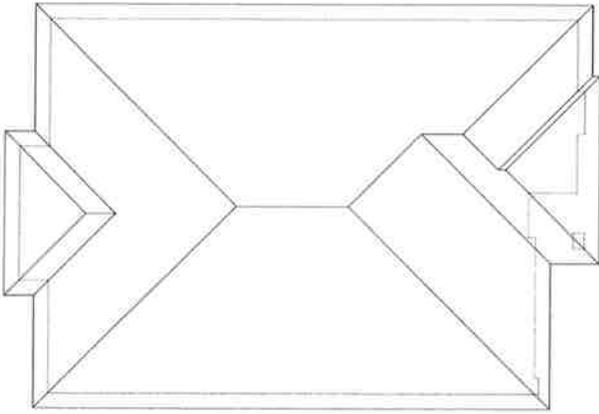
09.21.18

PLAN ONE | ELEVATION 'B' | CALIFORNIA WINE COUNTRY
RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA



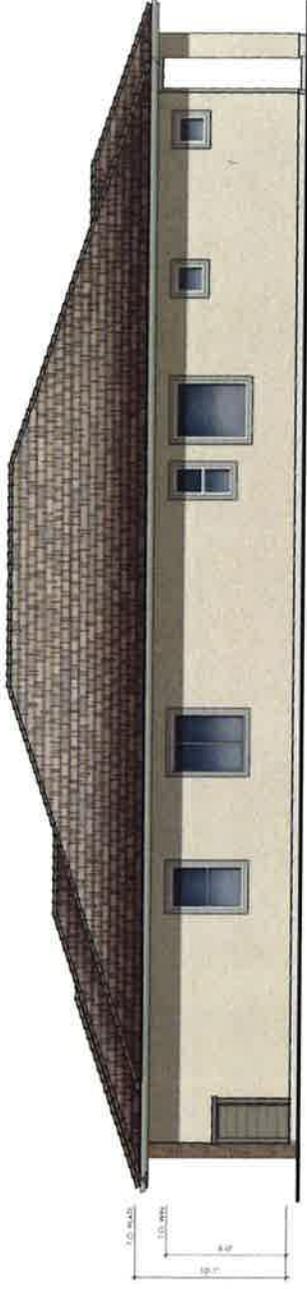
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 architectural
 group, inc.
 colorado // 731 southport ct, suite 8
 littleton, co 80120 / 303.883.7231
 colorado // 2744 north montross ave.
 denver, co 80207 / 948.363.8719

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ROOF PLAN

SCALE 1/8" = 1'-0"



RIGHT ELEVATION

SCALE 1/8" = 1'-0"



1'-0" MAX

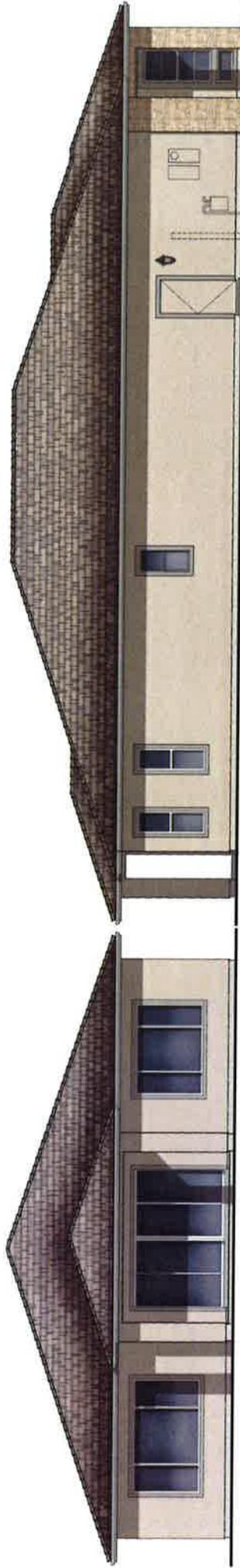
1'-0" MAX

8'-0"

10'-0"

REAR ELEVATION

SCALE 1/8" = 1'-0"



LEFT ELEVATION

SCALE 1/8" = 1'-0"

09.21.18

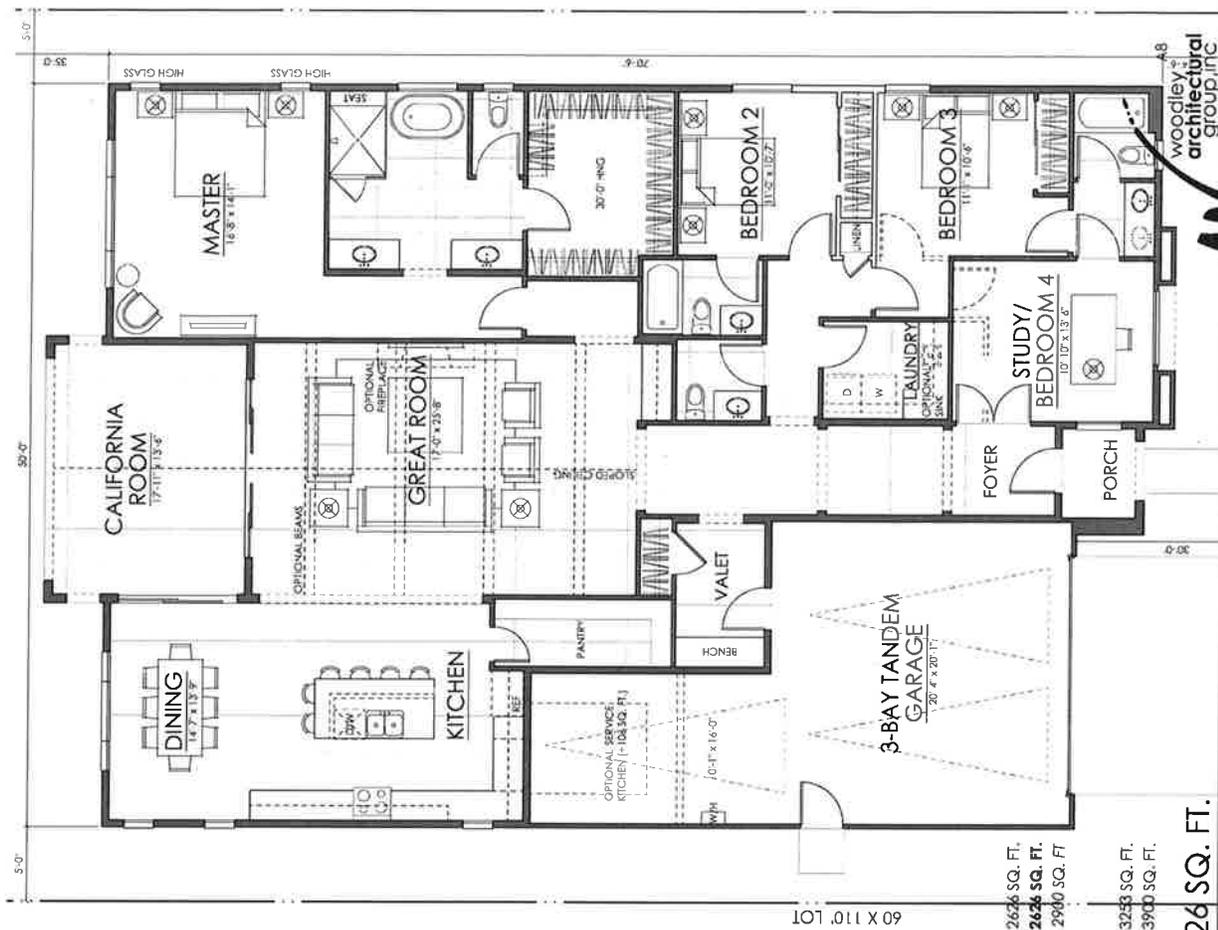
PLAN ONE | ELEVATION 'C' | CALIFORNIA PRAIRIE
RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA



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 architectural
 group, inc.

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FIRST FLOOR 2626 SQ. FT.
 TOTAL LIVING 2626 SQ. FT.
 TARGET 2900 SQ. FT.

LOT COVERAGE 3233 SQ. FT.
 MAX COVERAGE 3500 SQ. FT.

PLAN ONE | 2626 SQ. FT.

**RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA**

09.21.18



colorado 17731 soulinport dr. suite 8
 colorado springs, co 80904
 california 17845 so. hwy 99
 san joe, ca 92705 / 949 553 8979

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SCALE: 1/4" = 1'-0"

elevationA- CALIFORNIA VILLA



SCALE: 1/4" = 1'-0"

elevationB- CALIFORNIA WINE COUNTRY



SCALE: 1/4" = 1'-0"

elevationC- CALIFORNIA PRAIRIE

09.21.18

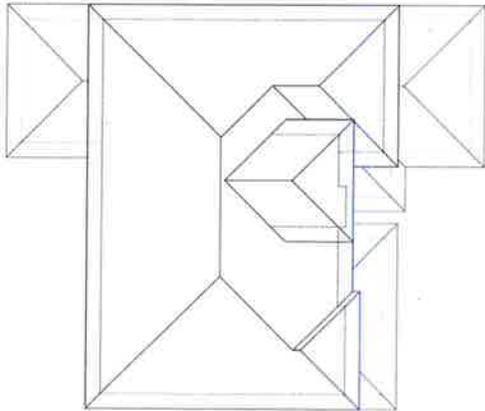
PLAN TWO | FRONT ELEVATIONS

RUSSELL RANCH | 60 X 110 | THE NEW HOME COMPANY
FOLOSOM, CALIFORNIA

A10

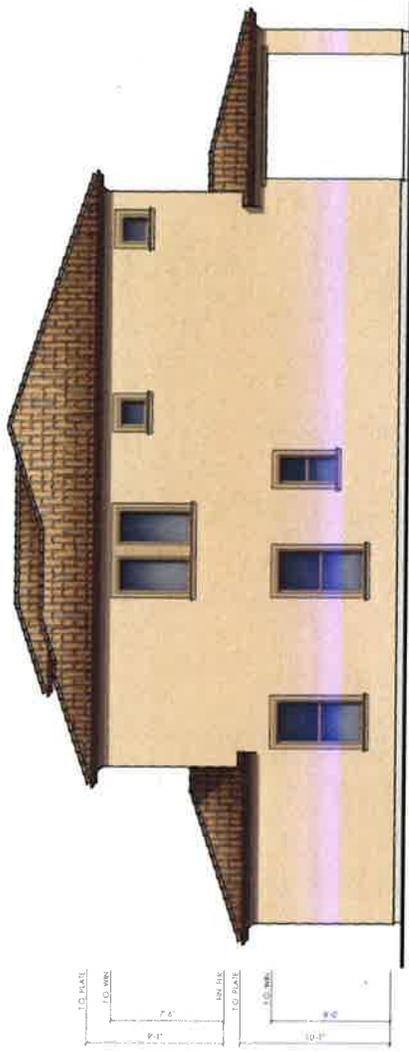
woodley
 architectural
 group inc

WOODLEY ARCHITECTURAL GROUP INC.
 10000 WOODLEY AVENUE
 SUITE 100
 FOLSOM, CA 95630-2322
 TEL: 916.451.1000
 WWW.WOODLEYARCHITECTURALGROUP.COM



ROOF PLAN

SCALE: 1/8" = 1'-0"



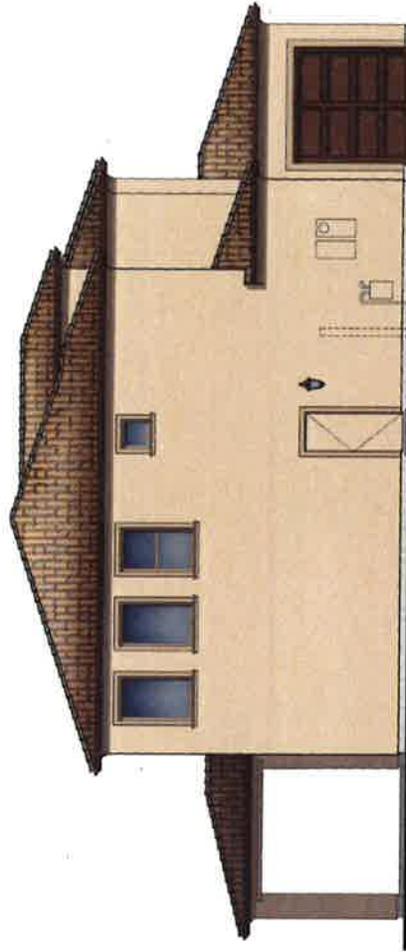
RIGHT ELEVATION

SCALE: 1/4" = 1'-0"



REAR ELEVATION

SCALE: 1/4" = 1'-0"



LEFT ELEVATION

SCALE: 1/4" = 1'-0"

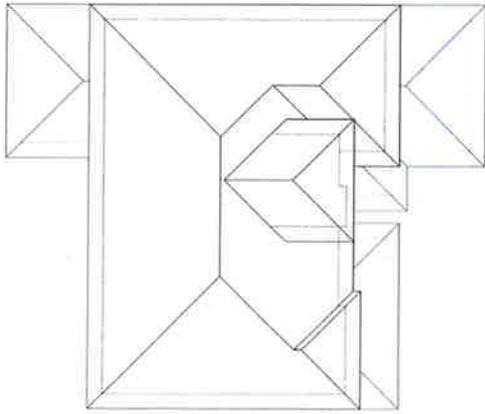
09.21.18

PLAN TWO | ELEVATION 'A' | CALIFORNIA VILLA
RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA

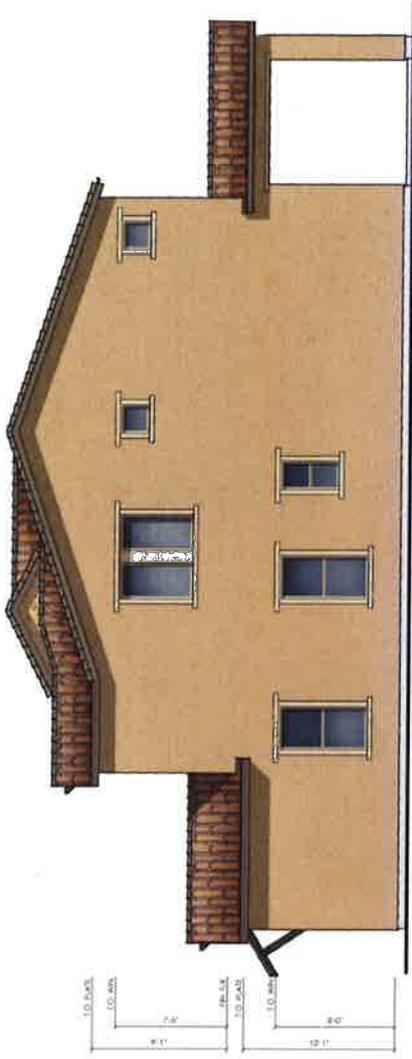


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 ALL
 colorado // 731 southpark dr. suite 8
 denver, co 80120 / 303.883.7231
 colorado // 10000 e. 1st ave. suite 100
 denver, co 80231 / 303.883.7231
 colorado // 9225 s. 7th ave. suite 100
 south denver, co 80232 / 720.555.8719

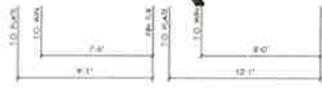
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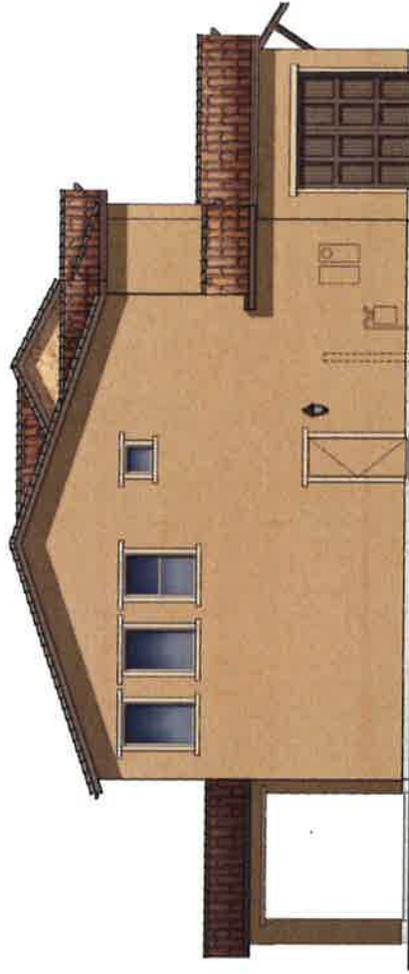
ROOF PLAN SCALE: 1/8" = 1'-0"



RIGHT ELEVATION SCALE: 1/8" = 1'-0"



REAR ELEVATION SCALE: 1/8" = 1'-0"



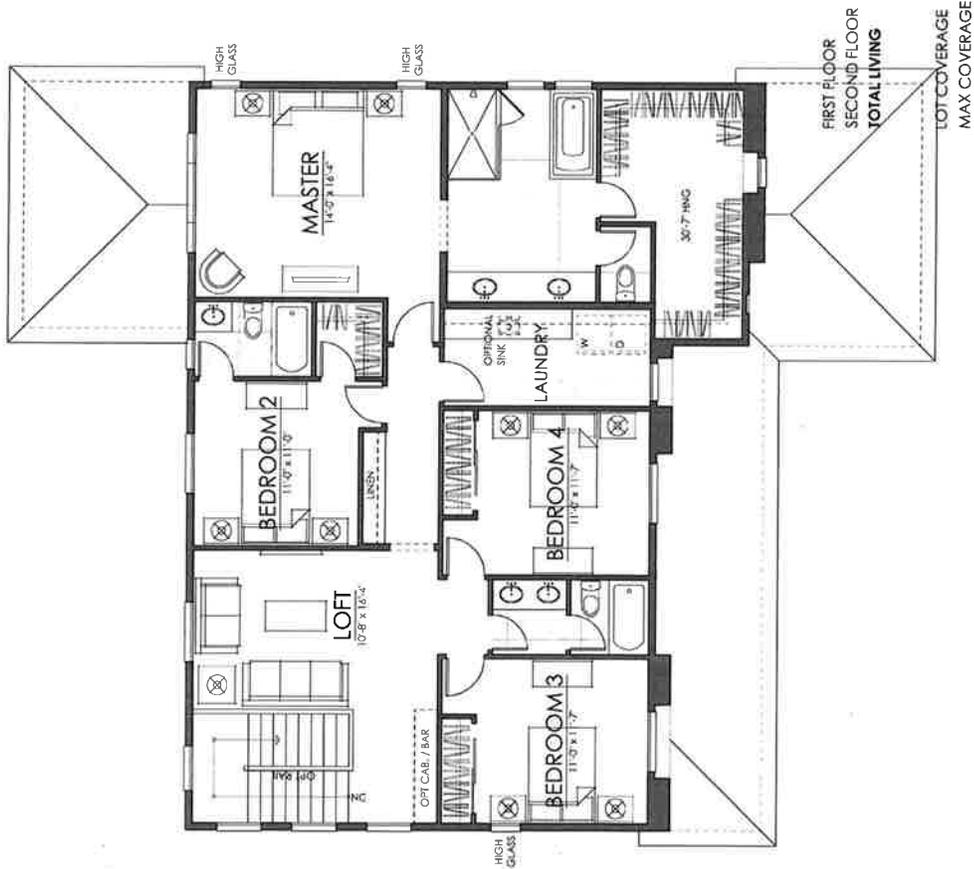
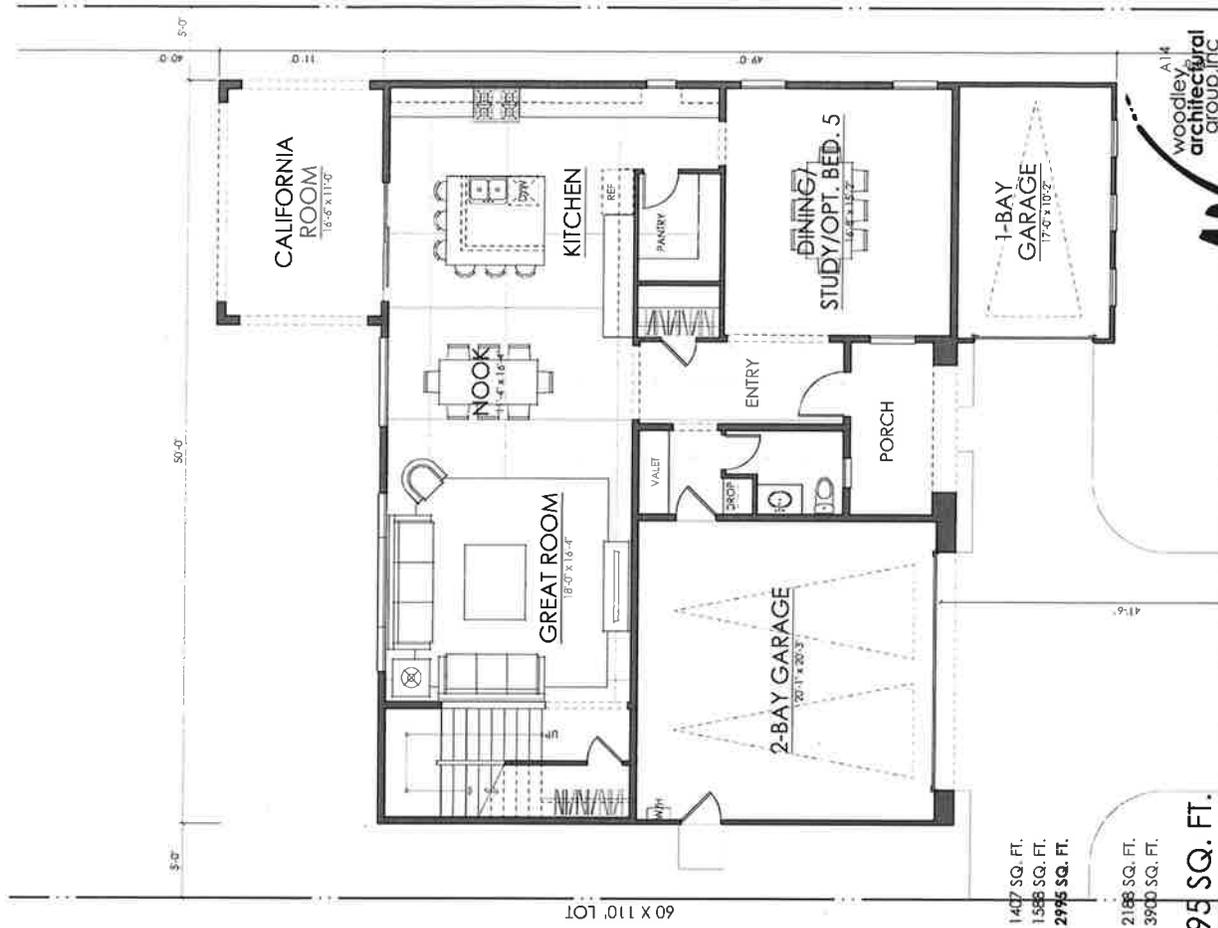
LEFT ELEVATION SCALE: 1/8" = 1'-0"

PLAN TWO | ELEVATION 'B' | CALIFORNIA WINE COUNTRY
RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA

09.21.18



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PLAN TWO | 2995 SQ. FT.

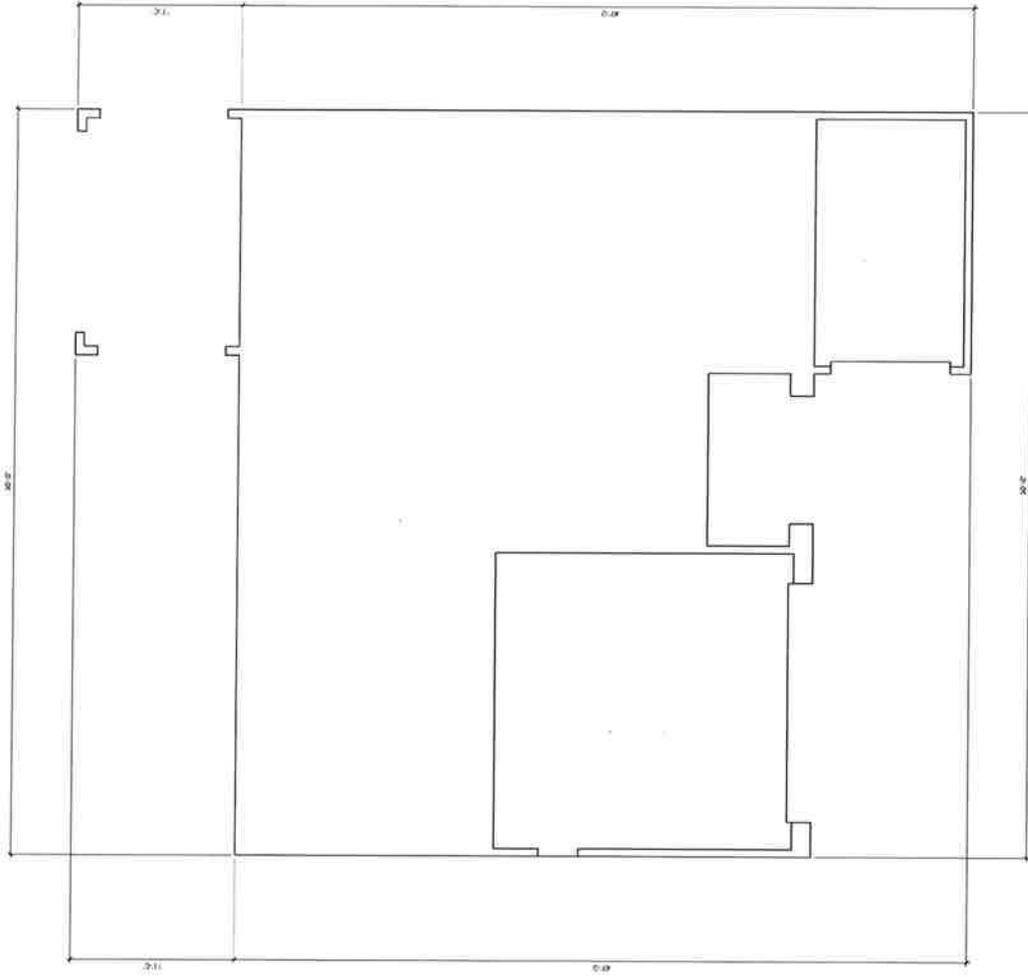
RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY

FOLSOM, CALIFORNIA

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WOODLEYARCH.COM
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PHOTO: JAMES W. HARRIS / JAMES W. HARRIS PHOTOGRAPHY



SLAB EDGE PLAN 'A'
1/4" = 1'-0"

CALIFORNIA VLA

09.21.18

PLAN TWO

**RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
FOLSOM, CALIFORNIA**

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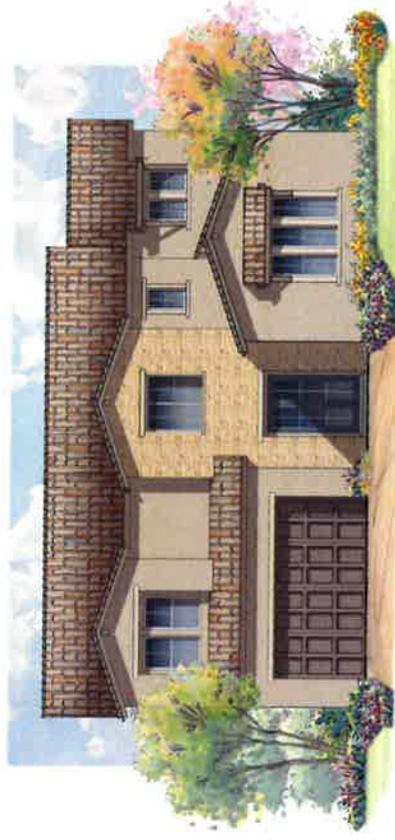
A15
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architectural
group, inc.

17701 San Ignacio Ct., Suite B
Folsom, CA 95630
California | 925.553.8919
San Carlos, CA 92005 | 949.553.8919



SCALE 1/4" = 1'-0"

elevationA- CALIFORNIA VILLA



SCALE 1/4" = 1'-0"

elevationB- CALIFORNIA WINE COUNTRY



SCALE 1/4" = 1'-0"

elevationC- CALIFORNIA PRAIRIE

09.21.18

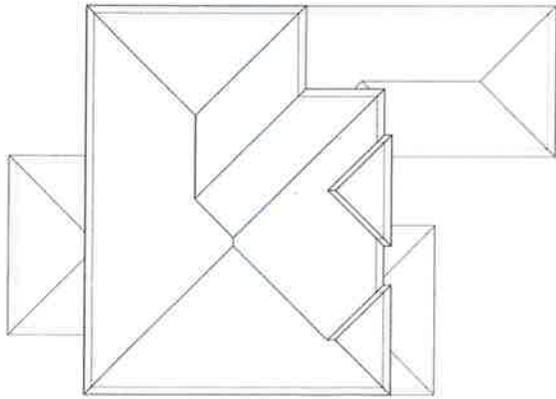
PLAN THREE | FRONT ELEVATIONS

RUSSELL RANCH | 60 x 110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA

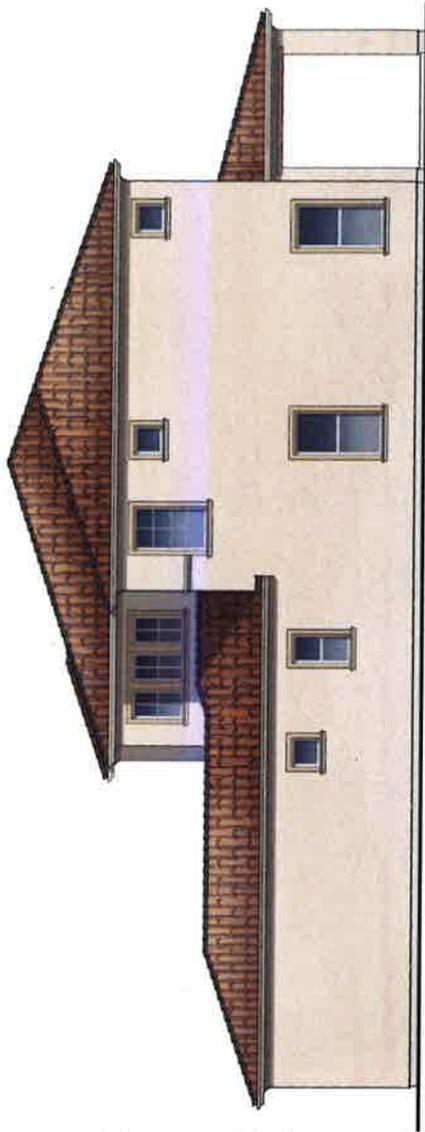
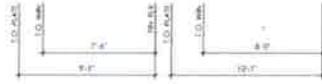
A16

woodley
 architectural
 GROUP, INC.

WOODLEY ARCHITECTURAL GROUP, INC.
 10000 WOODLEY AVENUE, SUITE 111
 FOLSOM, CA 95630-2000
 (916) 439-7200
 WWW.WOODLEYARCHITECTURALGROUP.COM



ROOF PLAN SCALE 1/8" = 1'-0"



RIGHT ELEVATION SCALE 1/8" = 1'-0"



REAR ELEVATION SCALE 1/8" = 1'-0"



LEFT ELEVATION SCALE 1/8" = 1'-0"

09.21.18

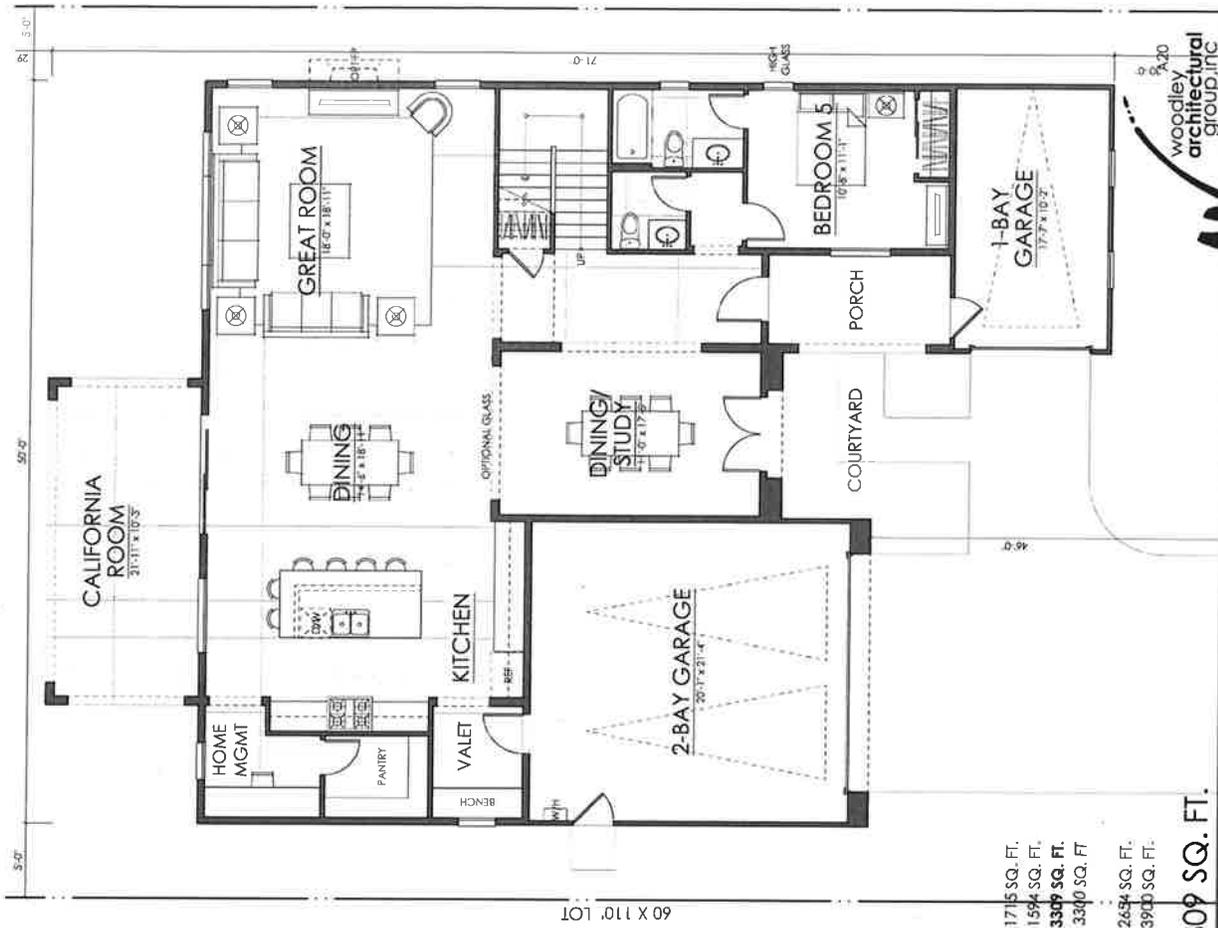
PLAN THREE | ELEVATION 'A' | CALIFORNIA VILLA
RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA



A17
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 architectural
 group, inc.

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 colorado // 3810 n. 1st ave, suite 100
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1715 SQ. FT.
 1594 SQ. FT.
3309 SQ. FT.
 3340 SQ. FT.

2634 SQ. FT.
 3900 SQ. FT.

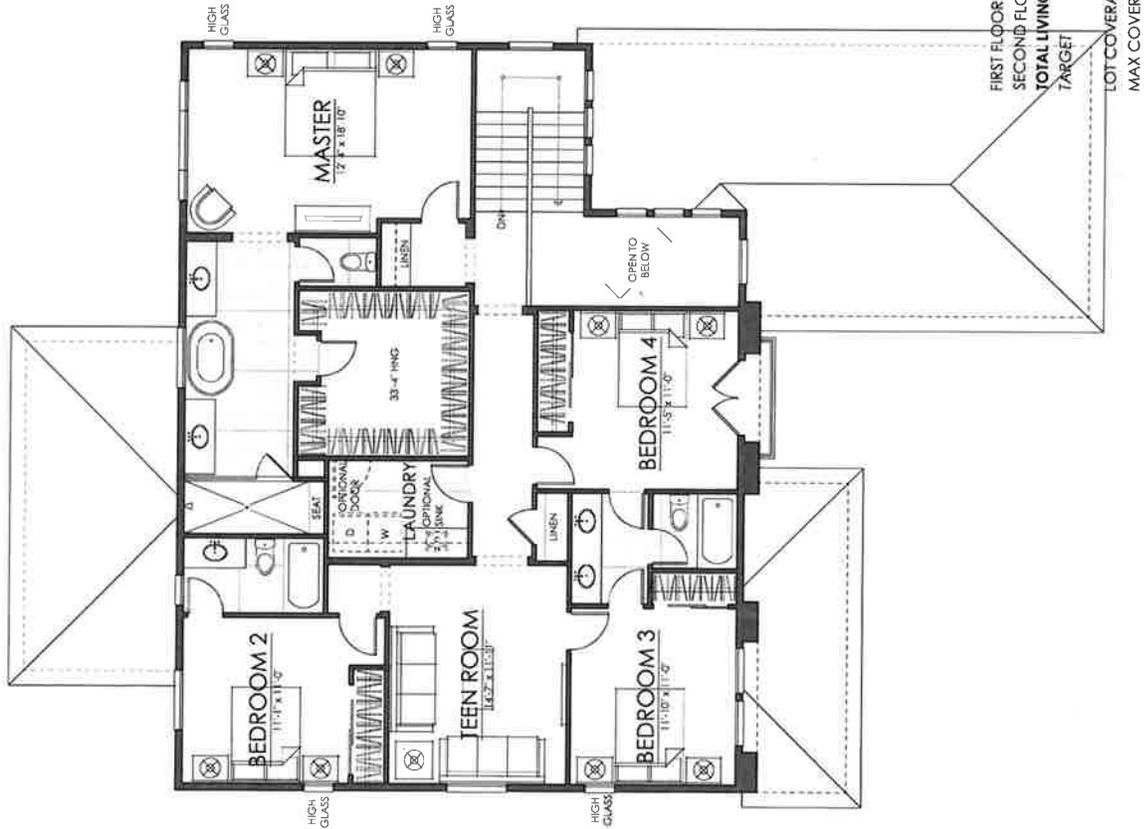
PLAN THREE | 3309 SQ. FT.

RUSSELL RANCH | 60X110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA

09.21.18



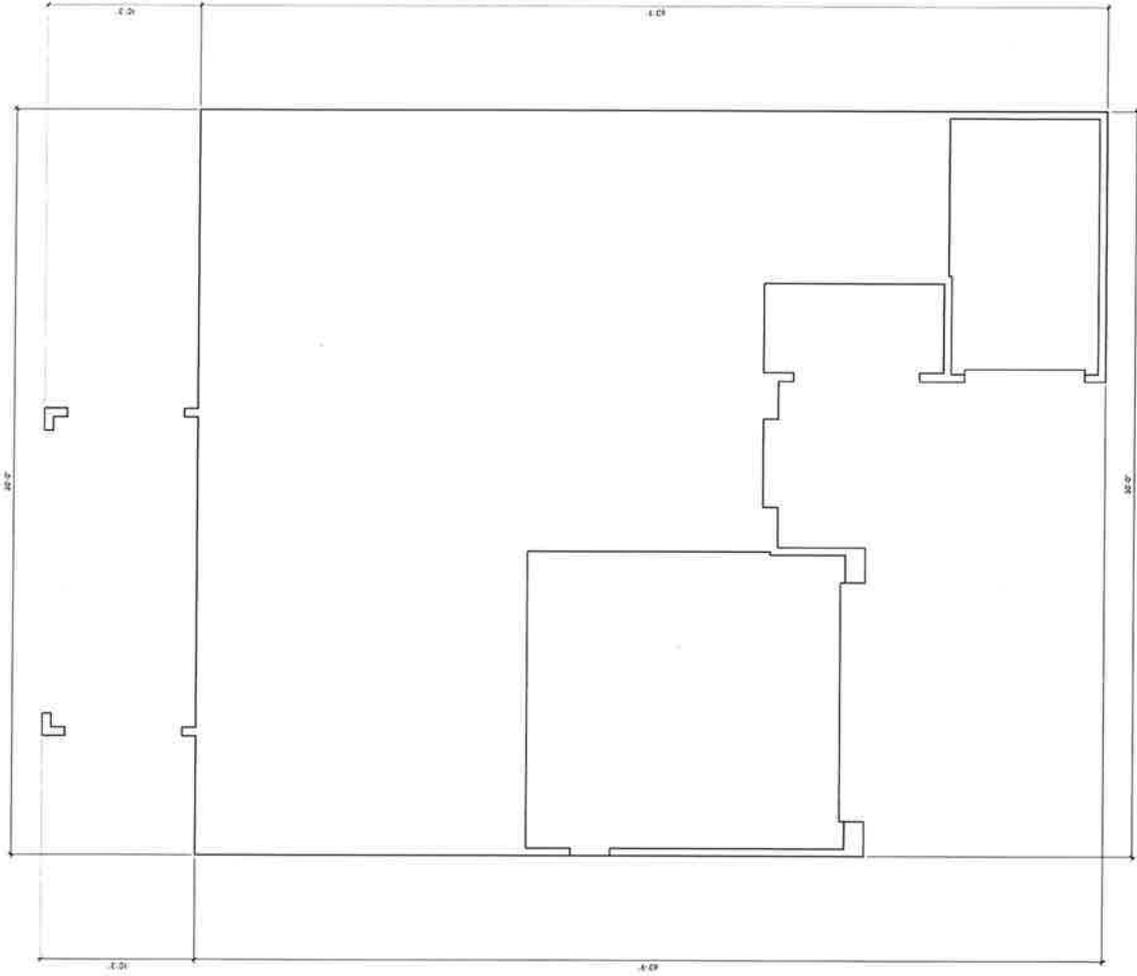
california // 731 southpark dr. suite 8
 folsom, ca 95630
 california // 724 a
 santa ana, ca 92703 // 949 333 8719



FIRST FLOOR
 SECOND FLOOR
TOTAL LIVING
 TARGET

LOT COVERAGE
 MAX COVERAGE

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SLAB EDGE PLAN 'A'
1/8" = 1'-0"

CALIFORNIA VILLA

PLAN THREE

RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY

FOLSOM, CALIFORNIA

NOTE: SQUARE FOOTAGE MAY VARY BASED ON CONSTRUCTION METHODS.

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denver, co 80120 // 303.683.7231
california // 2943 pullman st. suite A
san joaquin, ca 95705 // 916.553.8919



SCALE: 1/4" = 1'-0"

elevationA- CALIFORNIA VILLA



SCALE: 1/4" = 1'-0"

elevationB- CALIFORNIA WINE COUNTRY



SCALE: 1/4" = 1'-0"

elevationC- CALIFORNIA PRAIRIE

09.21.18

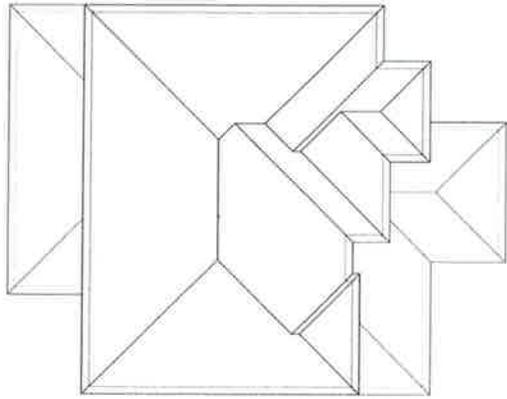
PLAN FOUR | FRONT ELEVATIONS

RUSSELL RANCH | 60 x 110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA

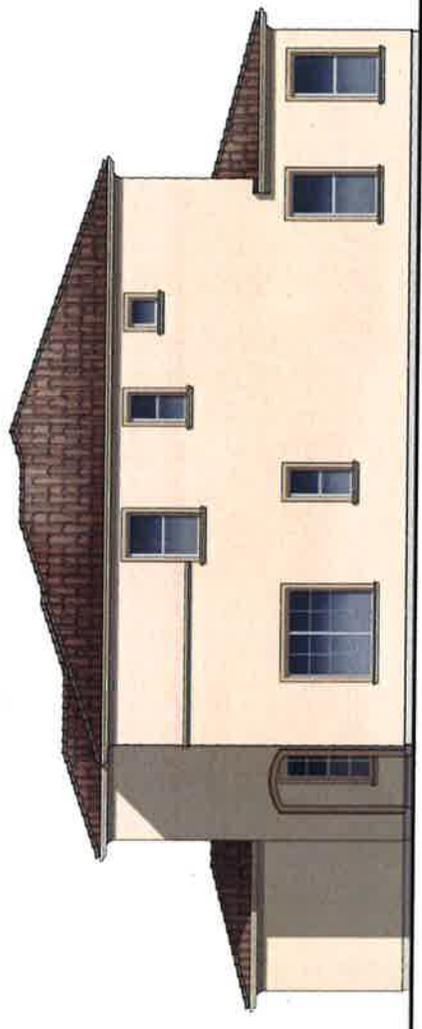
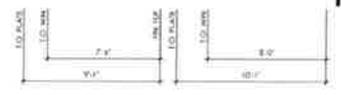
A22

woodley architectural group, inc.

WOODLEY ARCHITECTURAL GROUP, INC.
 2174 JEFFERSON BLVD., SUITE 3
 FOLSOM, CA 95630-7231
 TEL: 916.756.7400 FAX: 916.756.7401



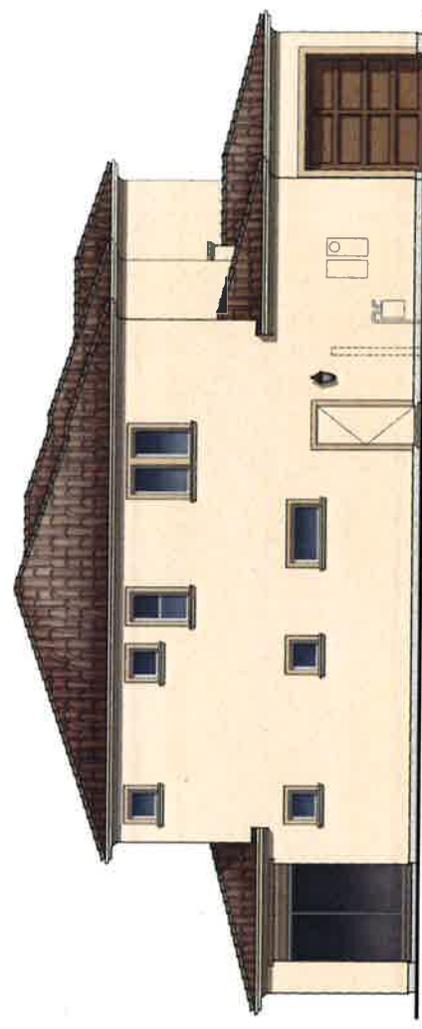
ROOF PLAN SCALE: 1/8" = 1'-0"



RIGHT ELEVATION SCALE: 1/8" = 1'-0"



REAR ELEVATION SCALE: 1/8" = 1'-0"



LEFT ELEVATION SCALE: 1/8" = 1'-0"

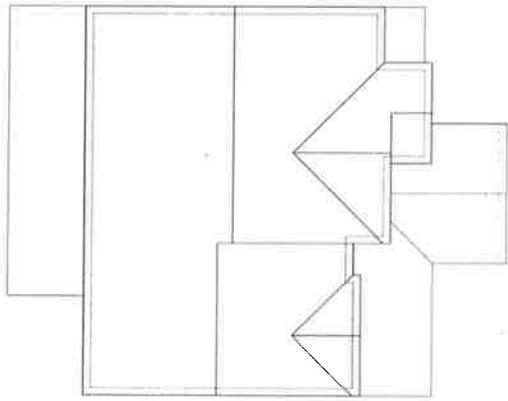
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PLAN FOUR | ELEVATION 'A' | CALIFORNIA VILLA
RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA



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 denver, co 80202 / 303.683.7231
 arizona // 2000 n. 24th ave, suite 100
 phoenix, az 85016 / 602.998.4100
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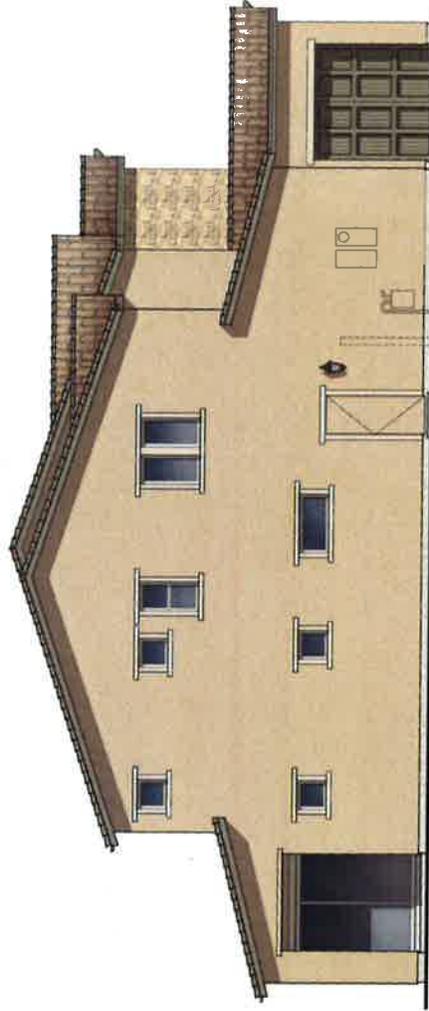
ROOF PLAN
SCALE: 1/8" = 1'-0"



RIGHT ELEVATION
SCALE: 1/4" = 1'-0"



REAR ELEVATION
SCALE: 1/4" = 1'-0"



LEFT ELEVATION
SCALE: 1/4" = 1'-0"

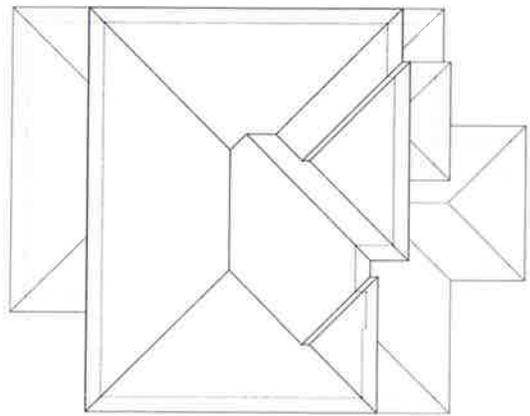
09.21.18

PLAN FOUR | ELEVATION 'B' | CALIFORNIA WINE COUNTRY
RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA

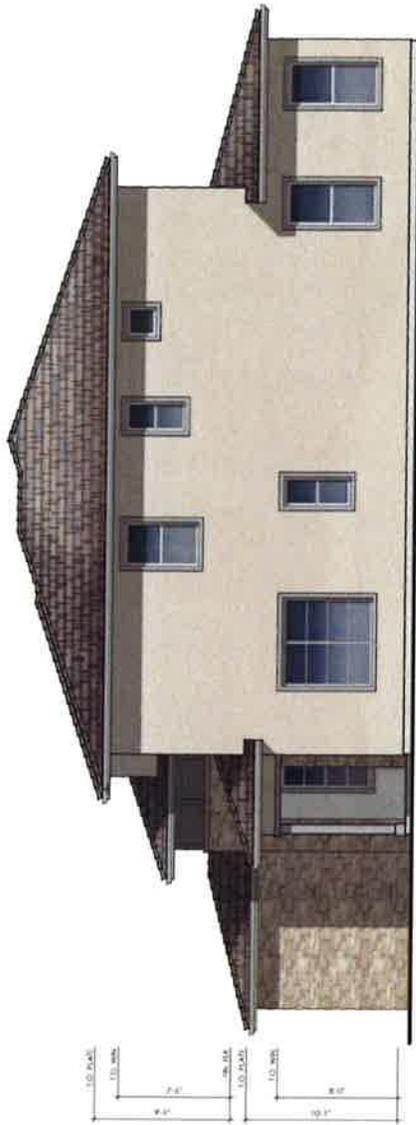


colorado // 731 southport ct suite 5
 littleton, co 80120 / 303.683.7231
 california // 274 n palmers rd suite a
 santa ana, ca 92705 / 714.368.8719

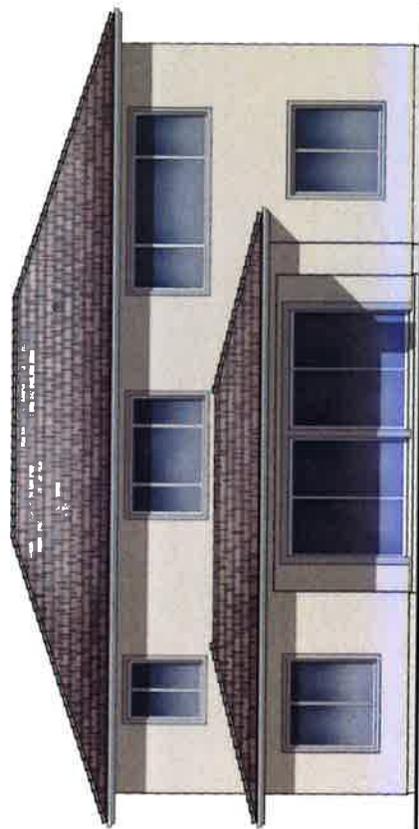
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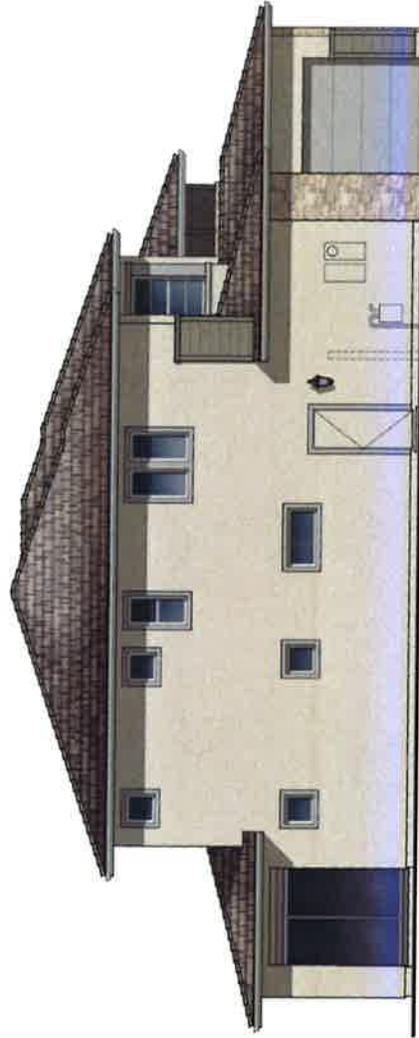
ROOF PLAN SCALE: 1/8" = 1'-0"



RIGHT ELEVATION SCALE: 1/8" = 1'-0"



REAR ELEVATION SCALE: 1/8" = 1'-0"



LEFT ELEVATION SCALE: 1/8" = 1'-0"

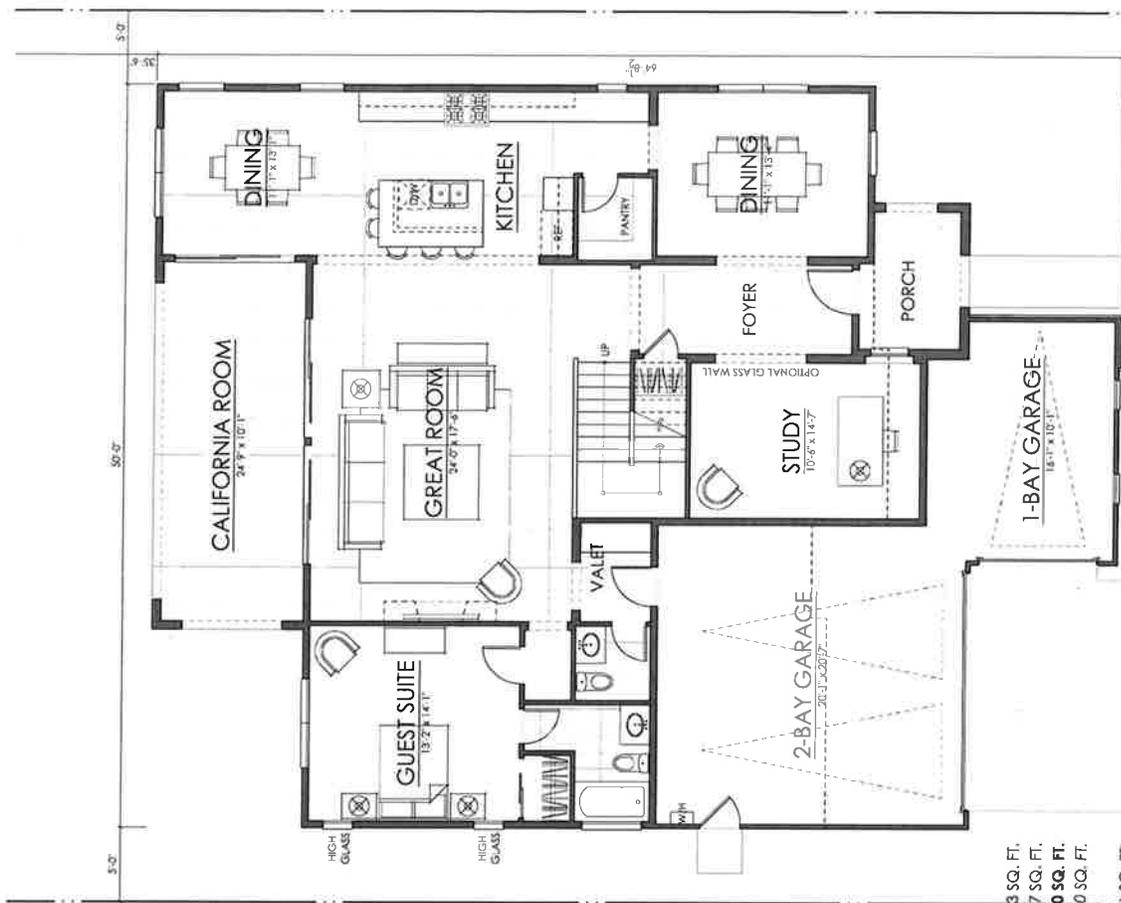
09.21.18

PLAN FOUR | ELEVATION 'C' | CALIFORNIA PRAIRIE
RUSSELL RANCH | 60x110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA



WOODLEY ARCHITECTURAL GROUP, INC. A25

colorado // 731 northpark at suite 8
 littleton, CO 80120 // 303.583.7231
 colorado // 10000 north
 juniper street
 denver, CO 80202 // 720.533.8879



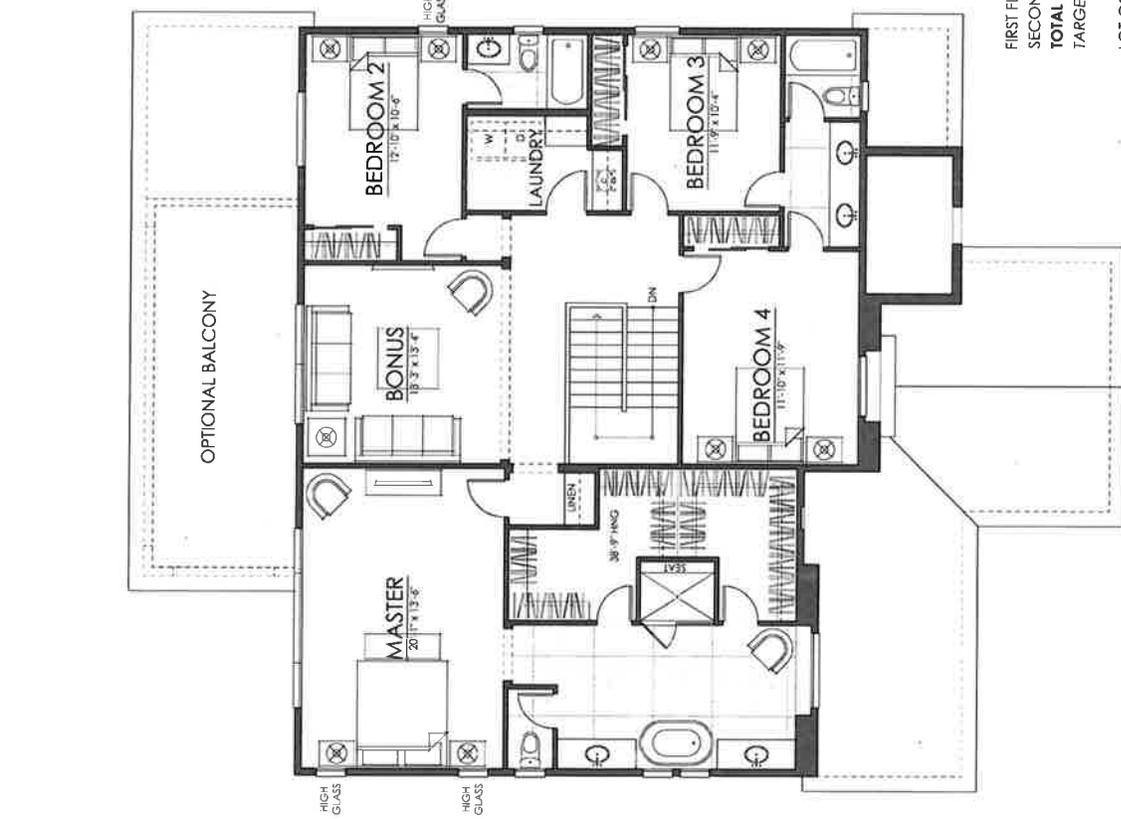
FIRST FLOOR 1749 SQ. FT.
 SECOND FLOOR 1737 SQ. FT.
 TOTAL LIVING 3500 SQ. FT.
 TARGET 3500 SQ. FT.

LOT COVERAGE 2501 SQ. FT.
 MAX COVERAGE 3900 SQ. FT.

PLAN FOUR | 3500 SQ. FT.

**RUSSELL RANCH | 60X110 | THE NEW HOME COMPANY
 FOLSOM, CALIFORNIA**

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California Villas (A) Elevations						
Color Scheme	Sherwin Williams Paint Company					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Raised Foam Color Flat Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
CV1	TBD	SW 7012 Creamy	SW 7532 Juban Putty	SW 6082 Cobble Brown	SW 6013 Bitter Chocolate	3684 San Rafael Blend
CV2	TBD	SW 9166 Drift of Mist	SW 7533 Relaxed Khaki	SW 2808 Rookwood Dark Brown	SW 9142 Moscow Midnight	SCC 8830 Albuquerque Blend
CV3	TBD	SW 7565 Oyster Bar	SW 7548 Porfico	SW 6076 Turkish Coffee	SW 6188 Shade-Grown	SCC 8806 Tucson Blend

California Wine Country (B) Elevations						
Color Scheme	Sherwin Williams Paint Company					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Raised Foam Color Flat Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
WC1	TBD	SW 9173 Shrike	SW 9166 Drift of Mist	SW 6200 Link Gray	SW 7621 Silvermist	TBD
WC2	TBD	SW 7051 Analytical Gray	SW 7541 Grecian Ivory	SW 7025 Backdrop	SW 6250 Granite Peak	TBD
WC3	TBD	SW 6108 Latte	SW 6148 Wool Steal	SW 7026 Griffin	SW 7060 Attitude Gray	TBD

California Prairie (C) Elevations						
Color Scheme	Sherwin Williams Paint Company					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Sliding Color Eggshell Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
PR1	TBD	SW 7654 Lattice	SW 7060 Attitude Gray	SW 7652 Mineral Deposit	SW 7510 Chateau Brown	TBD
PR2	TBD	SW 6150 Universal Khaki	SW 7026 Griffin	SW 7053 Adaptive Shade	SW 7062 Rock Bottom	TBD
PR3	TBD	SW 6198 Sensible Hue	SW 7067 Cityscape	SW 9166 Drift of Mist	SW 7083 Darkroom	TBD

General Notes:

- All paint is to be Sherwin Williams Paint Company.
- All metal elements are to be painted Sherwin Williams SW 7020 "Black Fox", Eggshell Finish
- All non-decorative items such as meter doors, non-decorative vents etc., are to be painted the same color and finish as the surrounding field color.
- All non-decorative roof metal is to be painted to match the darkest color from the roofing blend.
- Metal Roofing is to be Custom-Blit Metals. Color: "Pre-Weathered Galvalume"
- Rain gutters and downspouts are TBD

Masonry Notes at B Elevation:

- Mortar is to be Orco Blended Products MAC Mortar. Color: "OBP Soft White"
- Mortar is to be flush with the face of the masonry with the faces remaining clean.

Color Scheme	California Prairie (C) Elevations					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Sliding Color Eggshell Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
PR1	TBD	SW 7654 Lattice	SW 7060 Attitude Gray	SW 7652 Mineral Deposit	SW 7510 Chateau Brown	TBD
PR2	TBD	SW 6150 Universal Khaki	SW 7026 Griffin	SW 7053 Adaptive Shade	SW 7062 Rock Bottom	TBD
PR3	TBD	SW 6198 Sensible Hue	SW 7067 Cityscape	SW 9166 Drift of Mist	SW 7083 Darkroom	TBD

Color Scheme	California Wine Country (B) Elevations					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Raised Foam Color Flat Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
WC1	TBD	SW 9173 Shrike	SW 9166 Drift of Mist	SW 6200 Link Gray	SW 7621 Silvermist	TBD
WC2	TBD	SW 7051 Analytical Gray	SW 7541 Grecian Ivory	SW 7025 Backdrop	SW 6250 Granite Peak	TBD
WC3	TBD	SW 6108 Latte	SW 6148 Wool Steal	SW 7026 Griffin	SW 7060 Attitude Gray	TBD

Color Scheme	California Villas (A) Elevations					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Raised Foam Color Flat Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
CV1	TBD	SW 7012 Creamy	SW 7532 Juban Putty	SW 6082 Cobble Brown	SW 6013 Bitter Chocolate	3684 San Rafael Blend
CV2	TBD	SW 9166 Drift of Mist	SW 7533 Relaxed Khaki	SW 2808 Rookwood Dark Brown	SW 9142 Moscow Midnight	SCC 8830 Albuquerque Blend
CV3	TBD	SW 7565 Oyster Bar	SW 7548 Porfico	SW 6076 Turkish Coffee	SW 6188 Shade-Grown	SCC 8806 Tucson Blend

Color Scheme	California Wine Country (B) Elevations					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Raised Foam Color Flat Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
WC1	TBD	SW 9173 Shrike	SW 9166 Drift of Mist	SW 6200 Link Gray	SW 7621 Silvermist	TBD
WC2	TBD	SW 7051 Analytical Gray	SW 7541 Grecian Ivory	SW 7025 Backdrop	SW 6250 Granite Peak	TBD
WC3	TBD	SW 6108 Latte	SW 6148 Wool Steal	SW 7026 Griffin	SW 7060 Attitude Gray	TBD

Color Scheme	California Prairie (C) Elevations					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Sliding Color Eggshell Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
PR1	TBD	SW 7654 Lattice	SW 7060 Attitude Gray	SW 7652 Mineral Deposit	SW 7510 Chateau Brown	TBD
PR2	TBD	SW 6150 Universal Khaki	SW 7026 Griffin	SW 7053 Adaptive Shade	SW 7062 Rock Bottom	TBD
PR3	TBD	SW 6198 Sensible Hue	SW 7067 Cityscape	SW 9166 Drift of Mist	SW 7083 Darkroom	TBD

Color Scheme	California Wine Country (B) Elevations					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Raised Foam Color Flat Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
WC1	TBD	SW 9173 Shrike	SW 9166 Drift of Mist	SW 6200 Link Gray	SW 7621 Silvermist	TBD
WC2	TBD	SW 7051 Analytical Gray	SW 7541 Grecian Ivory	SW 7025 Backdrop	SW 6250 Granite Peak	TBD
WC3	TBD	SW 6108 Latte	SW 6148 Wool Steal	SW 7026 Griffin	SW 7060 Attitude Gray	TBD

Color Scheme	California Villas (A) Elevations					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Raised Foam Color Flat Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
CV1	TBD	SW 7012 Creamy	SW 7532 Juban Putty	SW 6082 Cobble Brown	SW 6013 Bitter Chocolate	3684 San Rafael Blend
CV2	TBD	SW 9166 Drift of Mist	SW 7533 Relaxed Khaki	SW 2808 Rookwood Dark Brown	SW 9142 Moscow Midnight	SCC 8830 Albuquerque Blend
CV3	TBD	SW 7565 Oyster Bar	SW 7548 Porfico	SW 6076 Turkish Coffee	SW 6188 Shade-Grown	SCC 8806 Tucson Blend

Color Scheme	California Wine Country (B) Elevations					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Raised Foam Color Flat Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
WC1	TBD	SW 9173 Shrike	SW 9166 Drift of Mist	SW 6200 Link Gray	SW 7621 Silvermist	TBD
WC2	TBD	SW 7051 Analytical Gray	SW 7541 Grecian Ivory	SW 7025 Backdrop	SW 6250 Granite Peak	TBD
WC3	TBD	SW 6108 Latte	SW 6148 Wool Steal	SW 7026 Griffin	SW 7060 Attitude Gray	TBD

Color Scheme	California Prairie (C) Elevations					
	Omega Integral Stucco Color 16/20 Sand Finish	Paint Match to Stucco Color Flat Finish	Sliding Color Eggshell Finish	Trim Color Eggshell Finish on wood and metal Flat Finish on stucco	Accent Color Eggshell Finish	Andersen Windows 100 Series
PR1	TBD	SW 7654 Lattice	SW 7060 Attitude Gray	SW 7652 Mineral Deposit	SW 7510 Chateau Brown	TBD
PR2	TBD	SW 6150 Universal Khaki	SW 7026 Griffin	SW 7053 Adaptive Shade	SW 7062 Rock Bottom	TBD
PR3	TBD	SW 6198 Sensible Hue	SW 7067 Cityscape	SW 9166 Drift of Mist	SW 7083 Darkroom	TBD

Attachment 6

Color and Materials Palette, dated March 16, 2018

California Villas (A) Elevations						
Color Scheme	Sherwin Williams Paint Company			Andersen Windows	Eagle S-Tile Profile Concrete Roofing	Eagle Concrete Roofing
	Paint Match to Stucco Color	Raised Foam Color	Trim Color			
CV1	SW 7012 Creamy	SW 7532 Urban Putty	SW 6082 Cobble Brown	SW 6013 Bitter Chocolate	TBD	3684 San Rafael Blend
CV2	SW 9166 Drift of Mist	SW 7533 Relaxed Khaki	SW 2808 Rookwood Dark Brown	SW 9142 Moscow Midnight	TBD	SCC 8830 Albuquerque Blend
CV3	SW 7565 Oyster Bar	SW 7548 Porfico	SW 6076 Turkish Coffee	SW 6188 Shade-Grown	TBD	SCC 8806 Tucson Blend

California Wine Country (B) Elevations						
Color Scheme	Sherwin Williams Paint Company			Creative Mines Stone	Andersen Windows	Eagle S-Tile Profile Concrete Roofing
	Paint Match to Stucco Color	Raised Foam Color	Trim Color			
WC1	SW 9173 Shellake	SW 9166 Drift of Mist	SW 6200 Link Gray	SW 7621 Silvermist	TBD	3816 San Ramon Range
WC2	SW 7051 Analytical Gray	SW 7541 Grecian Ivory	SW 7025 Backdrop	SW 6250 Granite Peak	TBD	3773 Walnut Creek Blend
WC3	SW 6108 Latte	SW 6148 Wool Skein	SW 7026 Griffin	SW 7060 Attitude Gray	TBD	3723 Adobe Blend

California Prairie (C) Elevations						
Color Scheme	Sherwin Williams Paint Company			Creative Mines Stone	Andersen Windows	Eagle Shake Profile Concrete Roofing
	Paint Match to Stucco Color	Sliding Color	Trim Color			
PR1	SW 7654 Lattice	SW 7060 Attitude Gray	SW 7652 Mineral Deposit	SW 7510 Chateau Brown	TBD	5699 Charcoal Range
PR2	SW 6150 Universal Khaki	SW 7026 Griffin	SW 7053 Adaptive Shade	SW 7062 Rock Bottom	TBD	5810 Avondale Blend
PR3	SW 6198 Sensible Hue	SW 7067 Cityscape	SW 9166 Drift of Mist	SW 7083 Darkroom	TBD	SCP 8803 Arlington Blend

General Notes:

- All paint is to be Sherwin Williams Paint Company.
- All metal elements are to be painted Sherwin Williams SW 7020 "Black Fox"; Eggshell Finish
- All non-decorative items such as meter doors, non-decorative vents etc. are to be painted the same color and finish as the surrounding field color.
- All non-decorative roof metal is to be painted to match the darkest color from the roofing blend.
- Metal Roofing is to be Custom-Blit Metals. Color: "Pre-Weathered Galvalume"
- Rain gutters and downspouts are TBD

Masonry Notes at B Elevation:

- Mortar is to be Orco Blended Products MAC Mortar. Color: "OBP Soft White"
- Mortar is to be flush with the face of the masonry with the faces remaining clean.

Attachment 7

Russell Ranch Design Guidelines



RUSSELL RANCH
AND BROADSTONE ESTATES AT RUSSELL RANCH
PLANNED DEVELOPMENT DESIGN GUIDELINES

CHAPTER 4: ARCHITECTURE

“Home is a name, a word, it’s a strong one; stronger than magician ever spoke, or spirit ever answered to, in the strongest conjuration.”

-Charles Dickens

4.1 INTRODUCTION

By employing an authentic architectural palette and creative site planning techniques, Russell Ranch will be a neighborhood with a strong architectural identity and distinctive character within the City of Folsom.

Chapter 4 defines the design principles and development standards that apply to all residential development within Russell Ranch. These guidelines and standards articulate the lot characteristics, setbacks, garage type and orientation, and building massing.

Further, Chapter 4 includes a detailed architectural design guidelines section, which identifies, defines, and articulates the architectural styles appropriate for Russell Ranch.

4.2 DESIGN PRINCIPLES

4.2.1 Diversity of Streetscape

An elegant and diverse streetscape is a defining characteristic of enduring landmark neighborhoods. Simple and elegant planning and design elements can change the essence of a community while maintaining an overall unified theme. The intent of this section is to articulate the standards and unique defining elements by which Russell Ranch shall be built in order to create a cohesive and animated streetscape with a diverse character.

A. MASTER HOME PLAN REQUIREMENTS

To achieve streetscape variation, a master home plan series must comprise master home plans with a minimum of three (3) associated elevations per plan (each elevation must be a different architectural style) per each collection of home offerings, based upon the number of lots to be built upon by one builder as an individual project within the neighborhood. Further, a certain number of architectural styles is required, which will be applied based on appropriate massing (i.e., every floor plan will not have every architectural style applied. Certain home collections will benefit from a greater number of elevations per floor plan and architectural styles, whereas others may benefit from a more cohesive palette (such as the court homes, which provide an opportunity for a unified architectural statement based upon a singular style). This selective architectural style application will enhance the eclectic nature and variety of the streetscape. Master home plans are defined as unique floor plans with a distinct footprint with regard to placement and relationship of garage, front door, and building massing.

B. MASSING AND ROOF FORM

Proportion and placement of architectural forms and elements must be appropriately and authentically applied in a manner consistent with the historical architectural style being represented. Roof articulation in the form of proper roof pitches and forms also plays a significant role in the authenticity and diversity of the streetscape.

- o Massing must be appropriate and authentic to the architectural style (e.g., the Monterey style has a cantilevered second story balcony as a signature defining element; it would be inauthentic to design a single story Monterey home).
- o One out of every three homes must have a significantly different roof form than its neighbors (e.g., forward-facing gable versus side-facing gable).
- o Horizontal and vertical articulation is required on all homes, as appropriate to each architectural style, and can be achieved through differing roof forms, combinations of one and two story elements, architectural projections, porches, etc.
- o Front porches, when appropriate to the building style, must have a minimum depth of six (6) feet.

D. REPETITION

Avoiding repetition of identical floor plans or architectural styles is important to create a sense that a neighborhood has been built over time.

- o In the areas of the neighborhood with a traditional home-to-street relationship, the same floor plan with the same architectural style shall be no less than three (3) lots away in any direction (on the same side of the street as well as the opposite side of the street).

- o It is appropriate to exhibit a cohesive architectural theme within the court homes to create a village concept, as such, deviation to the repetition requirement is allowable within this enclave.

4.2.2 Four-Sided Architecture

The continuation of style-specific architectural elements from the front façade around to the side and rear elevations creates an authentic architectural statement. As defined in the Architectural Guidelines section found in Section 4.4, there is a minimum level of enhancement required on all homes based on architectural style.

Russell Ranch features single-loaded streets with highly visible front and rear elevations and side elevations that are less prominent. The approach should be a hierarchy of treatment based on location. Blank, unadorned building faces are never permitted; a certain minimum amount of detail is required to reflect a unified architectural treatment. The front elevation should be the most highly detailed; the rear elevation should exhibit a specific number of style-specific architectural elements; typical side elevations may exhibit fewer style-specific architectural elements, while corner lots will feature a consistent level of detail on both the side and rear elevations.

The following section identifies enhanced lot situations as well as the four-sided elements that are required on these lots.

Figure 4.1 identifies home sites that are visible from multiple angles, public ways, open space, community edges, and major arterials. Home sites identified as enhanced lots are subject to the requirements in the following section.

A. ENHANCED LOTS

Home sites that are highly visible warrant special attention to any visible building faces to present an authentic and cohesive appearance. The following standards apply to highly visible lots within the neighborhood in addition to the standard requirements and enhancements for all homes described in section 4.4.

- All corner lots (those with two adjacent streets), identified in blue on Figure 4.1, must employ at least three enhancements from the enhanced elements portion of the corresponding architectural style matrix (found in Section 4.4) on all street adjacent building faces (in addition to the minimum enhancements required for all homes).
- All other highly visible home sites (such as those adjacent to open space corridors), identified in red on Figure 4.1, must employ at least two enhancements from the enhanced elements portion of the corresponding architectural style matrix (found in Section 4.4) on all building faces adjacent to public ways, open space, community edges, and/or major arterials (in addition to the minimum enhancements required for all homes).



Figure 4.1: Enhanced Lots

This illustration is a conceptual plan intended to guide the determination of highly visible lots. Actual determination of enhanced lots will be based on final lotting of subdivisions.

4.2.3 Reciprocal Use Easements

Reciprocal use easements are an innovative way to increase the usable yard area for a small lot home (reciprocal use easements are permitted on The Courts Homes only). By allowing one home to utilize the side yard of an adjacent home, side yard space effectively doubles. When reciprocal use easements are used, the following factors apply:

- The resident of the home relinquishing its side yard has the right to access the adjacent home's side yard for home maintenance and painting.
- Reciprocal use easements are required to be detailed on individual plot plans as part of the project construction phasing. Traditional setbacks shall not apply to reciprocal use easement areas, for landscape related features.
- Landscape structures, such as fountains, pergolas, etc. are permitted within the use easement and must be 3' from the face of the adjacent structure, consistent with building and fire code.

4.2.4 Garages

Reducing garage dominance on the streetscape and bringing living space closer to the street creates streetscenes that are inviting and safe with an "eyes on the street" environment. Using design techniques that enhance a home's architectural style and relegating the garage to a less visible position promotes a more pedestrian-oriented neighborhood. The following section describes the permitted garage mitigation measures for Russell Ranch.

- Garages must be recessed a minimum of 5' from living space or porches when accessed from a traditional street configuration. Garages that are located along

alleys or motor courts shall not be required to meet the 5' requirement.

- Garages accommodating more than two cars are allowable only in a split or tandem configuration. Three car front-loaded garages are not permitted.

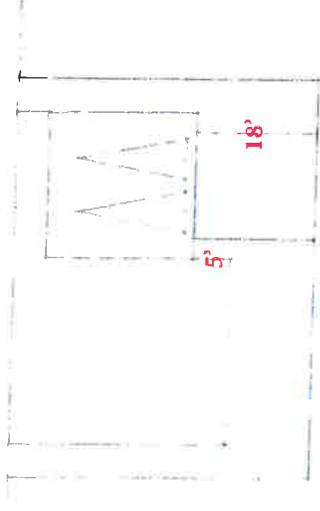


Figure 4.2: Garage Recess



Figure 4.3: View Lots (Easement)

This illustration is a conceptual plan intended to guide the determination of highly visible lots. Actual determination of view lots will be based on final lotting of subdivisions.

4.3 DEVELOPMENT STANDARDS

4.3.1 The Court Homes



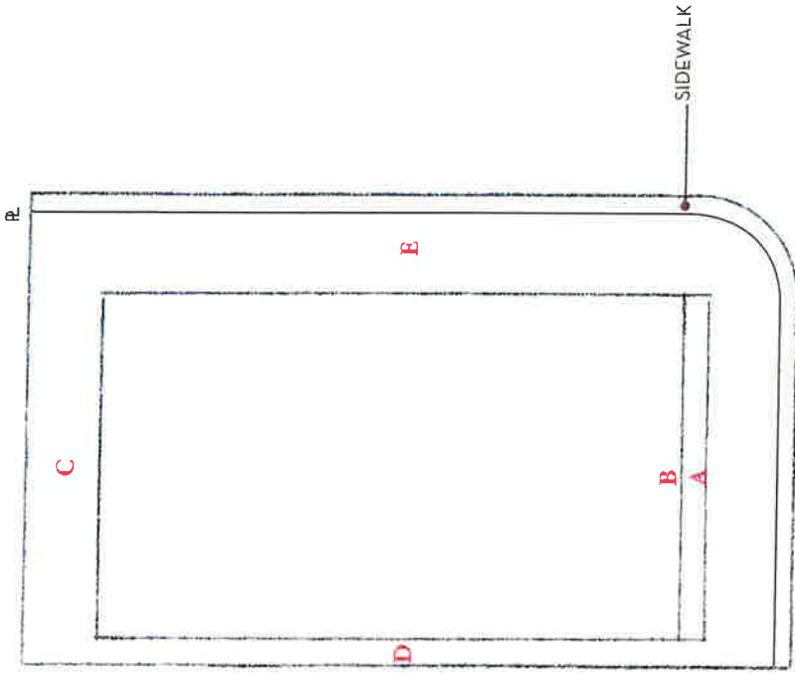
DESCRIPTION:

DESCRIPTION:	Court SFD	CATEGORY:	MLD
Minimum lot area:	2,150 Square Feet	A. Minimum front setback at court (to living or covered outdoor space):	4'
Minimum lot width:	43'	B. Minimum garage setback:	4'
Minimum lot depth:	50'	C. Minimum rear setback:	3'
Maximum lot coverage (single story / 2-story): ¹	60% / 60%	D. Minimum interior side setback:	3'
Maximum height (single story / 2-story):	28' / 35'	E. Minimum street side setback:	8'
Minimum parking requirement:	2 covered spaces/unit & 0.8 uncovered guest spaces/unit		

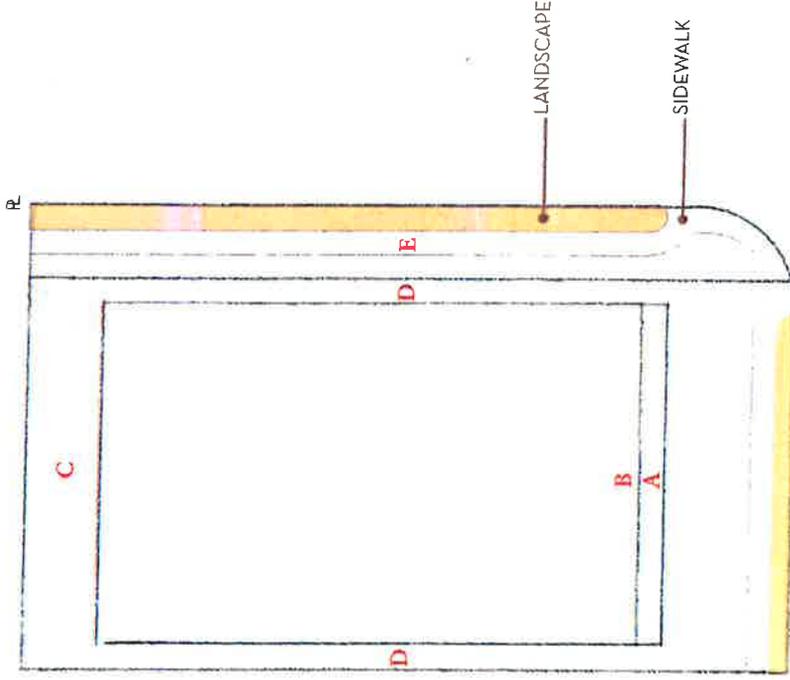
Note: Setbacks are measured from property line (P).

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4.3.2 Single Family High Density (SFHD)



Attached Sidewalk Condition



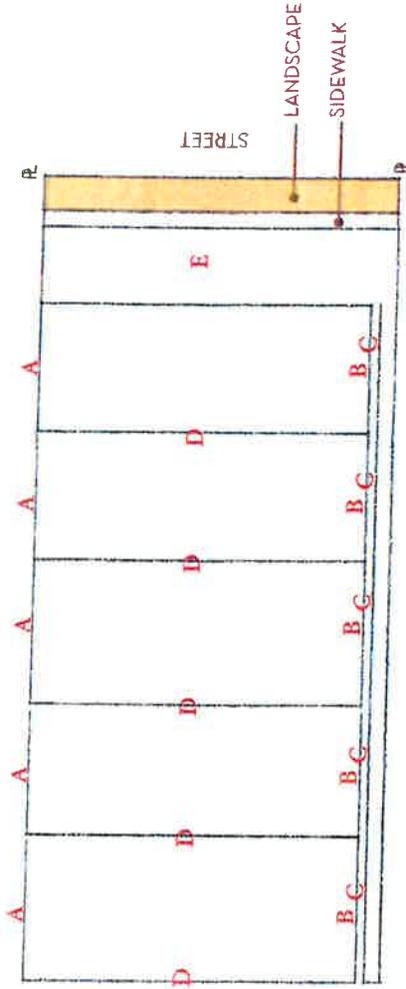
Detached Sidewalk Condition

DESCRIPTION:	SFD traditional lot	CATEGORY	SFHD
Minimum lot area:	4,500 Square Feet	A. Minimum front setback to living space or covered outdoor space (attached sidewalk/detached sidewalk); ²	12.5'/24.5'
Minimum lot width:	50'	B. Minimum garage setback (attached sidewalk/detached sidewalk); ²	18'/30'
Minimum lot depth:	90'	C. Minimum rear setback; ^{2,3}	15'
Maximum lot coverage: ^{1,5}	50%	D. Minimum interior side setback; ²	5'
Maximum height (single story / 2-story):	28' / 35'	E. Minimum street side setback; ^{2,4}	10'

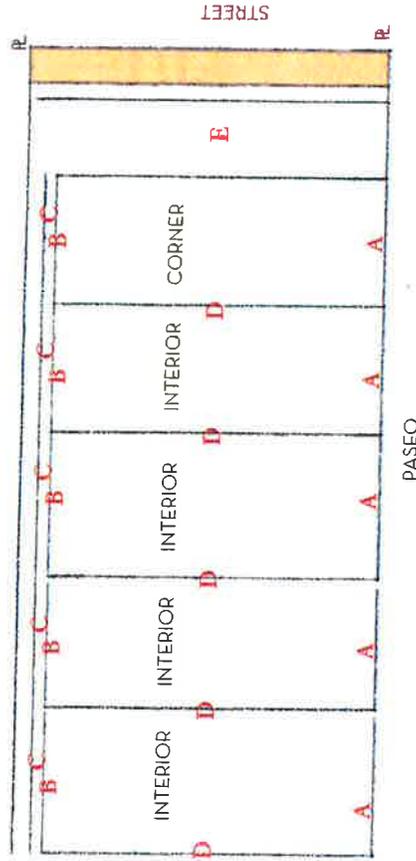
Note: Setbacks are measured from property line (R).

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4.3.3 Townhomes (MLD)



ALLEY



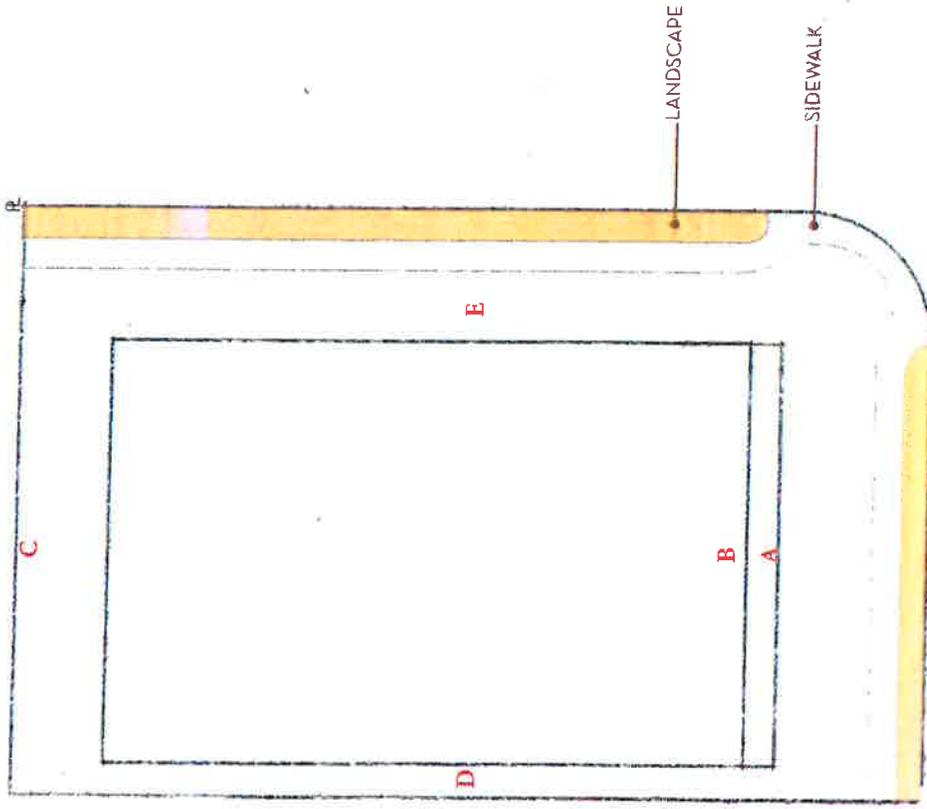
PASERO

DESCRIPTION:	MLD lot	CATEGORY	SFHD
Minimum lot area:	1,188.8 Square Feet	A. Minimum front setback at landscape/pasero to living space or covered outdoor space; ²	0'
Minimum lot width:	21.42'	B. Minimum rear garage setback at alley; ²	5'
/ 3' Minimum lot depth:	55.5'	C. Minimum rear setback at alley (lower floor/upper floor); ^{2,3}	5'/3'
Maximum lot coverage; ^{1,e}	90%	D. Minimum interior side setback; ²	0'
Maximum height (single story / 2-story):	35'	E. Minimum street side setback; ^{2,4}	12.5'
Minimum parking requirement:	2 covered spaces / unit & 0.8 uncovered guest spaces / unit		

Note: Setbacks are measured from property line (R).

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4.3.4 Single Family (SF)



Detached Sidewalk Condition

DESCRIPTION:	SFD traditional lot	CATEGORY:	SF
Minimum lot area:	6,600 Square Feet	A. Minimum front setback to living space or covered outdoor space: ²	24.5'
Minimum lot width:	60'	B. Minimum garage setback: ²	30'
Minimum lot depth:	110'	C. Minimum rear setback: ^{2,3}	15'
Maximum lot coverage: ^{1,6}	50%	D. Minimum interior side setback: ²	5'
Maximum height (single story / 2-story):	28' / 35'	E. Minimum street side setback: ^{2,4}	22'

Note: Setbacks are measured from property line (R).

4.3.5 Development Standards Footnotes

1. MLD lot coverage shall be calculated as the percentage of lot area covered by the roof, including covered porches and patios. SFHD and SF lot coverage shall be calculated as the percentage of lot area covered by the roof, including covered porches but excluding outdoor covered unconditioned spaces (i.e., patios) and guest house/pool house/casita (i.e., detached habitable structure with only sleeping, living, and bathroom).
2. Accessory structures shall be consistent with the FPASP standards of 5' side yard (interior lot lines) and 5' rear yard setbacks; notwithstanding provision 3a below.
3. Minimum rear setback (view lots)
 - a. View lots, as identified in Figure 4.3, shall have a rear yard setback measured from the top of slope of the lot (as opposed to the downslope property line).
 - b. A View Lot Easement shall be recorded over the sloping portion of the lot (from the hinge point to the rear property line, see page 25, Rear Yard View Fencing section).
 - c. The easement area landscape shall conform to the slope area landscape treatments described in Chapter 3.
 - d. Monitoring and compliance with slope area requirements shall be the responsibility of Russell Ranch Homeowners Association. Any violations observed by the City of Folsom shall be referred to the Russell Ranch Homeowners Association.

4. Street side setback shall be 5' (measured from back of sidewalk) when adjacent to an open space lot 5' or greater in width.
5. A 5% lot coverage bonus for 3-sided outdoor covered unconditioned spaces (except with fireplace option, then 2-sided permitted) and/or guest house/pool house/casita (i.e., detached habitable structure with only sleeping, living and bathroom) shall be granted. In no case shall the total lot coverage exceed 55%.
6. A 10% lot coverage bonus for 3-sided outdoor covered unconditioned spaces (except with fireplace option, then 2-sided permitted) and/or guest house/pool house/casita (i.e., detached habitable structure with only sleeping, living and bathroom) shall be granted. In no case shall the total lot coverage exceed 60%.
7. For SF and SFHD lots, front yard setbacks shall be measured from property lines. Property lines shall be located as depicted in Figure 4.3. The Public Utility Easement (PUE) shall be located co-terminous with the front property line and extend into the lots as shown in yellow on Figure 4.3.

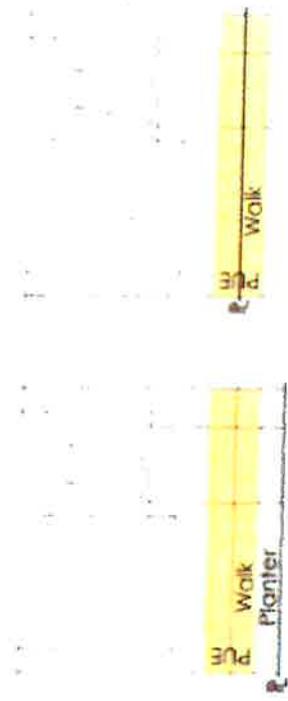


Figure 4.3: SF and SFHD PUE Location

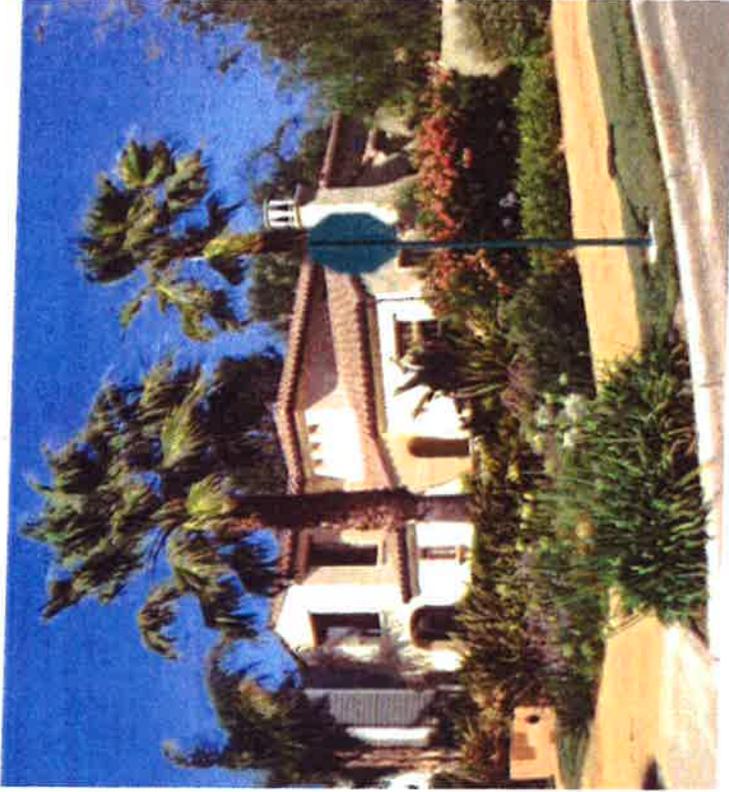
4.4 ARCHITECTURAL GUIDELINES

The concept, inspiration, and vision for Russell Ranch is one of a distinctively California village with a unique and compelling design character derived from successful landmark neighborhoods throughout California.

Russell Ranch presents a cohesive collection of nine architectural styles, which will create a diverse and eclectic streetscape through massing and form, material and color, and detailing. The variety of styles will animate and activate the streetscape, creating a dynamic and vibrant community palette. Additional architectural styles that are consistent with the neighborhood vision will be reviewed and approved by the RRDRRC on a case by case basis.

ARCHITECTURAL PALETTE

- o The California Wine Country
- o The Monterey
- o The Spanish Eclectic
- o The California Cottage
- o The Transitional Bungalow
- o The California Prairie
- o The Spanish Colonial Revival
- o The California Craftsman
- o The California Villa



Streetscape Example

HOW TO USE THESE GUIDELINES

The guidelines found in this section are formatted into individual style spreads, with each architectural style being defined and articulated over the course of three pages. The first page of the style spread defines the history and intent of the style; the second page provides a matrix that identifies the minimum elements for each style along with applicable enhanced elements and the third page identifies key style elements.

The core requirements for home design within the Russell Ranch neighborhood are concentrated in each style matrix (the second page of each spread), with the remaining pages intended as supporting and inspirational material. The sketches, key style elements, and details are intended to be an overview of the defining characteristics of the style and are not intended to be exhaustive, nor to represent all of the appropriate details inherent to each style.

With the intent of creating authentic representations of these architectural styles, all of the minimum elements outlined on the style specific matrix are required along with three enhanced elements.

To further define and emphasize the architecture of Russell Ranch, the following statements apply to all styles:

- Masonry must be applied authentically, wrapping outside corners and terminating at inside corners.
- Stone or brick scattered over stucco to mimic building age is not appropriate.
- Heavy knock down or "Spanish Lace" stucco is not permitted. Stucco finish options may include light lace, sand, smooth, imperfect smooth, cat face, or similar.

- All material changes must occur at an inside corner or other defined terminus (i.e., a fence line).
- No fascia gutter (gutter that serves as fascia) is permitted.
- No concrete rake tiles are permitted.
- Where wood is specified, cementitious material is acceptable to promote longevity and ease of maintenance.
- Grooved plywood siding and vinyl siding are not permitted.
- Garage doors shall complement the architectural style.
- House lights shall complement the architectural style.
- When shutters are used, each shutter shall be sized to one-half of the entire adjacent window width, such that if the shutters were closed, they would completely cover the window.

THE CALIFORNIA WINE COUNTRY

Reflective of the rolling terrain of the California Wine Country, this casual and rustic architectural style borrows European accents and reinterprets them to be authentically California. Asymmetrical forms are layered to create casual massing and the use of rustic accent materials, such as overgrouted stone or brick, wrought iron, and heavy beams add to the charm of this style.

The California Wine Country style will add a rustic element to the streetscape, offering a casual and lower profile complement to the more formal vertical styles.



Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1,2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> Simple rectangular massing in one, one-and-a-half, full two-story, or a combination thereof. 	
Roof	<ul style="list-style-type: none"> Low-pitched gabled primary roofs (3:12 to 5:12). Shed porches. Tight rake at gables (0" to 6"). 12" to 16" eaves. Barrel or S-tile roof. 	<ul style="list-style-type: none"> Exposed rafter tails. Boosted roof tiles.
Walls	<ul style="list-style-type: none"> Stucco is the primary wall material. 	<ul style="list-style-type: none"> Overgrouted stone or brick as entire massing element. Smooth or imperfect smooth stucco finish.
Windows & Doors	<ul style="list-style-type: none"> Head and sill window trim or full window surrounds. Simple wood panel doors with vertical panels. Divided light windows. 	<ul style="list-style-type: none"> Front elevation with all windows inset 2" or one feature window recessed 12". Shutters (plank or diagonal brace). Colored window frames (e.g. cranberry, sage green, dark brown, etc.)
Details	<ul style="list-style-type: none"> Rustic wood railing and column posts. 	<ul style="list-style-type: none"> Massive chimney (may be battered or tapered) clad in stucco, stone, or brick. Wood trellises, applied sheds over windows, or Bermuda shutters. Carriage style garage doors with hardware. Typical downspouts replaced with "rain chains" or round metal downspouts.

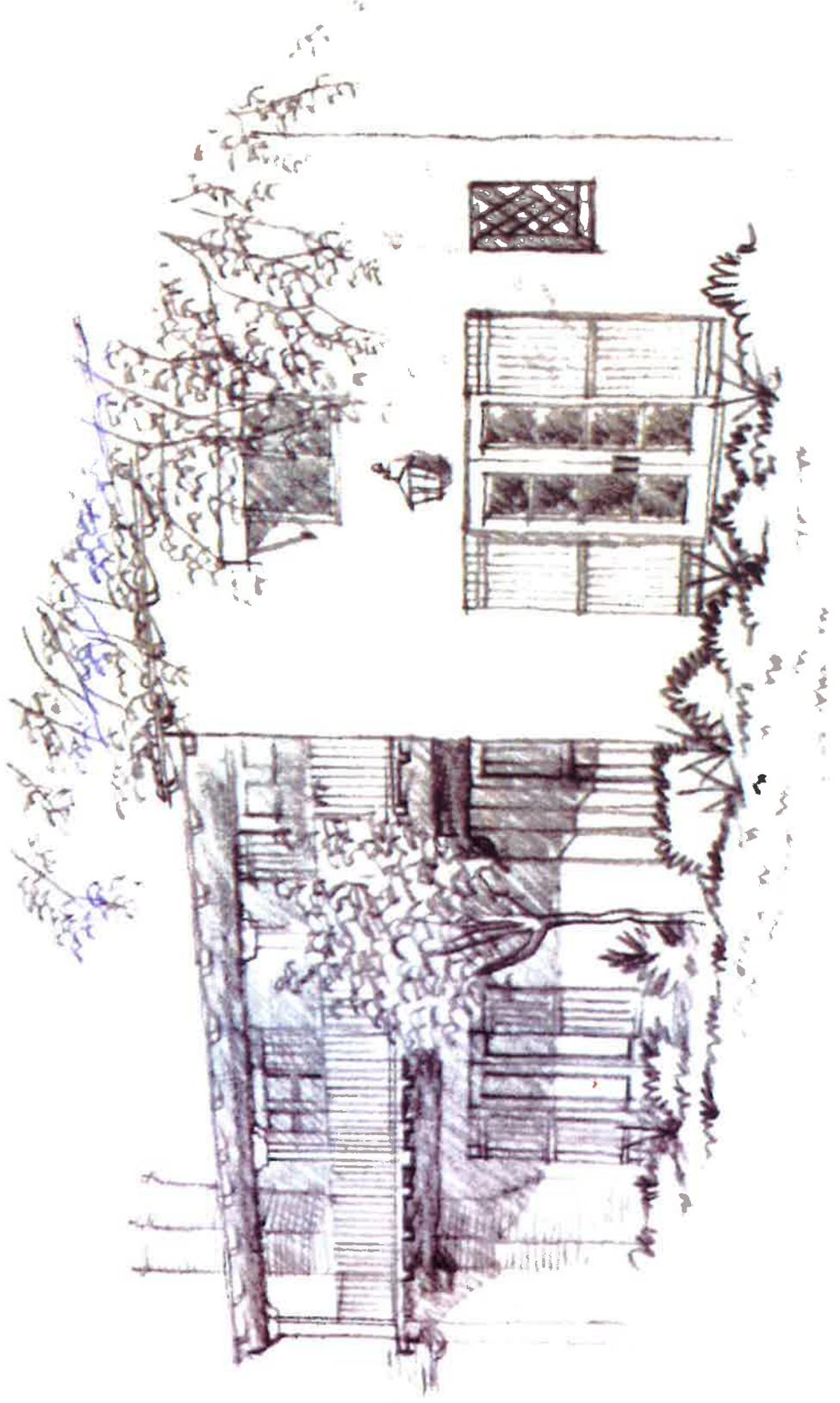
1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.

2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE MONTEREY

The historic Monterey style blends the Spanish cultures of original California residents with the influences brought to the west by the first European settlers. These homes are a juxtaposition of local indigenous materials with colonial detailing applied. The dominant feature of the Monterey style is the always-present upper-story balcony element, which is contained within the principal roof form and cantilevered. The balcony is of heavy timbered construction, defining the structure.

The Monterey style is a direct link to the colonial heritage of California. Only appropriate for two-story homes, this style will enhance the animation of the streetscape, both vertically and horizontally, through height, recess, and significant shadow play.



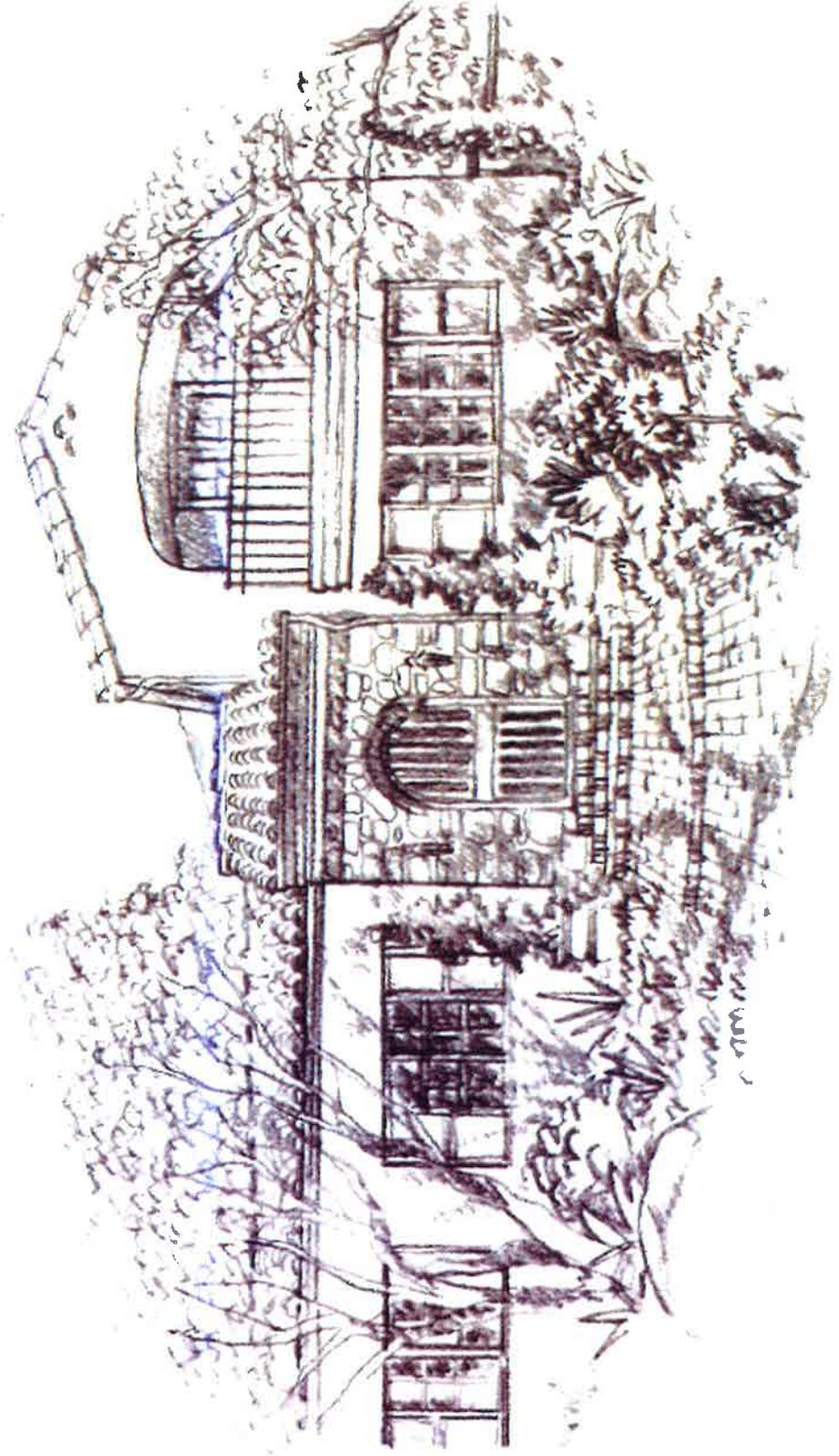
Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1, 2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • Two-story, rectangular form. • Principal side gabled roof. • Cantilevered second story balcony covered by principal roof. 	<ul style="list-style-type: none"> • L-shaped form with front facing cross gable. • Hipped roof elements.
Roof	<ul style="list-style-type: none"> • Low-pitched gabled roofs (4:12 to 5:12). • Flat tile roof with barrel ridge and hip tiles. • 12" to 16" overhangs. • Thin eaves with either a half-round or egee gutter. 	<ul style="list-style-type: none"> • Full S-tile roof. • Applied shed roof elements over windows • Exposed rafter tails.
Walls	<ul style="list-style-type: none"> • Stucco is the dominant exterior finish. 	<ul style="list-style-type: none"> • Brick at first floor, which may be painted. • Horizontal wood siding at upper floor. • Thickened walls.
Windows & Doors	<ul style="list-style-type: none"> • Paired windows in groups of two or three. • Tall vertical windows. • Rustic plank entry door. 	<ul style="list-style-type: none"> • First floor arched picture window at cross gable. • Bermuda shutters. • At least one pair of French doors accessing the balcony.
Details	<ul style="list-style-type: none"> • Fixed panel or louvered wood shutters. • Wood railing at balcony to match posts and beams. 	<ul style="list-style-type: none"> • Exposed decorative wood elements at balconies. • Ornate wrought iron railing at balcony.

1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.
2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE SPANISH ECLECTIC

The Spanish Eclectic architectural style in California gained in popularity and sophistication in surface design after the 1915 Panama-California Exposition in San Diego. The Spanish Eclectic house is popular in many historic neighborhoods due to its adaptability of formal and casual, playful character. Historic precedence can be drawn from a wide and diverse range of influences; region, chronology, and function all contribute to the evolution of the Spanish Eclectic style. Truly one of the most diverse architectural styles, the Spanish Eclectic can vary from playful to exotic, bungalow to hacienda.

The Spanish Eclectic style will add to the intrinsic character and rich diversity of the streetscape. Offering an opportunity for bold color statement and whimsical forms, the style will add a playful element to the neighborhood.



Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1, 2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • One, one-and-a-half (with strong one-story element and stepped back second story), or full two-story massing. (The form is not rigidly defined; this style can be applied to a wide variety of asymmetrical building mass configurations.) • Roof form is predominately pitched, hipped or gabled, but may also be parapeted. 	<ul style="list-style-type: none"> • Massive chimney with buttressed form and elaborate cap with arched opening and small tiled roof. • Massive battered (tapered) chimney with finial chimney cap. • Recessed arcade along front elevation. • Arcaded wing wall. • Balconies.
Roof	<ul style="list-style-type: none"> • Low pitched roof (3:12 to 5:12). • Simple flat, hip, or gable roof with one intersecting gable roof. • Overhangs are typically tight, but can be up to 18". • Fascia is either tight to the building (6" max) or nonexistent with rake tile providing the transition from wall to roof. • S-shaped concrete tiles. • Thin eaves with either a half-round or ogee gutter. 	<ul style="list-style-type: none"> • "Boosted" or raised tiles (may be all or a percentage of the roof field). • Applied shed roof elements over windows. • Exposed rafter tails.
Walls	<ul style="list-style-type: none"> • Stucco is the dominant exterior finish. 	<ul style="list-style-type: none"> • Overgrouted stone, brick, or adobe expressed as a single massing element.
Windows & Doors	<ul style="list-style-type: none"> • Feature recessed arched picture window or three grouped arched windows. • Vertical multi-paned windows or inserts at front elevations. • Window head and jamb trim is absent. • Modest (4" max) window sill trim. 	<ul style="list-style-type: none"> • Accent beveled glass recessed window. • Single or grouped arched windows. • Decorative precast concrete door and window surrounds. • Heavy wood head trim at windows. • Thickened walls. • Juliet balconies. • Bermuda shutters.
Details	<ul style="list-style-type: none"> • Masonry vents. • Canales. 	<ul style="list-style-type: none"> • Shaped rafter tails at feature areas. • Wrought iron balconies and accent details. • Arched stucco column porches. • Vibrant and colorful glazed Spanish tile accents.

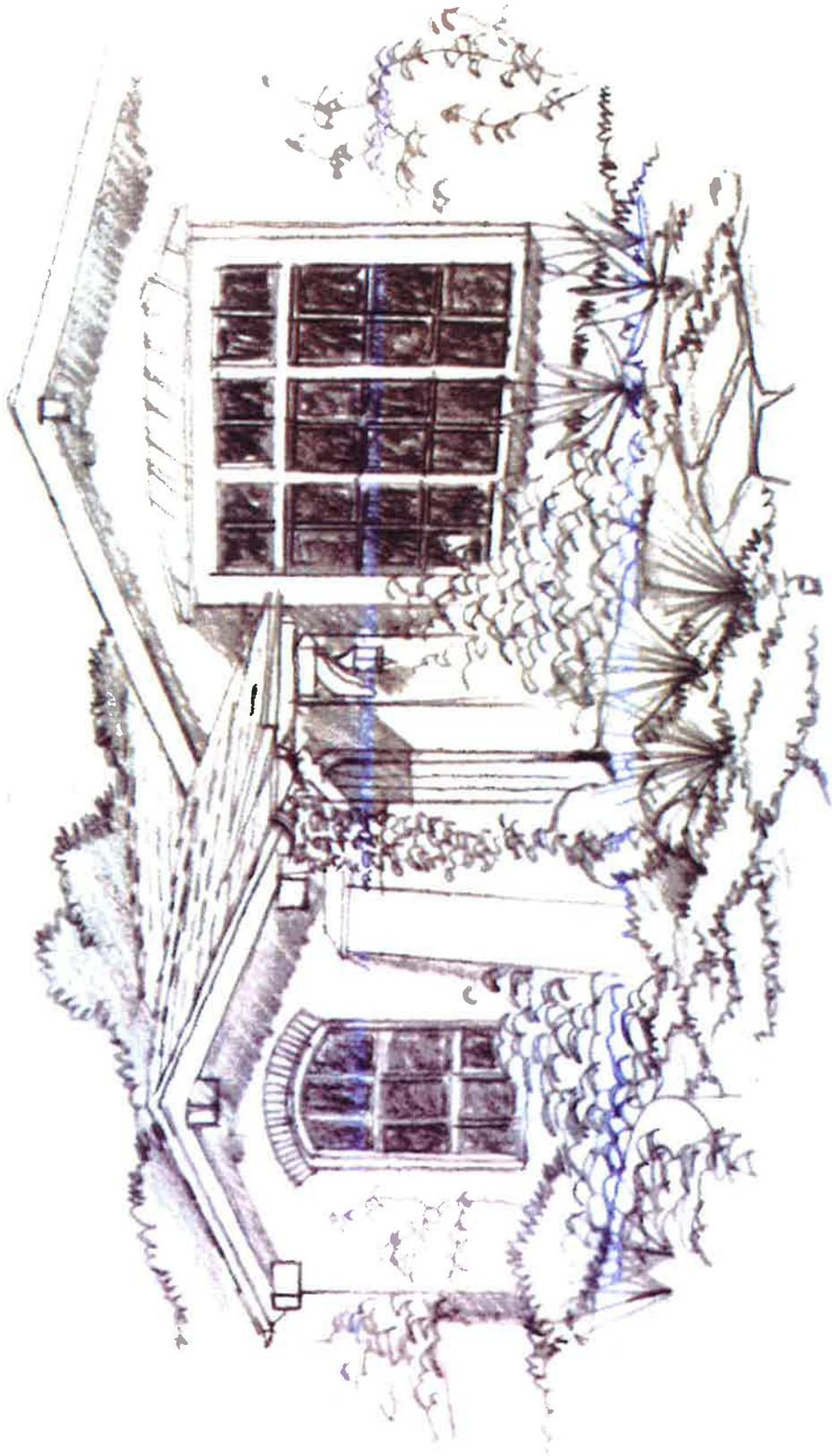
1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.

2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE CALIFORNIA COTTAGE

The California Cottage introduces a quaint and picturesque element to the eclectic Russell Ranch streetscape. The Cottage typically has a steeply pitched roof, with the principal roof being side gabled, and multiple asymmetric cross gables. The Cottage is a popular style throughout historic California neighborhoods, with French and English European influences.

Homes may have applied half-timbering, and may feature face brick, which can be painted. Gabled dormers are common, with only modest eave extension. Details such as eyebrow dormers, bell-cast eaves, and arched windows add charm and whimsy to the storybook architectural style. Windows tend to be vertically oriented, often with casements, and often with square gridded or diamond-pane leaded muntins. Cottage houses generally have prominent chimneys.



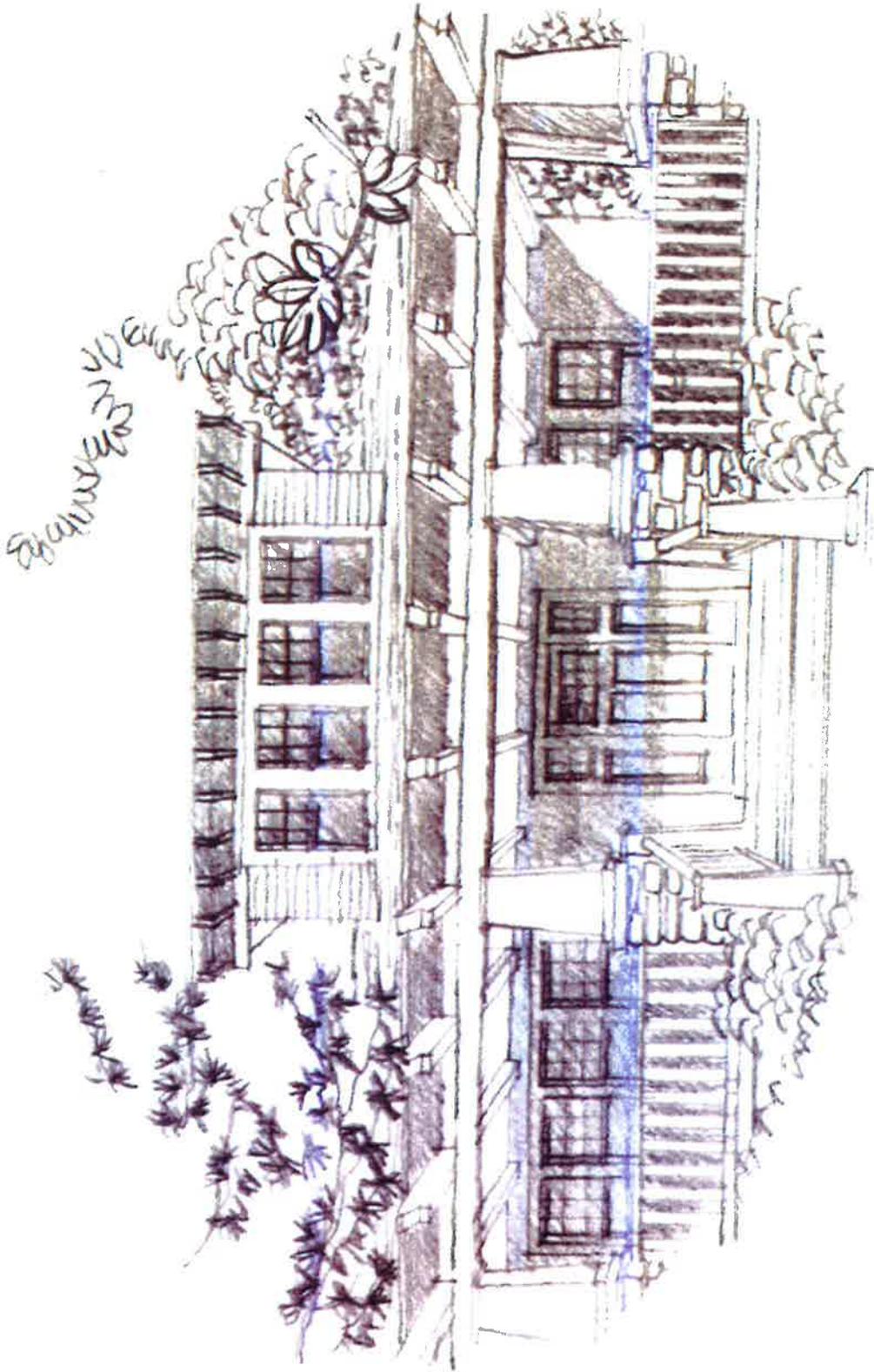
Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1,2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • One, one-and-a-half, or full two-story massing. • Asymmetrical massing and proportions. • Gable roof form (either front-to-back, side-to-side, or cross-gable). 	<ul style="list-style-type: none"> • Single eyebrow dormer. • Multiple (two or three) gable dormers. • Massive chimney, usually integrated with the dominant gable.
Roof	<ul style="list-style-type: none"> • Low pitched main roof (3:12 to 6:12). • Asphalt composition shingles preferred, concrete tile allowed with raised bargeboard. • Rake at gables up to 12°. 	<ul style="list-style-type: none"> • Steeply pitched accent gable (6:12 to 9:12). • Standing seam metal accent roof at dormers or bay windows. • Up to 16" eaves.
Walls	<ul style="list-style-type: none"> • Stucco, lap siding, masonry/brick, stone, or any combination thereof. 	<ul style="list-style-type: none"> • Smooth or imperfect smooth stucco. • Stone or brick wainscot. • Horizontal siding accents at gables and single massing elements. • Masonry as an entire massing element (e.g. chimney gable end, etc.) • Painted brick.
Windows & Doors	<ul style="list-style-type: none"> • Divided lights common on all windows. • Vertical windows in groupings of two and three. • Head and sill window trim or full window surrounds. • Entry doors accented by trim surrounds. 	<ul style="list-style-type: none"> • Front elevation with all windows inset 2" or one feature window recessed 12". • Round accent window or arched accent window flanked with arched shutters. • Soft arch or radius top windows. • Casement windows. • Mull window groupings. • Arched entry door. • Brick or stone window and/or door surrounds at key locations. • Bay window. • Deep recessed entry door.
Details	<ul style="list-style-type: none"> • Shutters 	<ul style="list-style-type: none"> • Exposed accent wood timbers and beams. • Cast concrete door surrounds, window trim accents, and/or lentils. • Balcony or windows with decorative metal railings and French doors. • Recessed gable vents. • Leader heads at downspouts. • Brick window and/or door headers at key locations.

1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.
2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE TRANSITIONAL BUNGALOW

The Craftsman bungalow was the first truly American vernacular style and became a very popular and beloved addition to historic California streetscapes. The Craftsman plan form broke with earlier formal Victorian spatial arrangements and changed the way that families lived in and related to their houses.

Russell Ranch's Transitional Bungalow reinterprets the traditional Craftsman style through a reduction in ornamentation and delicacy, bold material placement emphasizing major architectural elements, and prominent strong architectural forms. Focused on the blending of structure with nature, window walls and clerestory forms effectively bring the outdoors in to the home.



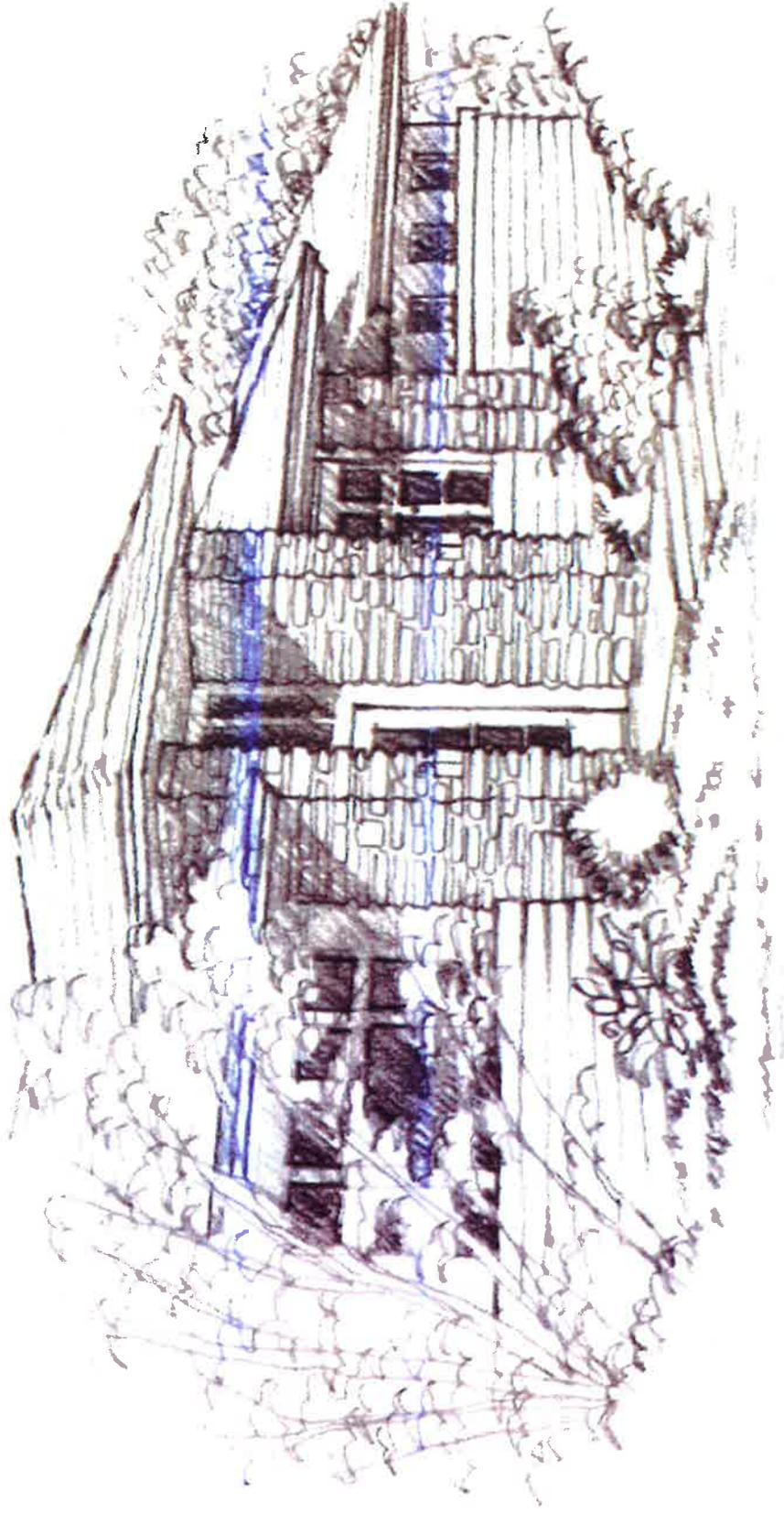
Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1, 2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> Simple massing, front or side gabled, typically one- to one-and-a-half-story massing. Symmetrical or asymmetrical form. Deep front entry porch. Stylized column and beam detailing at porches 	<ul style="list-style-type: none"> Cross-gabled massing. Two stories with a combination of one and two-story elements. 6' minimum full width, deep porch at entry.
Roof	<ul style="list-style-type: none"> Low-pitched roofs with large over-hanging eaves, emphasizing horizontal planes. 4:12 to 6:12 roof pitch. 12" to 18" overhangs. Asphalt composition shingles 	<ul style="list-style-type: none"> Concrete roof tile with raised bargeboard. Varied porch roofs; shed or gabled. Metal roof at porch (standing seam or corrugated). Cascading (multiple) gables. Single large shed dormer. 18" to 36" overhangs. Extended and shaped barge rafters. Exposed rafter tails at eaves.
Walls	<ul style="list-style-type: none"> Exterior wall materials with combinations of materials, such as wood shingles, horizontal siding, board and batten, and stucco. 	<ul style="list-style-type: none"> Brick chimneys. Battered (tapered) stone foundation or wainscot. Foundation or wainscot using brick.
Windows & Doors	<ul style="list-style-type: none"> Single hung windows at front elevations. Use windows individually or in groups (typically two or three). Doors with full surrounds. Windows with full surrounds and a projected sill/apron. 	<ul style="list-style-type: none"> Casement windows. Three or more windows in a "ribbon." Grouped windows with a high transom. Large feature picture window flanked by two narrow vertical windows. Wide wood entry door with integrated glass. Wood door and window surrounds.
Details	<ul style="list-style-type: none"> Entry porches with columns resting on larger piers or bases. Porch rails of repeated vertical elements. 	<ul style="list-style-type: none"> Entry porch columns consisting of single or multiple wood posts with rectangular or battered brick or stone piers or bases. Wood brackets or knee braces. Porch rails comprised of decoratively cut boards that create a horizontal pattern. Typical downspouts replaced with "rain chains" or round metal downspouts. Open eave overhangs with plumb or square cut rafter tails. Exposed square cut ridge beams, outlookers, and purlins. Porte-cochère, pergola, or trellis that continues, or is integrated with, the front porch.

1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.
 2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE CALIFORNIA PRAIRIE

The Prairie style was borne of the Chicago Prairie School movement. The style is organic in nature, integrated with the land, using natural materials and abstracted nature forms. The Prairie emphasizes the integration of indoor and outdoor spaces with its trademark wide overhangs, appropriate for the regional climate, identifying the style. Although not as prevalent in the area as the Craftsman style, Prairie homes are very distinctive and add a strong horizontal presence to the community.

The California Prairie at Russell Ranch is a slightly abstracted version of the traditional Prairie style with simplicity in form and detail. A commitment to strong orthogonal forms and linear arrangements tie the California Prairie to its Midwestern roots.



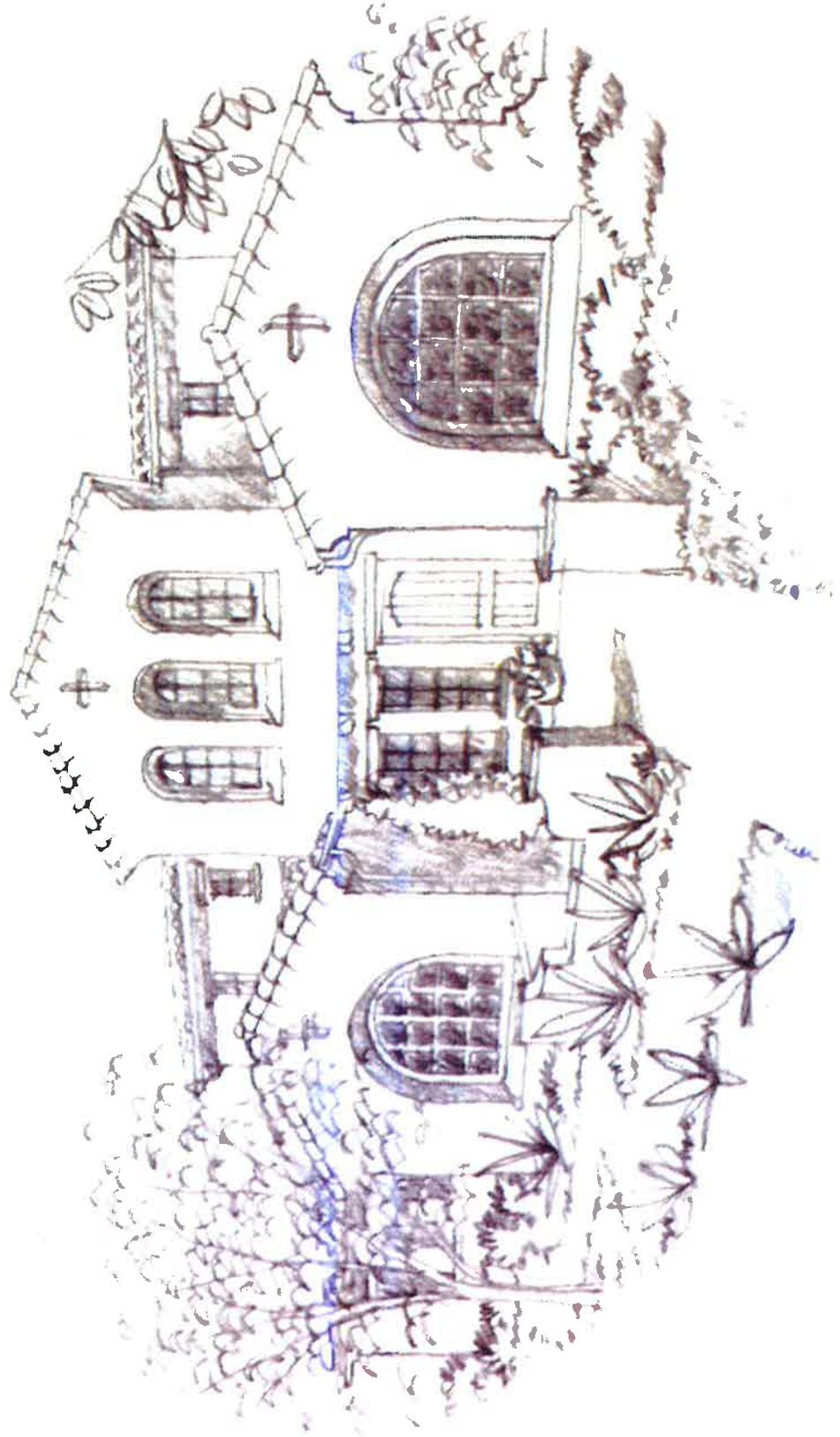
Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1,2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • One or two-story with strongly horizontal massing. • Secondary masses perpendicular to the primary forms. 	<ul style="list-style-type: none"> • Porte-cocheres (where applicable) and raised porches extending out from the entry of the home. • Accentuated horizontal base extending out as a site or planter wall.
Roof	<ul style="list-style-type: none"> • Long, horizontal low-pitched hip roofs with large overhanging eaves, emphasizing horizontal planes (3.5:12 to 4:12 roof pitch). • 36" minimum overhangs. • Gable roof forms are also appropriate. • Flat concrete tile with a shingle appearance or asphalt composition shingles. 	<ul style="list-style-type: none"> • Terraces covered by primary roof form with massive rectilinear stone piers for roof support. • 12" - 18" overhangs
Walls	<ul style="list-style-type: none"> • Stucco in combination with ledge stone or masonry wainscot base. • Ledge stone used as post bases and fireplaces only. 	<ul style="list-style-type: none"> • Extensive use of brick or ledge stone, used to emphasize the horizontal planes, with struck horizontal grout joints. • Cement plank lap siding is found in some examples.
Windows & Doors	<ul style="list-style-type: none"> • Square or rectangular windows • Grouping and arrangement of windows should emphasize the geometry of the elevation. • Ribbons of windows under deep roof overhangs. • Wood window and door trim. 	<ul style="list-style-type: none"> • Clerestory windows. • Leaded glass inserts at entry. • Accent colored window frames. • Style-specific divided lights
Details	<ul style="list-style-type: none"> • Ornamental railings and gates. • Wood beams and brackets. 	<ul style="list-style-type: none"> • Metal or wood fascia. • Carpenter detailing³. • Style-specific unique lighting fixtures. • Low garden walls to enclose and frame outdoor living spaces. • Massive chimney forms, wrapped in stone or brick.

1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.
2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.
3. Wood trim details (e.g., at doors, windows, eaves, terraces and pergola style porte-cocheres).

THE SPANISH COLONIAL REVIVAL

The 1915 Panama-California Exposition, designed by Spanish Colonial architecture expert Bertram Grosvenor Goodhue, ended the precedent of free adaptations of the Mission style and brought the emergence of more elaborate Spanish prototypes. Goodhue wanted to go beyond the then prevalent Mission interpretations and emphasize the richness of Spanish precedents found throughout Latin America. Fashionable architects of the time began to look directly to Spain for source material, finding a long and rich sequence of architectural traditions, which became melded into a style that became the Spanish Colonial Revival.

The Spanish Colonial Revival introduces a more formal and symmetrical counterpart to the playful Spanish Eclectic style, adding diversity to the Russell Ranch streetscape.



Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1, 2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • One, one-and-a-half (with strong one-story elements), or full two-story massing. • Roof form is predominantly pitched, hipped or gabled, but may also have a parapeted accent, which may be shaped in a traditional mission form. • Primarily symmetrical form. 	<ul style="list-style-type: none"> • Massive chimney with buttressed form and elaborate cap with arched opening and small tiled roof. • Massive battered (tapered) chimney with finial chimney cap. • Recessed arcade along front elevation. • Arcaded wing wall. • Balconies. • Cantilevered second story elements with brackets.
Roof	<ul style="list-style-type: none"> • Low pitched roof (3:12 to 5:12). • Overhangs are typically tight, but can be up to 18". • Fascia is either tight to the building (6" max) or nonexistent with rake tile providing the transition from wall to roof. • S-shaped concrete tiles. • Thin eaves with either a half-round or ogee gutter. 	<ul style="list-style-type: none"> • "Boosted" or raised tiles (may be all or a percentage of the roof field). • Exposed rafter tails.
Walls	<ul style="list-style-type: none"> • Stucco is the dominant exterior finish. 	
Windows & Doors	<ul style="list-style-type: none"> • Feature recessed arched picture windows or three grouped arched windows. • Vertical multi-paned windows or inserts at front elevations. • Window head and jamb trim is absent. • Modest (4" max) window sill trim. • Oversized wood entry door. 	<ul style="list-style-type: none"> • Multiple feature recessed arched windows. • Accent beveled glass recessed window. • Single or grouped arched windows. • Decorative precast concrete door and window surrounds. • Heavy wood head trim at windows. • Thickened walls. • Juliet balconies. • Bermuda shutters.
Details	<ul style="list-style-type: none"> • Masonry vents. • Canales. 	<ul style="list-style-type: none"> • Scalloped eaves. • Shaped rafter tails at feature areas. • Wrought iron balconies and accent details. • Arched stucco column porches. • Vibrant and colorful glazed Spanish tile accents.

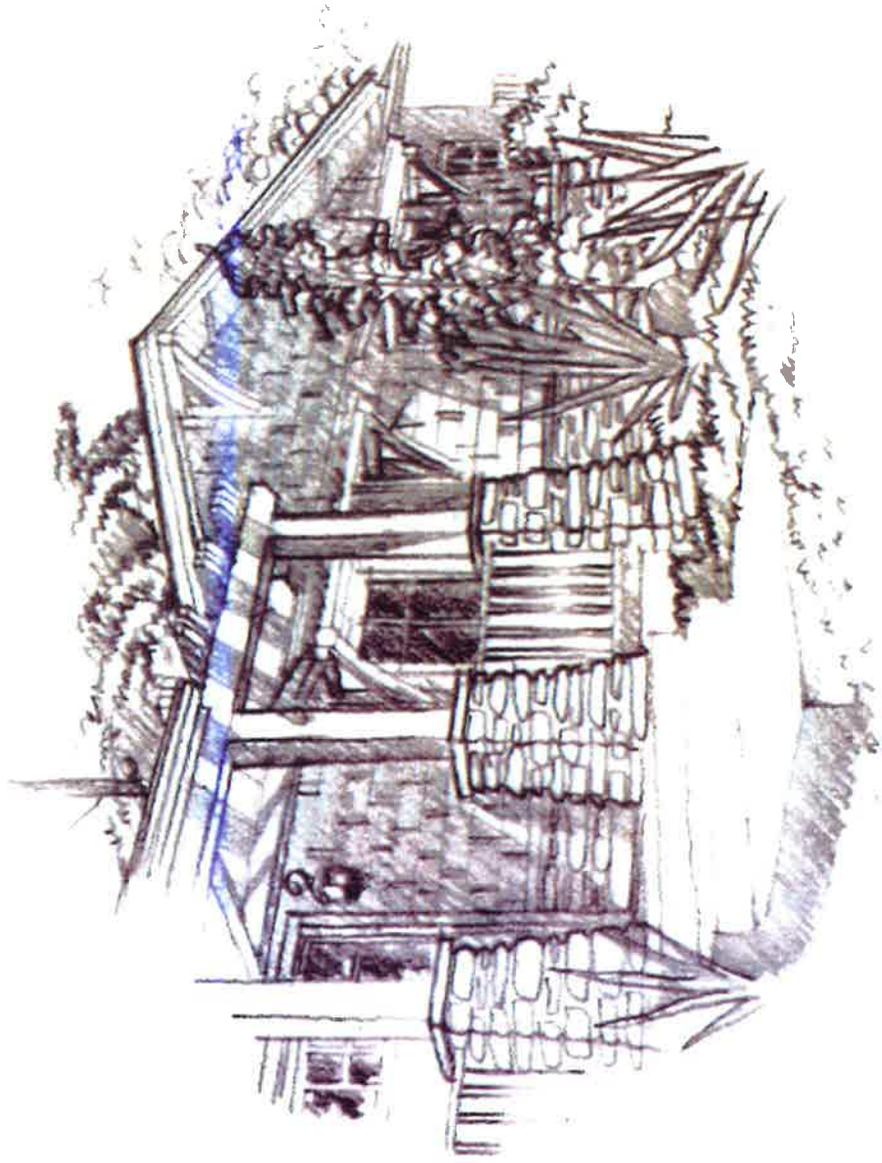
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 2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE CALIFORNIA CRAFTSMAN

The Craftsman bungalow was an enormously influential form and style of architecture in America between 1906 and 1918, the first truly American vernacular style. The Craftsman plan form broke with earlier formal Victorian spatial arrangements and changed the way that families lived in and related to their houses.

Architecturally, the Craftsman bungalow was designed to achieve harmony between the house and its landscape, to get as close as possible to nature. A Craftsman bungalow has many of the hallmarks of the Arts and Crafts aesthetic: clinker brick, carved rafter tails, a mixture of cladding (brick, clapboard, tile, and shingle), and oversized eave brackets painted in colors of nature.

The California Craftsman recalls the comfortable and welcoming nature of the much-loved historic Craftsman bungalows with a uniquely California flair with large windows and an emphasis on the blending of indoor and outdoor spaces. These homes reflect a sense of permanence that only artisanship and careful design can convey.



Style Elements	Minimum Elements (All Homes)	Enhanced Elements ^{1,2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> Simple massing, front or side gabled, typically one- to one-and-a-half-story massing. Symmetrical or asymmetrical form. 6' minimum deep front entry porch. Stylized column and beam detailing at porches 	<ul style="list-style-type: none"> Cross-gabled massing. Two stories with a combination of one and two-story elements. Full width, deep porch at entry.
Roof	<ul style="list-style-type: none"> Low-pitched roofs with large over-hanging eaves, emphasizing horizontal planes. 4:12 to 6:12 roof pitch. 12" to 18" overhangs. Asphalt composition shingles preferred. 	<ul style="list-style-type: none"> Concrete roof tile with raised bargeboard. Varied porch roofs; shed or gabled. Cascading (multiple) gables. Roof dormers (shed or gable form). 18" to 36" overhangs. Extended and shaped barge rafters. Exposed rafter tails at eaves.
Walls	<ul style="list-style-type: none"> Exterior wall materials with combinations of materials, such as wood shingles, horizontal siding, board and batten, and stucco. 	<ul style="list-style-type: none"> Stone, brick or combination chimneys. Eliminate stucco as a wall treatment. Foundation or wainscot using brick, stone, or a combination. Battered (tapered) stone foundation or wainscot.
Windows & Doors	<ul style="list-style-type: none"> Single hung windows at front elevations. Use windows individually or in groups (typically two or three). Doors with full surrounds. Windows with full surrounds and a projected sill/apron. 	<ul style="list-style-type: none"> Casement windows. Divided light windows. Three or more windows in a "ribbon." Grouped windows with a high transom. Wide wood entry door with integrated glass. Wood door and window surrounds.
Details	<ul style="list-style-type: none"> Entry porches with columns resting on larger piers or bases. Porch rails of repeated vertical elements. 	<ul style="list-style-type: none"> Wood brackets or knee braces. Entry porch columns consisting of single or multiple wood posts with rectangular or battered brick or stone piers or bases. Porch rails comprised of decoratively cut boards that create a pattern. Additional "stick-work" in gable ends. Typical downspouts replaced with "rain chains." Open eave overhangs with shaped rafter tails. Decorative ridge beams, outlookers, and purlins. Porte-cochère, pergola, or trellis that continues, or is integrated with, the front porch.

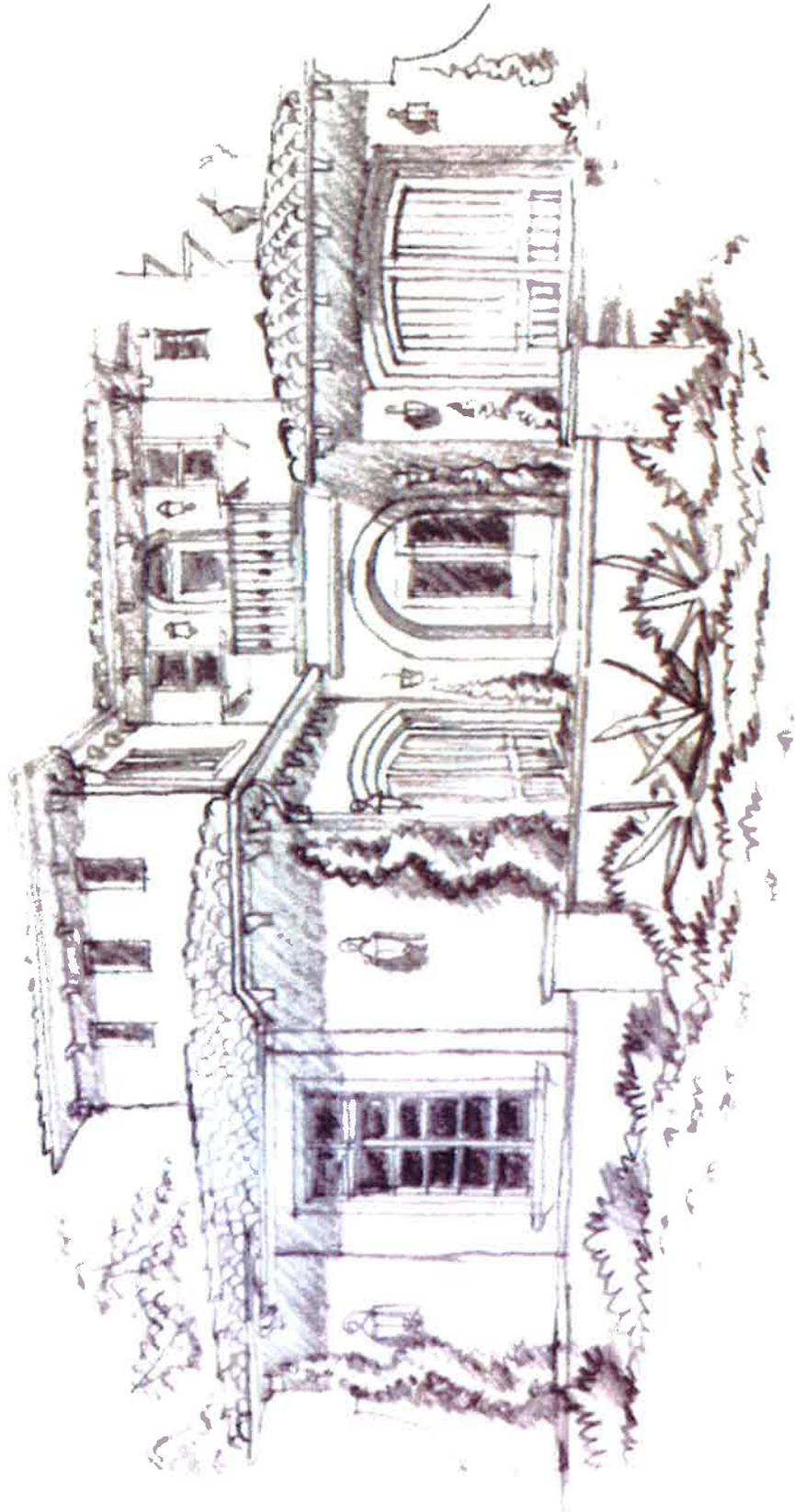
1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.

2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

THE CALIFORNIA VILLA

The Villa pays homage to Italian Renaissance architecture while introducing elements of California style. Simple, symmetrical, unornamented facades with decorative elements focused to high-visibility areas, such as doors and windows. Small details concentrated in high impact locations work to create a highly ordered style with classical elegance and restrained formality.

Being more formal and vertical in nature, the Villa adds an attractive contrast to some of the casual and eclectic styles within the neighborhood.



Style Elements	Minimum Elements (All Required on All Homes)	Enhanced Elements ^{1,2} (Minimum of 3 Required on All Homes)
Form	<ul style="list-style-type: none"> • Two-story form. • Simple hipped roof with a flat, symmetrical facade. 	<ul style="list-style-type: none"> • Full-width loggia with a formal and elegantly detailed colonnade.
Roof	<ul style="list-style-type: none"> • Low pitched roof (4:12 to 5:12). • Simple hipped roof. • Broadly overhanging (24" min) boxed eaves. • Barrel or S-shaped concrete tiles. 	<ul style="list-style-type: none"> • Decorative brackets at eaves. • Hipped roof with single-story projecting wings (i.e., porte-cochere or sunroom). • Decorative frieze.
Walls	<ul style="list-style-type: none"> • Stucco is the dominant exterior finish. 	<ul style="list-style-type: none"> • Yellow brick walls. • Horizontal rusticated base of stone or masonry.
Windows & Doors	<ul style="list-style-type: none"> • Formal window arrangement across full facade. • Symmetrical placement of windows. • Smaller windows on upper floors. • Classical door surrounds. 	<ul style="list-style-type: none"> • Full-length first-story windows with arches above. • Palladian window arrangements. • Precast concrete door and window surrounds. • Pedimented door surround with columns. • Arched entry door. • Pedimented windows.
Details	<ul style="list-style-type: none"> • Belt-course to accent second floor plate or window sills. 	<ul style="list-style-type: none"> • Roof-line balustrades. • Molded cornices. • Bracketed window cornices. • Molded precast concrete belt-course to accent second floor plate or window sills. • Shutters (louvered).

1. An additional 2 enhancements required on visible lots shown on Figure 4.1 for a total of 5 Enhanced Elements.
 2. An additional 3 enhancements required on corner visible lots shown on Figure 4.1 for a total of 6 Enhanced Elements.

4.5 ACTIVE ADULT

Russell Ranch will feature an active adult neighborhood within the community for residents seeking a living experience designed specifically for residents 55 and better. The active adult neighborhood will feature a private, gated entrance, its own private clubhouse amenity and will share access to the Russell Ranch trail system.

The Active Adult neighborhood will comply with the SFHD guidelines set forth in the Architecture section of this document.

4.6 THE TOWNHOMES

The Townhomes provide an opportunity to create an eclectic urban micro-neighborhood environment within Russell Ranch and there are certain defining elements that the Townhomes must exhibit. When designing townhomes, which are typically distinguished as being narrow in nature, the quantity, scale, and placement of architectural detail must be judicious to not overwhelm the scale of the building.

- The architectural style must be selected from the permitted architectural styles described in this chapter.
- To avoid dominant unbroken planes and create shadow lines, The Townhomes must provide vertical articulation at the front elevation.
- Varied setbacks for different portions of the home, such as garages, second floors, balconies, etc., are encouraged.
- Massing of forms must be established using the fundamental characteristics of the selected architectural style.

- Contemporary interpretations of the architectural styles permitted for The Townhomes are encouraged through the use of eclectic materials, such as metals (must be anti-reflective) and variations on traditional siding, asymmetrical roof pitches, and playful massing and use of color.



Townhome Concept Imagery

4.7 THE RECREATION CENTER

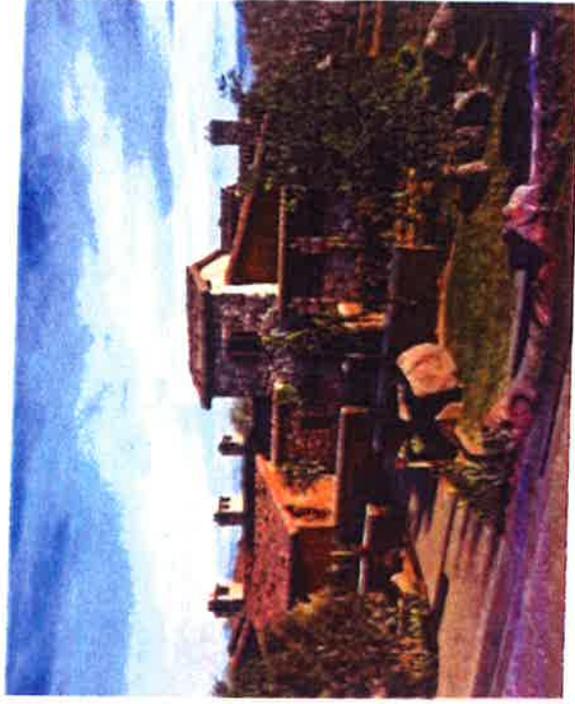
Russell Ranch will feature two recreation centers: one for the multigenerational portion of the neighborhood and one for the active adult portion of the neighborhood. The recreation centers will provide an opportunity for residents of the neighborhood to gather and enjoy a beautiful indoor-outdoor environment. The Recreation Centers will feature architecture that blends historic with contemporary to create a truly iconic neighborhood building that residents and visitors alike will admire. The design intent is to convey the impression of an historic structure with a contemporary addition.

The following defining characteristics are intended as a kit of parts and set of rules to influence the design of the Recreation Centers. A combination of these elements should be used to inform the design of the building (all of the elements are not required).

- Casual, asymmetrical form with a combination of gable and shed roof forms.

- Overgrouted stone as a significant wall material, utilized to present full massing elements. For example, an entire central mass of the structure may feature stone, with wings of a complementary material, such as stucco. This design solution gives the appearance of an original structure with wings added on over time.
- A clerestory roof form in the central portion of the structure is appropriate.
- Wall materials may be overgrouted stone or brick, smooth or imperfect smooth stucco or plaster, and glass.
- Accent materials include heavy beams, rusted metal, wrought iron, copper, and precast concrete.
- The primary roof should be barrel or S-tile, which may be boosted.
- The contemporary portion of the building may feature a metal standing seam roof.
- Rain chains should be used in place of traditional downspouts.
- Windows may be recessed and void of trim, or feature full window surrounds, which may be precast concrete or wood.
- Arched windows are appropriate as feature or accent windows.
- Exposed beams and structural connections should be celebrated as part of the design statement.
- Site structures, such as restrooms, cabanas, pool equipment rooms, and trash enclosures must complement the design of the primary structure.

- Mechanical and pool equipment must be screened from sight through integrated walls, landscaping, or a combination of both.



Recreation Center Concept Imagery

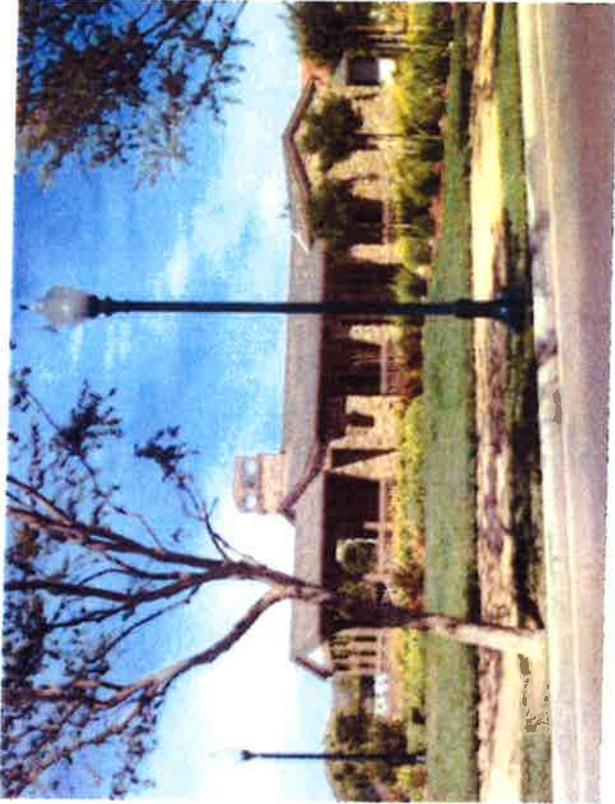
4.8 THE SCHOOL

Although it is recognized that school architecture is governed by functionality and a specialized kit of parts and set of rules, the elementary school located at Russell Ranch is encouraged to complement the surrounding neighborhood in its design. The following recommendations are encouraged for consideration in design of the school:

- The school is encouraged to draw inspiration from the approved architectural styles found in these Guidelines.
- A simplified interpretation of the selected architectural style is appropriate.
- Earth tone colors are encouraged in rich hues.
- Recommended wall materials include stucco, brick, horizontal siding, and board and batten siding.
- Standing seam metal is the recommended material for the roof in dark earth tone colors.



School Concept Imagery



PLANNING COMMISSION STAFF REPORT

PROJECT TITLE	The Shops at Folsom Ranch
PROPOSAL	Request for approval of a Vesting Tentative Parcel Map and Planned Development Permit for development of a 27,900-square-foot commercial shopping center
RECOMMENDED ACTION	Approve, based upon findings and subject to conditions
OWNER/APPLICANT	The Shops at Folsom Ranch, LLC/TK Consulting Inc.
LOCATION	The 5.9-acre project site, which is situated within the Folsom Plan Area, is located at the southeast corner of the intersection of East Bidwell Street and Alder Creek Parkway
ASSESSORS PARCEL NUMBERS	APN: 072-3190-044 and 072-3190-045
SITE CHARACTERISTICS	The project site, which is currently undeveloped, is elevated approximately 10-15 feet above the grade of the adjacent roadways (East Bidwell Street and Alder Creek Parkway). The topography of the site is characterized by gently rolling mounds covered in non-native and naturalized grasslands.
GENERAL PLAN DESIGNATION	GC (General Commercial)
SPECIFIC PLAN DESIGNATION	SP-GC-PD (Specific Plan, General Commercial, Planned Development District)
ADJACENT LAND USES/ZONING	North: Alder Creek Parkway with Undeveloped Commercial Property (SP-GC-PD) beyond South: Future Old Ranch Way with Undeveloped Residential Property (SP-MLD-PD) Beyond

East: Undeveloped Residential Property (SP-MLD-PD) with Westwood Drive Beyond

West: East Bidwell Street with Undeveloped Commercial Property (SP-GC-PD) Beyond

PREVIOUS ACTION

City Council Approval of the Folsom Plan Area Specific Plan and Tier 1 Development Agreement in 2011

City Council Approval of Folsom Plan Area Specific Plan Public Facilities Financing Plan in 2014

City Council Approval of First Amended and Restated Tier 1 Development Agreement in 2014

City Council Approval of the South of Highway 50 Backbone Infrastructure Project in 2015

City Council Approval of the Westland/Eagle General Plan Amendment and Specific Plan Amendment in 2015

City Council Approval of a Tentative Parcel Map, Vesting Tentative Subdivision Map, and Planned Development Permit for development of a 111-unit single-family residential subdivision (Enclave at Folsom Ranch) in 2016

City Council Approval of a Modification of a Condition of Approval and Extension of the Tentative Parcel Map, Vesting Tentative Subdivision Map, and Planned Development Permit associated with the Enclave at Folsom Ranch Subdivision in 2017

Recordation of the Large Lot Parcel Map (created the two subject parcels) associated with the Enclave at Folsom Ranch project on June 29, 2018

FUTURE ACTION

Approval of the Parcel Map, Approval of Improvement Plans, and Issuance of Grading and Building Permits

APPLICABLE CODES

FMC 16.12, Maps Required
FMC 16.24, Parcel Maps
FMC 17.22, Commercial Land Use Zones
FMC 17.37, Specific Plan District
FMC 17.38, Planned Development District
FMC 17.59, Signs
Folsom Plan Area Specific Plan (FPASP)
Subdivision Map Act

ENVIRONMENTAL REVIEW

An Environmental Impact Report has been certified for the Folsom Plan Area Specific Plan (FPASP) project in accordance with the California Environmental Quality Act (CEQA). This project is consistent with the FPASP and the Westland Eagle Specific Plan Amendment to the FPASP. This project meets the criteria in Government Code Section 65457 and of CEQA Guidelines Section 15182, which exempts the project from further review. The project also qualifies for streamlined environmental review under Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183

ATTACHED REFERENCE MATERIAL

1. Vicinity Map
2. Vesting Tentative Parcel Map, dated October 25, 2018
3. Preliminary Site Plan, dated October 25, 2018
4. Preliminary Grading and Drainage Plan, dated October 25, 2018
5. Preliminary Utility Plan, dated October 25, 2018
6. Preliminary Landscape Plan and Tree Shading Exhibit, dated October 5, 2018
7. Preliminary Site Furnishings and Courtyard Exhibit, dated September 4, 2018
8. Onsite Bicycle and Pedestrian Circulation Exhibit, dated October 5, 2018
9. Preliminary Off-Site Roadway Configuration Exhibit, dated October, 2018
10. Preliminary Off-Site Utility Exhibit, dated October, 2018
11. Preliminary On-Site and Off-Site Grading Exhibit, dated October, 2018
12. Building Elevations, dated August 31, 2018 and October 5, 2018
13. Building Floor Plans, dated October 5, 2018
14. Building Renderings and Perspectives, dated August 31, 2018 and October 5, 2018
15. Color and Materials Board, dated August 31, 2018
16. Roof Screening and Lighting Exhibit, dated August 31, 2018
17. Uniform Sign Criteria for The Shops at Folsom Ranch, dated August 31, 2018
18. CEQA Exemption and Streamlining Analysis and Checklist (Bound Separately)
19. Mitigation Monitoring and Reporting Program for Westland-Eagle Specific Plan Amendment
20. Mitigation Monitoring and Reporting Program for Highway 50 Backbone Infrastructure Project

- 21. Mitigation Monitoring and Reporting Program for Off-Site Water Facility Alternative
- 22. Site Photographs

PROJECT PLANNER

Steve Banks, Principal Planner

BACKGROUND

The 5.9-acre project site, which is situated within the Folsom Plan Area, is located at the southeast corner of the intersection of East Bidwell Street and Alder Creek Parkway. Historically, mining was the dominant historical theme in the project area and in the surrounding lands. The region, later known as the Folsom Mining District, was extensively placer mined during the Gold Rush era. Since the early 1900's, the subject property and the surrounding areas have been primarily utilized for cattle grazing and associated activities. When construction of the Phase 1 Backbone Infrastructure Improvement project commenced within the Folsom Plan Area in April of 2017, the northern and western borders of the project site were disturbed by grading activities to allow for construction of Alder Creek Parkway and reconstruction/realignment of East Bidwell Street. The remainder of the project site that was not disturbed features gently rolling mounds covered in non-native and naturalized grasslands.

The proposed project site is part of the approved Folsom Plan Area Specific Plan (FPASP), which is a comprehensively planned community that proposes new development based upon principles of "Smart Growth" and Transit Oriented Development. Consistent with these principles, the FPASP includes a mix of residential, commercial, employment and public uses complemented by recreational amenities including a significant system of parks and open space, all within close proximity to one another and interconnected by a network of "Complete Streets", trails and bikeways consistent with the SACOG Blueprint Principles and the requirements of SB 375 (Sustainable Communities and Climate Protection Act). The FPASP includes 11,461 residential units at various densities on approximately 1,625 acres; 320 acres designated for commercial and industrial use; 275 acres designated for public/quasi-public uses, elementary/middle school/high schools, and community/neighborhood parks; and 1,067 acres for open-space areas.

On September 22, 2015, the City Council approved an Addendum to the Folsom Plan Area Specific Plan EIR/EIS, a General Plan Amendment, a Specific Plan Amendment, and Amendment No. 1 to the First Amended and Restated Tier 1 Development Agreement for the Westland-Eagle project. The Westland-Eagle project included a significant reduction in the amount of retail commercial land area and an increase in the number of allowed residential dwelling units within the Folsom Plan Area. The net result of the aforementioned land use modifications was a decrease of 1,445,710 square feet of commercial building area and an increase of 922 residential units within the Plan Area. In addition, the Westland-Eagle project contained modifications to the FPASP including: elimination of the Entertainment Overlay Zone, relocation of more intense land uses toward Alder Creek Parkway, strengthening focus of the town center, relocation of Alder Creek Parkway, and realignment of Old Placerville Road. The proposed project is located within the previously approved Westland-Eagle project area.

On November 8, 2016, the City Council approved a Tentative Parcel Map, Vesting Tentative Subdivision Map, and Planned Development Permit for development of a 111-unit single-family residential subdivision (Enclave at Folsom Ranch) on a 14.7-acre site situated within a larger 75.3-acre portion of the Westland-Eagle project area generally located south of U.S. Highway 50, north of White Rock Road, east of East Bidwell Street, and west of Placerville Road. The aforementioned

Tentative Parcel Map subdivided an existing 75.3-acre parcel into four individual parcels for future sale and development including the subject parcels. On June 29, 2018, a Parcel Map creating the four aforementioned parcels (including the two subject parcels) was recorded with Sacramento County. The improvement plans associated with the Enclave at Folsom Ranch Subdivision are currently in the process of being reviewed by the Community Development Department.

APPLICANT’S PROPOSAL

The applicant, TK Consulting Inc., is requesting approval of a Vesting Tentative Parcel Map and Planned Development Permit for development of a 27,900-square-foot commercial shopping center (The Shops at Folsom Ranch) on a 5.9-acre site located within the Folsom Plan Area at the southeast corner of the intersection of East Bidwell Street and Alder Creek Parkway. The Vesting Tentative Parcel Map is proposed to subdivide the 5.9-acre project site (which currently consists of two parcels) into five (5) individual parcels for development of commercial uses. The Planned Development Permit is proposed for development of five commercial pad buildings within the 27,900-square-foot shopping center. The five proposed single-story pad buildings, which range from 1,900 to 9,000 square feet in size, will feature two retail/restaurant pad buildings, two fast-food restaurant pad buildings with drive-thru service, a large restaurant pad building, and an automotive fuel station with car wash facility. In terms of building design, the proposed project features a contemporary California Ranch Style architectural theme that is combined with modern building materials and earth tone colors. The following table provides specific details on each of the proposed buildings within the shopping center:

The Shops at Folsom Ranch		
Land Use	Description	Size
Retail/Restaurant	Building S-1	5,000 S.F.
Retail/Restaurant	Building S-2	5,000 S.F.
Restaurant	Building P-2	9,000 S.F.
Fast-Food Restaurant with Drive-Thru	Building P-3	1,900 S.F.
Fast-Food Restaurant with Drive-Thru	Building P-4	4,000 S.F.
Automotive Fuel Station with Car Wash	Building P-1	3,000 S.F.
Totals		27,900 S.F.

Vehicle access to the project site will be provided by four new project driveways including two driveways located on the south side of Alder Creek Parkway, one driveway located on the east side of East Bidwell Street, and one driveway located on the north side of Old Ranch Way. The project driveway on East Bidwell Street and the westernmost project driveway on Alder Creek Parkway are considered the primary access points to the project site, while the easternmost project driveway on Alder Creek Parkway and the project driveway on Old Ranch Way will serve as secondary access points. It is important to note that the easternmost driveway on Alder Creek Parkway is necessary to accommodate commercial service vehicles and fuel truck deliveries to the proposed automotive fuel station. Internal vehicle circulation is facilitated by a series of internal drive aisles, which provide access throughout the project site. Bicycle and pedestrian circulation is accommodated by a combination of Class II and Class III bicycle lanes, street-separated sidewalks, street-attached sidewalks, internal pedestrian walkways, and a designated pedestrian pathway connection to the future Enclave at Folsom Subdivision directly to the east of the project site.

Proposed on-site improvements include underground utilities, drainage improvements, retaining walls, sound walls, driveways, drive aisles, parking stalls, sidewalks, pedestrian walkways, curbs, gutters, outdoor patios, site furnishings, site lighting, site landscaping, monument signs, and trash/recycling enclosures. Proposed off-site improvements include interim intersection improvements at East Bidwell Street/Alder Creek Parkway, East Bidwell Street/Old Ranch Way, and Alder Creek Parkway/Westwood Drive, frontage improvements along East Bidwell Street, Alder Creek Parkway, and Old Ranch Way, and construction of Old Ranch Way between East Bidwell Street and the eastern project boundary. Additional off-site improvements include constructing sewer infrastructure within Old Ranch Way from the eastern project boundary to East Bidwell Street, constructing a hydromodification basin (Hydromodification Basin No. 19) along East Bidwell Street to the south of the project site, constructing storm drain infrastructure within Alder Creek Parkway from East Bidwell Street to eastern project boundary, constructing storm drain infrastructure within East Bidwell Street from Old Ranch Way to Hydromodification Basin No. 19, constructing storm drain improvements within Old Ranch Way from East Bidwell Street to eastern project boundary, installing potable and recycled water infrastructure at numerous locations, and extending dry utilities within Alder Creek Parkway from Placerville Road to East Bidwell Street.

GENERAL PLAN AND SPECIFIC PLAN CONSISTENCY

In 2011, the City of Folsom adopted a General Plan Amendment for the circulation and land use designations, as well as the Folsom Plan Area Specific Plan (FPASP), which is designed to guide and regulate development for the area south of U.S. Highway 50. In 2015, the City Council approved a General Plan Amendment and Specific Plan Amendment for the Westland-Eagle project (encompasses The Shops at Folsom Ranch project site), which resulted in a modification and redistribution of various land use categories. The adopted General Plan land use designation for the project site is GC (General Commercial) and the adopted Specific Plan land use designation is SP-GC-PD (Specific Plan, General Commercial, Planned Development District). The proposed project is consistent with both the General Plan land use and Specific Plan land use designations, as retail and commercial uses (including retail shops, restaurants, restaurants with drive-thru service, and fuel stations with car washes) are identified as permitted land uses for this particular site. The proposed project also complies with the newly established floor area-ratio (FAR) standard established by the General Plan (2035) for the GC land use category by having an overall FAR of 0.11 whereas the FAR maximum target is 0.50.

The recently approved City of Folsom General Plan (2035) outlines a number of goals, policies, and implementation programs designed to guide the physical, economic, and environmental growth of the City. State law requires the Folsom Plan Area Specific Plan to be consistent with the General Plan including its goals and policies. In addition, new development must not only meet the unique requirements of the Folsom Plan Area Specific Plan, but also the broader policies set forth in the General Plan. The proposed project is consistent with the General Plan goals and policies as well as the Folsom Plan Area Specific Plan objectives and policies as outlined and discussed below:

APPLICABLE GENERAL PLAN GOALS AND POLICIES

GP GOAL LU 7.1 (Land Use/Commercial Centers)

Provide for a commercial base of the City to encourage a strong tax base, more jobs within the City, a greater variety of good and services, and businesses compatible with Folsom's quality of life.

GP POLICY LU 7.1.1 (Standards for Commercial Uses)

Require new commercial uses to be subject to design and parking standards relative to building configuration, building design, parking, signage, and landscaping.

The proposed project is consistent with this policy in that the project meets all applicable development standards (lot size, floor area ratio, building setbacks, landscape coverage, distance between buildings, and parking) established by the Folsom Plan Area Specific Plan for the General Commercial land use category. In addition, the proposed shopping center has been designed to reflect an architectural style (contemporary California Ranch Style) that is complimentary to the Folsom Plan Area (Folsom Ranch, Central District Design Guidelines).

GP POLICY LU 7.1.3 (Commercial Expansion)

Support the expansion of Folsom's commercial sector to meet the needs of Folsom residents, employees, and visitors.

The proposed project is consistent with this policy in that it represents the first retail commercial development to be constructed within the Folsom Plan Area. In addition, the proposed project will facilitate the expansion of Folsom's commercial sector by introducing a range of services (retail stores, restaurants, fuel station, car wash) that will serve the City's expanding population base, particularly those residents located in the Folsom Plan Area where these types of services do not currently exist.

GP GOAL LU 9.1 (Land Use/Community Design)

Encourage community design that results in a distinctive, high-quality built environment with a character that creates memorable places and enriches the quality of life of Folsom's residents.

GP POLICY LU 9.1.1 (Combine Driveways)

Encourage property owners in retail corridors to reduce the number of driveways along arterial roads. When possible, property owners should cooperate through reciprocal access and parking or similar agreements linking parking lots to minimize traffic congestion on the arterial road.

The proposed project is consistent with this policy in that the project involves development of an integrated shopping center that will feature common driveways and provide shared parking among the various buildings, thereby minimizing traffic congestion on the adjacent arterial roadways (East Bidwell Street and Alder Creek Parkway). The project is also conditioned to remove the Eastern Project Driveway on Alder Creek Parkway in the event that the fuel station is not developed within the shopping center, thereby reducing the number of driveways on Alder Creek Parkway (major arterial roadway) and reducing the number of driveways in close proximity to the intersection of East Bidwell Street and Alder Creek Parkway. In addition, the project is required to execute a reciprocal access and parking agreement for common use of all project driveways and parking spaces within the shopping center.

GP POLICY LU 9.1.8 (Cool Paving)

Identify opportunities to use cool paving materials and consider the use of concrete or permeable pavement instead of asphalt for streets and trails where feasible.

The proposed project is consistent with this policy in that concrete (cool paving) will be utilized instead of asphalt within specific areas throughout the project site.

GP POLICY LU 9.1.10 (Renewable and Alternative Energy Generation Systems)

Require the use of solar, wind, and other on-site renewable energy generation systems as part of the design of new planned developments.

The proposed project is consistent with this policy in that a photovoltaic (PV) solar system will be placed on the rooftop of a 9,000-square-foot building (Building P2) within the shopping center, resulting in an on-site renewable energy system that will offset energy demand for the building.

GP GOAL M 1.1 (Mobility/General Mobility)

Provide a comprehensive, integrated, and connected network of transportation facilities and services for all modes of travel that also incorporates emerging transportation technologies and services to increase transportation system efficiency.

GP POLICY M 1.1.1 (Complete Streets)

Develop its streets to serve the needs of all users, including bicyclists, public transit users, children, seniors, persons with disabilities, pedestrians, motorists, and movers of commercial goods.

The proposed project is consistent with this policy in that shopping center is designed to accommodate various modes of transportation including buses, cars, bicycles, and pedestrians. Specifically, a future transit corridor that will accommodate bus rapid transit is located adjacent to the project site along Alder Creek Parkway, Class II and Class III lanes/routes are provided along East Bidwell Street, Alder Creek Parkway and Old Ranch Way respectively, pedestrian sidewalks and walkways are being constructed along the project's street frontages and internally, and a designated pedestrian connection is being provided from the project site directly to the future Enclave at Folsom Ranch Subdivision to the east of the project site.

GP GOAL M 2.1 (Mobility/Pedestrians and Cyclists)

Maintain and expand facilities and programs that encourage people to walk and bike in safety and comfort, and support the lifestyle and amenities that Folsom residents value.

GP POLICY M 2.1.3 (Pedestrian and Bicycle Linkages in New Development)

Require developers to provide a system of sidewalks, trails, and bikeways that link all land uses, provide accessibility to parks and schools, and connect to all existing or planned external street and trail facilities.

The proposed project is consistent with this policy in that Class II bicycle lanes will be constructed along the project's frontage with Alder Creek Parkway and East Bidwell Street and a Class III bicycle route will be established along the frontage of Old Ranch Way. Street sidewalks will also be constructed along the project's frontage with Alder Creek Parkway, East Bidwell Street, and Old Ranch Way. In addition, an enhanced pedestrian walkway will provide access from the project site to a future dedicated pedestrian connection that provides entry to the future Enclave at Folsom Ranch Subdivision located to the east of the project site.

GP GOAL M 3.1 (Mobility/Transit)

Support and maintain a comprehensive, safe, and integrated transit system that responds to the needs of all residents and allow frequent and convenient travel throughout the city and region.

GP POLICY M 3.1.1 (Access to Public Transit)

Strive to ensure that all residents have access to safe and convenient public transportation options.

The proposed project is consistent with this policy in that a future transit corridor is located adjacent to the project site along Alder Creek Parkway. The transit corridor is expected to accommodate a future bus rapid transit route, which will accommodate the public transportation needs of the proposed project (employees and customers) and future development in the project area.

GP GOAL M 4.1 (Mobility/Vehicle Traffic and Parking)

Ensure a safe and efficient network of streets for cars and trucks, as well as provide an adequate supply of vehicle parking.

GP POLICY M 4.1.3 (Level of Service)

Strive to achieve at least a traffic Level of Service “D” (or better) for local streets and roadways throughout the City. In designing transportation improvements, the City will prioritize use of smart technologies and innovative solutions that maximize efficiencies and safety while minimizing the physical footprint. During the course of Plan buildout, it may occur that temporarily higher Levels of Service result where roadway improvements have not been adequately phased as development proceeds. However, this situation will be minimized based on annual traffic studies and monitoring programs. Staff will report to the City Council at regular intervals via the Capital Improvement Program process for the Council to prioritize projects integral to achieving Level of Service D or better.

The proposed project is consistent with this policy in that no project-related reduction in intersection level of service is projected in the AM or PM peak hours under Existing Plus Project Conditions (2018), Existing Plus Approved Project Conditions (2023), and Cumulative Plus Project Conditions (2036) at the five study intersections (East Bidwell Street/Alder Creek Parkway, East Bidwell Street/Old Ranch Way, Alder Creek Parkway/Eastern Project Driveway, Alder Creek Parkway/Western Project Drive, East Bidwell Street/Project Driveway, and Old Ranch Way/Project Driveway). Furthermore, the proposed project will be constructing a number of off-site roadway improvements (East Bidwell Street/Alder Creek Parkway intersection improvements, Alder Creek Parkway/Westwood Drive intersection improvements, East Bidwell Street/Old Ranch Way intersection improvements, Alder Creek Parkway improvements, East Bidwell Street improvements, and Old Ranch Way improvements) that will reduce the level of delay at the intersection of East Bidwell Street and Alder Creek Parkway (left-turn movement from Alder Creek Parkway onto East Bidwell Street) under the Existing Conditions Plus Project conditions. A detailed discussion of the project’s traffic-related impacts are contained within the Traffic/Access/Circulation portion of this staff report.

GP GOAL M 4.2 (Mobility/Vehicle Traffic and Parking)

Provide and manage a balanced approach to parking that meets economic development and sustainability goals.

GP POLICY M 4.2.4 (Electric Vehicle Charging Stations)

Encourage the installation of electric vehicle charging stations in parking spaces throughout the city, prioritizing installations at multi-family residential units.

The proposed project is consistent with this policy in that thirteen (13) electric vehicle parking spaces/charging stations (6% of the total parking spaces within the shopping center) will be provided within the shopping center. The electric vehicle parking spaces/charging stations will be evenly distributed throughout the parking lot area of the shopping center.

GP GOAL M 7.1 (Transportation Funding)

Provide sufficient funding to construct, maintain, and operate transportation facilities and services needed to achieve the City’s mobility goals.

GP POLICY M 7.1.1 (New Development)

Require new development to contribute towards the construction of offsite facilities and provision of services to achieve the City’s mobility goals.

The 2011 Folsom Plan Area Specific Plan Environmental Impact Report/Environmental Impact Statement included not only a detailed analysis of traffic-related impacts within the Plan Area, but also an evaluation of traffic-related impacts on the surrounding communities. In total, there are fifty-five (55) traffic-related mitigation measures associated with development of the FPASP, which are included as conditions of approval for the proposed project. Many of these mitigation measures are expected to reduce traffic impacts to East Bidwell Street. Specific improvements planned for the East Bidwell Street/Iron Point Road Intersection area include but are not limited to; elimination of the U-turn movement on northbound East Bidwell Street at Iron Point Road, installing signage on Placerville Road directing motorists south to Alder Creek Parkway to access U.S. Highway 50, and modifying the right-turn-lane on Iron Point Road at the approach to East Bidwell Street to accommodate a free-flowing movement. Included among the mitigation measures are requirements to; fund and construct roadway improvements within the Plan Area, pay fair-share contribution for construction of improvements north of U.S. Highway 50, participate in the City’s Transportation System Management Fee Program, and Participate in the U.S. Highway 50 Corridor Transportation Management Association. The proposed project is subject to all traffic-related mitigation measures required by the 2011 FPASP EIR/EIS and the Westland-Eagle Specific Plan Amendment Addendum to the Folsom Plan Area Specific Plan Final EIR/EIS, thus it is consistent with this policy.

GP GOAL EP 5.1 (Economic Prosperity/Retail Development)

Maintain and expand retail and services to meet local and regional demands and generate tax revenues for City operations.

GP POLICY EP 5.1.1 (Diverse Retail)

Encourage a diverse mix of community and regional retail options to serve Folsom and surrounding communities.

The proposed project is consistent with this policy in that proposed shopping center will include a diverse mixture of retail and commercial services including but not limited to retail shops, sit-down restaurants, fast-food restaurants, a coffee shop, a fuel station, and a car wash.

GP GOAL SN 6.1 (Noise)

Protect the citizens of Folsom from the harmful effects of exposure to excessive noise and to protect the economic base of Folsom by preventing the encroachment of incompatible land uses within areas affected by existing noise-producing uses.

GP POLICY SN 6.1.4 (Noise and Project Review)

Develop, maintain, and implement procedures to ensure that requirements imposed pursuant to the findings of an acoustical analysis are implemented as part of the project review and building permit processes. The appropriate time for requiring an acoustical analysis would be as early in the project review process as possible so that noise mitigation may be an integral part of the project design.

The proposed project is consistent with this policy in that an Environmental Noise Assessment (Assessment) was prepared for the proposed project on May 22, 2018. The Assessment evaluated potential noise impacts generated by the commercial uses associated with the proposed project and the potential impact on future residential development in the project area. Potential project-related noise sources that were evaluated included restaurant drive-thru speakers, car wash equipment, and mechanical equipment. The Assessment concluded that the aforementioned noise sources associated with the proposed project would not exceed the applicable City of Folsom noise standards. However, to further ensure that the proposed project will not impact future nearby residential uses, the Assessment recommended of number of measures be incorporated into the project design including construction of a six-foot-tall masonry wall along the project's eastern boundary, locating car vacuum stalls on the west side of the car wash building, equipping the car wash with an automatic door that closes during car washes, and screening of all rooftop mechanical equipment.

APPLICABLE FOLSOM PLAN AREA SPECIFIC PLAN OBJECTIVES AND POLICIES

SP OBJECTIVE 4.2 (Land Use)

Locate commercial centers, public buildings, parks, and schools within walking distance of residential neighborhoods.

SP POLICY 4.12

Commercial and office areas should be accessible via public transit routes, where feasible.

The proposed project is consistent with this policy in that the project site is located adjacent to a future transit corridor that runs along Alder Creek Parkway from Prairie City Road to White Rock Road.

SP POLICY 4.13

The Plan Area land use plan should include commercial, light industrial/office park, and public/quasi-public land uses in order to create employment.

The proposed project is consistent with this policy in that the project site has a Specific Plan land use designation of SP-GC (Specific Plan, General Commercial) as shown on the Folsom Plan Area Specific Plan Land Use Designations Diagram (Figure 4.3). In addition, the proposed project will create employment opportunities through development of a 27,900-square-foot shopping center.

SP OBJECTIVE 7.1 (Circulation/General)

Consistent with the California Complete Streets Act of 2008 and the Sustainable Communities and Climate Protection Act (SB 375), create a safe and efficient circulation system for all modes of travel.

SP OBJECTIVE 7.3 (Circulation/General)

Encourage non-vehicular travel options by providing sidewalks, trails, and bikeway connectivity between neighborhoods and destination points.

SP POLICY 7.1

The roadway network in the Plan Area shall be organized in a grid-like pattern of streets and blocks, except where topography and natural features make it infeasible, for the majority of the Plan Area in order to create neighborhoods that encourage walking, biking, public transit, and other alternative modes of transportation.

Consistent with the requirements of the California Complete Streets Act, the FPASP identified and planned for hierarchy of connect “complete streets” to ensure that pedestrian, bike, bus, and automobile modes are travel are designed to have direct and continuous connections throughout the Plan Area. Every option, from regional connector roadways to arterial and local streets, has been carefully planned and designed. Recent California legislation to reduce greenhouse gas emissions (AB 32 and SB 375) has resulted in an increased market demand for public transit and housing located closer to service needs and employment centers. In response to these changes, the FPASP includes a regional transit corridor that will provide public transportation links between the major commercial, public, and multi-family residential land uses in the Plan Area. As shown in the various exhibits attached to this staff report, the proposed project has been designed with multiple modes of transportation options consistent with the approved FPASP circulation plan. In addition, the proposed project provides non-vehicular connectivity through the construction of pedestrian sidewalks, pedestrian walkways, and bicycle lanes.

SP OBJECTIVE 7.5 (Circulation/Roadway Classification)

Provide multiple and direct street routing based on a traditional rectilinear both macro- and micro-level grid patterns of street in the town center, mixed use neighborhood centers, multi-family residential neighborhoods, and single-family high density residential neighborhoods.

SP POLICY 7.8

Roadway improvements shall be constructed to coincide with the demands of new development, as Required to satisfy minimum level of service standards.

SP POLICY 7.8A

Concurrent with development of the SP-RC and SP-GC parcels located at the intersection of East Bidwell Street and Alder Creek Parkway, the following roadway improvements will be constructed:

- Alder Creek Parkway from Prairie City Road to East Bidwell Street
- East Bidwell Street from White Rock Road to U.S. Highway 50
- Rowberry Road (including overcrossing of U.S. Highway 50)

The proposed project is consistent with these policies in that the project will be constructing various roadway improvements to the following street segments and intersections:

- Alder Creek Parkway from East Bidwell Street east to Westwood Drive
- East Bidwell Street from Alder Creek Parkway to Old Ranch Way
- Intersection of East Bidwell Street and Alder Creek Parkway
- Intersection of Alder Creek Parkway and Westwood Drive
- Intersection of East Bidwell Street and Old Ranch Way

The proposed project is also subject to all traffic-related mitigation measures required by the 2011 FPASP EIR/EIS and the Westland-Eagle Specific Plan Amendment Addendum to the Folsom Plan Area Specific Plan Final EIR/EIS, many of which will provide funding for future improvements to Alder Creek Parkway and East Bidwell Street. In addition, the proposed project is consistent with these policies in that the Traffic Impact Analysis prepared for this project determined that no project-related change in intersection level of service is projected in the AM or PM peak hours under Existing Plus Project Conditions (2018), Existing Plus Approved Project Conditions (2023), and Cumulative Plus Project Conditions (2036) at the five study intersections (East Bidwell Street/Alder Creek Parkway, East Bidwell Street/Old Ranch Way, Alder Creek Parkway/Eastern Project Driveway, Alder Creek Parkway/Western Project Drive, East Bidwell Street/Project Driveway, and Old Ranch Way/Project Driveway.

SP OBJECTIVE 7.8 (Circulation/Public Transit)

Promote the use of public transit in the Plan Area by providing a safe, secure, and cost effective transit system that provides frequent and convenient transit service to local and regional destinations.

SP POLICY 7.10

Consistent with the most recent update of the RT master plan and the Plan Area Master Plan, a transit corridor shall be provided through the Plan Area for future regional “Hi-Bus” service (refer to Figure 7.29 and the FPASP Transit Master Plan). Sufficient right-of-way shall be dedicated for the transit corridor as described in Section 7.3 and Figures 7.2, 7.3, 7.14, and 7.15.

The proposed project is consistent with this policy in that sufficient right-of-way is provided within Alder Creek Parkway to accommodate development of the future transit corridor.

LAND USE COMPATIBILITY

As noted earlier within this report, the 5.9-acre project site is located within the Folsom Plan Area at the southeast corner of the intersection of East Bidwell Street and Alder Creek Parkway. The project site is currently surrounded by vacant and undeveloped property. However, as shown on the approved FPASP Land Use Plan and Zoning Diagram (FPASP, Figure 4.1), the project site will eventually be bound by single family residential development to the east (approved Enclave at Folsom Ranch Subdivision), commercial development across East Bidwell Street to the west, commercial development across Alder Creek Parkway to the north, and multi-family, low-density residential development across future Old Ranch Way to the south. As described above, the project site is located in an area that will be predominantly commercial in nature when it is fully developed. In addition, the proposed project is consistent with the land use designations in the FPASP and meets all of the policies and regulations contained therein. Based on the aforementioned information, staff has determined that the proposed project is compatible with future planned land uses in the project area.

VESTING TENTATIVE PARCEL MAP

A Vesting Tentative Parcel Map is proposed to subdivide the existing 5.9-acre project site (consists of two existing parcels that are 2.5 and 3.4-acres in size respectively) into five (5) individual parcels for development of commercial uses. The proposed parcels, which will be developed with a total of six commercial pad buildings, include numerous site improvements including but not limited to underground utilities, drainage improvements, driveways, drive aisles, parking, spaces, sidewalks,

pedestrian walkways, retaining walls, sound walls, landscaping, site lighting, and trash/recycling enclosures. The following table provides a description of each of the proposed parcels.

The Shops at Folsom Ranch Parcel Summary				
Parcel #	Size	GP Designation	SP Designation	Land Use
1	0.9-Acres	GC	SP-GC-PD	General Commercial
2	0.6-Acres	GC	SP-GC-PD	General Commercial
3	1.6-Acres	GC	SP-GC-PD	General Commercial
4	1.3-Acres	GC	SP-GC-PD	General Commercial
5	1.5-Acres	GC	SP-GC-PD	General Commercial
Totals	5.9-Acres			

The Shops at Folsom Ranch Shopping Center is proposed to be an integrated commercial development in which each of the parcels will share common access driveways, parking, and improvements. Staff recommends that the applicant dedicate reciprocal access easements for driveway access, parking, landscaping, lighting, sewer, water, and fire protection systems. Condition No. 45 is included to reflect this requirement. Staff has determined that the proposed Vesting Tentative Parcel Map complies with all City requirements, as well as with the requirements of the State Subdivision Map Act.

Normally, a division of land involving five or more parcels requires a subdivision map. However, an exception applies to projects zoned for industrial or commercial development that have approved access to a public street or highway, when the governing body has approved street alignments and widths. (Government Code § 66426(c); Folsom Municipal Code § 16.12.020(A)(3). In those cases, a parcel map, rather than a subdivision map, is required. This project qualifies for that exception because it is zoned General Commercial, has approved access to a public street, and the City Council has approved the street alignments and widths.

PLANNED DEVELOPMENT PERMIT

In general, the purpose of the Planned Development Permit process is to allow greater flexibility in the design of integrated developments than otherwise possible through strict application of land use regulations. The Planned Development Permit process is also designed to encourage creative and efficient uses of land. However, in this particular case, the proposed project is not seeking to modify or change any of the existing development standards previously approved for the Folsom Plan Area Specific Plan. The applicant is simply requesting approval of a Planned Development Permit as the Folsom Plan Area Specific Plan requires development of the subject site obtain approval of a Planned Development Permit from the Planning Commission. The applicant’s intent is to request approval of a Planned Development Permit for development of a 27,900-square-foot retail commercial shopping center. In reviewing the applicant’s request for approval of a Planned Development Permit, staff considered a variety of factors including existing development standards, traffic/access/circulation, parking, noise, site lighting, site landscaping, trash/recycling, signage, and architecture/design as outlined below.

Development Standards

The proposed project complies with all of the development standards established by the Folsom Plan Area Specific Plan for the General Commercial land use category as listed in the table below.

Folsom Plan Area Specific Plan General Commercial (SP-GC) Development Standards					
	Floor Area Ratio (Max)	Front Yard Setback	Side Yard Setbacks	Rear Yard Setback	Building Height limit
General Commercial Standard	0.25	20 feet	20 feet	20 feet	50 feet
Proposed Project	0.11	35 feet	30/80 feet	51 feet	31 feet

As shown in the table above, the proposed project meets all of the development standards established for the General Commercial land use category including floor area ratio, building setbacks, and building height. Parking and landscape requirements are addressed separately within the parking and landscape sections of this staff report.

Traffic/Access/Circulation

Existing Roadway Network:

The 5.9-acre project site is located at the southeast corner of the intersection of Alder Creek Parkway and East Bidwell Street. Regional access to the project site is provided by U.S. Highway 50 to the north via East Bidwell Street, White Rock Road to the south via East Bidwell Street, and Placerville Road to the east via Alder Creek Parkway. Direct access to the project site is provided by two new driveways on Alder Creek Parkway, one new driveway on East Bidwell Street, and one new driveway on Old Ranch Way. Existing roadways in the project area include Alder Creek Parkway and East Bidwell Street. Alder Creek Parkway is currently a two-lane, median-divided roadway with bicycle lanes that from extends from East Bidwell Street east to Placerville Road. In the project vicinity East Bidwell Street is a two-lane, non-divided roadway with bicycle lanes that extends from U.S. Highway 50 south to White Rock Road. The westbound approach to the intersection of Alder Creek Parkway and East Bidwell Street from Alder Creek Parkway, which is currently Stop-Sign controlled, includes two lanes; a left-turn pocket and a through lane that currently only allows right-turn movements.

Proposed Roadway Network:

In order to serve the proposed 27,900-square-foot shopping center, the applicant is proposing to construct a number of roadway improvements as shown on the submitted Off-Site Roadway Configuration Exhibit (Attachment 9) including but not limited to modifications to the East Bidwell/Street Alder Creek Parkway intersection, construction of Old Ranch Way, realigning a portion of East Bidwell Street, constructing a raised median in East Bidwell Street, providing U-turn and right-turn pockets on the East Bidwell Street approach to Alder Creek Parkway, providing a left-turn pocket into Old Ranch Way from southbound East Bidwell Street, and widening eastbound Alder Creek Parkway to provide two through lanes plus turn pockets. Staff recommends that the owner/applicant construct the following roadway improvements as depicted on the Off-Site Roadway Configuration Exhibit to the satisfaction of the Community Development Department (Condition No. 14):

- East Bidwell Street

Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure East Bidwell Street as follows:

- On the west side of the existing East Bidwell Street fronting the Project, construct a raised median and one new southbound lane. Realign southbound traffic to be on the west side of the median. The raised median shall extend south from Alder Creek Parkway, past Old Ranch Way, to prevent left turns out from the Project's East Bidwell Street driveway and from Old Ranch Way.
- Construct roadway transitions for the new southbound alignment, north of Alder Creek Parkway and south of Old Ranch Way.
- Restripe pavement on the east side of the new median for exclusive northbound use (Stripe as one U-turn lane in a 100-foot (or longer) pocket plus 60-foot taper, one through lane, a 5-foot bike lane, and one right-turn lane in a 150-foot pocket plus 60-foot taper) as described below for the East Bidwell Street/Alder Creek Parkway, East Bidwell Street/Old Ranch Way, and East Bidwell Street/Project Driveway intersections.
- Frontage improvements including curb, gutter, sidewalk, and landscaping along the east side of East Bidwell Street from Old Ranch Way to Alder Creek Parkway to complete roadway section "I" as shown on the Vesting Tentative Parcel Map.

- Alder Creek Parkway

Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure Alder Creek Parkway as follows:

- Widen eastbound Alder Creek Parkway between East Bidwell Street and the Eastern Project boundary to two through lanes plus turn pocket as described below for the Alder Creek Parkway/Western Project Driveway intersection.
- Frontage improvements including curb, gutter, sidewalk, and landscaping along the south side of Alder Creek Parkway from East Bidwell Street to the Eastern project boundary to complete roadway section "B" as shown on the Vesting Tentative Parcel Map.

- Old Ranch Way

Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure Old Ranch Way as follows:

- Construct Old Ranch Way within ultimate right-of-way as a two-lane urban street, between East Bidwell Street and the Old Ranch Way Project Driveway.

- Frontage improvements including curb, gutter, sidewalk, and landscaping along the north side of Old Ranch Way from East Bidwell Street to the Eastern project boundary to complete roadway section “S” as shown on the Vesting Tentative Parcel Map.
- East Bidwell Street/Alder Creek Parkway Intersection
Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure the East Bidwell Street/Alder Creek Parkway Intersection as follows:
 - Northbound Approach: Stripe as one U-turn lane in a 100-foot (or longer) pocket plus 60-foot taper, one through lane, a 5-foot bike lane, and one 150-foot right-turn lane plus 60-foot taper. Widen the west side of East Bidwell Street at the intersection to accommodate U-turns as shown on Roadway Configuration Exhibit.
 - Southbound Approach: Construct as one through lane, and one left-turn lane in a 100-foot (or longer) pocket plus 60-foot taper.
 - Westbound Approach: Retain existing westbound geometry.
 - Control: Retain existing westbound stop-control.
 - The southeast corner of the intersection shall include a “bulb-out” into Alder Creek Parkway that facilitates turn pockets described for the Alder Creek Parkway/Western Project Driveway intersection below.
- East Bidwell Street/East Bidwell Street Project Driveway Intersection
Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure East Bidwell Street/East Bidwell Street Project Driveway Intersection as follows:
 - Northbound Approach: Stripe as one through lane, and a one right-turn in a 150-foot pocket plus a 60-foot taper and 5-foot bike lane.
 - Southbound Approach: Construct as one through lane on the west side of the median.
 - Westbound Driveway: Construct as a single right-turn lane.
 - Eastbound Driveway: Construct a two-lane entrance as shown on the preliminary site plan
 - A raised median on East Bidwell Street is included to prevent left-turns out from the Project Driveway.
- East Bidwell Street/Old Ranch Way Intersection
Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure East Bidwell Street/Old Ranch Way Intersection as follows:

- Northbound Approach: Stripe as shared through-right lane with a 60-foot taper for the right-turn.
 - Southbound Approach: Construct as one through lane, and one left-turn lane in a 90-foot pocket plus 60-foot taper.
 - Westbound Approach: Construct as a single right-turn only lane.
 - A raised median or curb is included on East Bidwell Street to prevent left-turns out from Old Ranch Way. There shall be no westbound left-turn from Old Ranch Way onto East Bidwell Street.
- Alder Creek Parkway/Western Project Driveway Intersection
Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure Alder Creek Parkway/Western Project Driveway Intersection as follows:
 - Eastbound Approach: Construct as two through lanes and one right-turn deceleration lane in a 130-foot pocket plus 30-foot taper, and 5-foot bike lane.
 - Northbound Driveway: Construct as a single right-turn only lane.
 - Driveway Entrance: Construct as a single lane as shown on the preliminary site plan.
- Alder Creek Parkway/Eastern Project Driveway Intersection
Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure Alder Creek Parkway/Eastern Project Driveway Intersection as follows:
 - Eastbound Approach: Construct as two through lanes, and 5-foot bike lane.
 - Northbound Driveway: Construct as a single right-turn only lane.
 - Driveway Entrance: Construct as a single lane as shown on the preliminary site plan.
- Old Ranch Way/Old Ranch Way Project Driveway Intersection
Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure Old Ranch Way/Old Ranch Way Project Driveway Intersection:
 - Eastbound Approach: Construct as one lane
 - Westbound Approach: Construct as one lane.
 - Southbound Driveway: Construct one shared lane for outgoing left and right turns from the Project. Left-turns out of the driveway will not be permitted until future extension of Old Ranch Way east.
 - Driveway Entrance: Construct as a single lane as shown on the preliminary site plan.

Traffic:

The Folsom Plan Area Specific Plan established a series of plans and policies for the circulation system within the entire Plan Area. The FPASP circulation system was designed with a sustainable community focus on the movement of people, not cars, and provides a number of mobility alternatives such as walking, cycling, carpooling, and viable forms of public transportation in addition to vehicular circulation. The circulation plan evaluated regional travel, both in terms of connectivity and capacity as well as local internal connections and access. The circulation plan also addressed the concerns of regional traffic, including parallel capacity to U.S. Highway 50, and connectivity with surrounding jurisdictions while considering community-wide connectivity, alternative modes of travel, and the provision of complete streets.

The 2011 Folsom Plan Area Specific Plan Environmental Impact Report/Environmental Impact Statement included not only a detailed analysis of traffic-related impacts within the Plan Area, but also an evaluation of traffic-related impacts on the surrounding communities. In total, there are fifty-five (55) traffic-related mitigation measures associated with development of the FPASP that are included as conditions of approval for the Shops at Folsom Ranch project (Condition Nos. 58-64 to 58-119). Many of these mitigation measures are expected to reduce traffic impacts to East Bidwell Street. Specific improvements planned for the East Bidwell Street/Iron Point Road Intersection area include but are not limited to; elimination of the U-turn movement on northbound East Bidwell Street at Iron Point Road, installing signage on Placerville Road directing motorists south to Alder Creek Parkway to access U.S. Highway 50, and modifying the right-turn-lane on Iron Point Road at the approach to East Bidwell Street to accommodate a free-flowing movement. Included among the mitigation measures are requirements to; fund and construct roadway improvements within the Plan Area, pay fair-share contribution for construction of improvements north of U.S. Highway 50, participate in the City's Transportation System Management Fee Program, and Participate in the U.S. Highway 50 Corridor Transportation Management Association. The Shops at Folsom Ranch project is subject to all traffic-related mitigation measures required by the 2011 FPASP EIR/EIS.

On May 5, 2015, Fehr & Peers completed a Traffic Impact Analysis for the Westland-Eagle Specific Plan Amendment project (an Addendum to the FPASP EIR/EIS was certified in association with the Westland-Eagle Specific Plan Amendment) and determined that the traffic impacts associated with that project had been adequately addressed in the 2011 Folsom Plan Area Specific Plan EIR/EIS with inclusion of some minor adjustments to account for changes that have occurred since the EIR/EIS was certified. The adjustments include requiring the Westland-Eagle project to modify the westbound approach to the East Bidwell Street/Iron Point Road intersection to include three left-turn lanes, two through lanes, and one right-turn lane. In addition, the project was required to pay a fair-share contribution towards improvements to the East Bidwell Street/Alder Creek Parkway intersection including the addition of a channelized westbound right-turn lane.

On October 24, 2018, T.KEAR Transportation Planning & Management completed a Transportation Impact Study (Study)(Attachment 18, Exhibit C) for the Shops at Folsom Ranch project to ensure that no additional impacts would occur that were not previously identified and addressed by the 2011 FPASP EIR/EIS and the 2015 Westland-Eagle Specific Plan Addendum to the FPASP EIR/EIS. In addition, the purpose of the Study was to evaluate site-specific impacts associated with the proposed 27,900-square-foot shopping center project relative to traffic, access, circulation, and parking. The Study analyzed traffic operations at two street intersections (East Bidwell Street/Alder Creek Parkway and East Bidwell Street/Old Ranch Way) and four project

driveway intersections (Alder Creek Parkway/Eastern Driveway, Alder Creek Parkway/Western Driveway, East Bidwell Street/Project Driveway, and Old Ranch Way/Project Driveway) under five scenarios: Existing Conditions (2018), Existing Plus Project Conditions (2018), Existing Plus Approved Projects Conditions (2023) Without Project, Existing Plus Approved Projects Conditions (2023) With Project, and Cumulative Plus Project Conditions (2036).

The proposed Shops at Folsom Ranch project is expected to generate 255 AM Peak-Hour trips and 190 PM Peak Hours trips. Including existing trips in the project area, there are expected to be 750 AM Peak Hour trips and 500 PM peak hour trips. Under Existing Conditions Plus Project, all of the study intersections and project driveways are anticipated to operate at Level of Service C (LOS C) or better with exception of the East Bidwell Street/Alder Creek Parkway intersection. The East Bidwell Street/Alder Creek Parkway intersection currently operates at LOS E without the project. At this stop-sign controlled intersection, the level of service is defined by the worst turning movement, which is the low volume (only 5 vehicles make this turning movement in the AM and PM Peak Hours) westbound left-turn from Alder Creek Parkway onto southbound East Bidwell Street. The proposed project is expected to add one AM Peak Hour trip and two PM Peak Hour trips to this particular turning movement. Frontage improvements (improved sight distance with grading) provided by the proposed project are expected to significantly reduce the existing level of delay associated with this left-turn movement (25.5/44.6 second reduction in the AM and PM Peak Hours respectively) and improve the East Bidwell Street/Alder Creek intersection to LOS D, which is consistent with the City's General Plan policy of LOS D.

As mentioned previously within this section of the report, the proposed project does not include construction of Westwood Drive or Old Ranch Way east of the project boundary. Vehicles exiting the project site onto eastbound Alder Creek Parkway would be required to head east and make a U-turn movement at Placerville Road in order to access U.S. Highway 50. To prevent project traffic that exits onto eastbound Alder Creek Parkway from potentially utilizing Placerville Road to access U.S. Highway 50, staff recommends that an eastbound U-turn pocket on eastbound Alder Creek Parkway, prior to the Alder Creek Parkway/Placerville Road be constructed as described in the following recommendation (Condition No. 50-1):

- The owner/applicant shall construct an eastbound Left-Turn Lane that provides for a U-Turn movement on Alder Creek Parkway at the future location of Westwood Drive as shown in Figure ES-2 of the Traffic Study. The Left-Turn Lane shall be constructed with a 100-foot-long (or longer) turn pocket plus 60-foot taper. Widening on the north side of Alder Creek Parkway is required to accommodate the U-turn movement. The area to be widened is shown conceptually in Figure ES-2 of the Traffic Study, widening shall accommodate turning radius for emergency vehicles and delivery trucks serving the Project.

Existing Plus Approved Projects Conditions (2023) Without Project and Existing Plus Approved Projects Conditions (2023) With Project were also analyzed in the Traffic Study. Existing Plus Approved Project Conditions (2023) takes into consideration traffic impacts associated with approved projects within the Folsom Plan Area which includes an additional 2,576 residential dwelling units (Broadstone Estates Subdivision, Carr Trust Subdivision, Enclave at Folsom Ranch Subdivision, Folsom Heights Subdivision, Mangini Ranch Phase 1 and 2 Subdivisions, Russell Ranch Subdivision, and White Rock Springs Ranch Subdivision). The proposed project is not expected to result in any traffic-related impacts at the study intersections or project driveways if the

project is developed prior to development of the aforementioned residential projects. However, if the proposed project is developed after the aforementioned residential projects, mitigation measures associated with the residential projects may impact access to the proposed project site. Existing mitigation measures associated with development of the aforementioned residential units include signalization and widening of the East Bidwell Street/Alder Creek Parkway intersection, as well as widening of East Bidwell Street and construction of a raised median along the Shops at Folsom Ranch frontage with East Bidwell Street. These mitigation measures result in the blocking of key left-turn and U-turn movements associated with the proposed project. To ensure that adequate access is provided to the project site, staff recommends implementation of the following measures (Condition No 50-2/3):

- If not already constructed, the owner/applicant shall construct a northbound U-turn at the East Bidwell Street/Alder Creek Parkway intersection with a 100-foot long (or longer) pocket plus 60-foot taper.
- If neighboring projects construct a raised median within East Bidwell Street, the owner/applicant shall construct a southbound left-turn through the raised median at the East Bidwell Street/Old Ranch Way intersection with a 90-foot long (or longer) pocket plus 60-foot taper.

With incorporation of the aforementioned measures, each of the study intersections and project driveways will operate at LOS D or better under Existing Plus Approved Projects Conditions (2023) Without Project and Existing Plus Approved Projects Conditions (2023) With Project, which is consistent with the City's General Plan policy of LOS D.

Under Cumulative Plus Project Conditions (2036), the Study also determined that all study intersections and project driveways would operate LOS D, which again is consistent with the City's LOS D policy. As part of the cumulative analysis, the applicant requested that the Study evaluate providing an additional eastbound left-turn pocket on Alder Creek Parkway (across from project site) into an undeveloped commercial property (also owned by the project applicant) located on the north side of Alder Creek Parkway. The Study determined that no traffic-related impacts would result from development of the left-turn pocket. To avoid future construction-related disruptions on Alder Creek Parkway, staff recommends that the following measure be implemented (Condition No. 50-4):

- The owner/applicant shall construct a 150-foot left turn pocket with 60-foot taper on eastbound Alder Creek Parkway to facilitate future access into the 50-acre commercial property located on the north side of Alder Creek Parkway as shown on the Offsite Roadway Configuration Exhibit (Attachment 9). The left-turn pocket shall be barricaded and striped to prevent left or U-turns prior to development of the 50-acre commercial property (Parcel 85 A), and further analyses of its site access.

Project Access and On-Site Circulation

As shown on the submitted site plan, access to the project site is provided by four project driveways including two driveways on Alder Creek Parkway (Eastern Driveway and Western Driveway), one driveway on East Bidwell Street, and one driveway on Old Ranch Way. The Western Driveway on Alder Creek Parkway and the driveway on East Bidwell Street are considered the primary

driveways for the project and are expected to handle a majority trips into and out of the shopping center. The Eastern Driveway on Alder Creek Parkway is primarily designed to accommodate fueling trucks entering and leaving the gas station and commercial service vehicles within the shopping center. The project driveway on Old Ranch Way is a secondary driveway that is intended to provide access to local residents in the project area. Internal vehicle circulation is provided by a series of internal drive aisles, which provide access throughout the project site. Bicycle and pedestrian circulation is accommodated by a combination of Class II and Class III bicycle lanes, street-separated sidewalks, street-attached sidewalks, internal pedestrian walkways, and an enhanced pedestrian pathway connection to the adjacent Enclave at Folsom Subdivision project site.

The Traffic Study evaluated a number of issues associated with the proposed shopping center including driveway locations, driveway throat depths, right-turn lanes and tapers, internal circulation, and drive-thru queue lengths. The Study determined that the driveway throat depths are sufficient to accommodate vehicles entering and exiting the project site. The Study also determined that the proposed right-turn deceleration lanes and tapers located on northbound East Bidwell Street and eastbound Alder Creek Parkway are adequately designed to safely facilitate vehicle movement into and out of the project site. In addition, the study determined that the location of the four project driveways is appropriate based on the proposed land uses (retail shops, restaurants, and a fuel station with car wash) with the shopping center. However, in the event that the fuel station is not developed within the shopping center, staff recommends that the Eastern Driveway on Alder Creek Drive be eliminated in order to reduce the number of driveways on Alder Creek Parkway (major arterial roadway) and to reduce the number of driveways in close proximity to the intersection of East Bidwell Street and Alder Creek Parkway. Condition No. 50.5 is included to reflect this requirement.

The Traffic Study evaluated the vehicle queues provided for the two proposed fast food restaurants (Buildings P3 and P4 as shown on submitted site plan) to ensure that adequate vehicle storage is provided in the drive-thru lanes so that vehicles don't back up into primary internal drive aisles or into the public right-of-way. The Study determined that each of the drive-thru lanes for the respective fast food restaurant pads could accommodate approximately ten (10) vehicles before the vehicles would begin affecting internal drive aisles within the shopping center. Staff has determined that the proposed vehicle queues are consistent with City policy (stacking or queues for 10 vehicles) with regard to typical fast food restaurants (McDonald's, Burger King, Starbucks, etc.). However, because some fast food restaurants (In-N-Out Burger, Dutch Bros., etc.) are known to experience longer vehicle queuing, staff recommends that an additional vehicle queue analysis may be required once the specific tenants have been identified for individual fast food restaurants (Building P3 and P4) within the shopping center. Condition No. 50-6 is included to reflect this requirement.

Parking

The applicant proposes to provide a total of 230 parking spaces within the shopping center including 215 striped parking spaces and 15 unstriped parking spaces in the fuel station area where the gas pumps are located. Thirteen (13) of the aforementioned parking spaces are specifically designated as electric vehicle parking spaces and will have electric vehicle charging stations installed. The Folsom Plan Area Specific Plan has established specific parking standards for projects located on sites with the GC (General Commercial) land use designation (FPASP, Table A.14, Vehicle Parking Requirements). The following table compares the parking provided by the

proposed project in relation to the parking requirements established for the GC land use designation:

Folsom Plan Area Specific Plan					
General Commercial (SP-GC) Vehicle Parking Requirements					
Building Name	Building Area	Restaurant Seats	Land Use	Parking Ratio	Parking Required
S1	2,500 S.F.	63	Restaurant	1 Space/3 Seats	21
S1	2,500 S.F.	NA	Retail	3 Space/1,000 S.F.	8
S2	2,500 S.F.	63	Restaurant	1 Space/3 Seats	21
S2	2,500 S.F.	NA	Retail	3 Space/1,000 S.F.	8
P1	3,000 S.F.	NA	Gas Station	5 Space/1,000 S.F.	15
P2	9,000 S.F.	215	Restaurant	1 Space/3 Seats	72
P3	1,900 S.F.	60	Restaurant/Drive-Thru	1 Space/3 Seats	20
P4	4,000 S.F.	127	Restaurant/Drive-Thru	1 Space/3 Seats	42
Total Parking Required					207
Total Parking Provided (215 striped spaces/15 un-striped spaces at fuel station)					230

As shown on the table above, the proposed project meets the parking requirements established by Folsom Plan Area Specific Plan by providing 230 parking spaces whereas 207 parking spaces are required. The proposed project is considered an integrated shopping center where each of the parcels will share common use of all the project driveways and parking spaces. A condition of approval (Condition No. 43) has been placed on the project requiring that easements for reciprocal access and parking be recorded on the Parcel Map. The proposed project, which includes 23 bicycle parking spaces that are evenly distributed throughout the project, meets the bicycle parking requirements of the Folsom Plan Area Specific Plan by providing 23 bicycle parking spaces whereas 21 spaces are required.

Noise

A supplemental Environmental Noise Assessment (Assessment) was prepared by Bollard Acoustical Consultants on May 22, 2018 in order to verify that there would be no new noise-related impacts associated with the proposed project that were not contemplated and addressed by the 2011 FPASP EIR/EIS and the 2015 Westland-Eagle Specific Plan Amendment Addendum. The purpose of the supplemental Assessment was to quantify noise levels generated by the proposed project and potential impacts of those noise sources on future residential development in the project area. Potential project-related noise sources that were evaluated included drive-thru speakers associated with two fast-food restaurants (Buildings P3 and P4 as shown on the submitted site plan), car wash equipment associated with the fuel station (Building P1), and mechanical equipment associated with every commercial building in the development. The Assessment concluded that the aforementioned noise sources associated with the proposed project would not exceed the applicable City of Folsom noise standards. However, to further ensure that the proposed project will not impact future nearby residential uses, staff recommends that the following project design measures be implemented as suggested by the Environmental Noise Assessment (Condition No. 53):

- The owner/applicant shall construct a solid 6-foot-tall masonry wall (noise barrier) along the eastern project boundary as shown in Figure 1 of the Environmental Noise Assessment (Attachment 18).
- The vacuum stalls associated the fuel station and car wash shall be located on the west side of Building P3 as shown on Figure 1 of the Environmental Noise Assessment (Attachment 18).
- The car wash entrance shall be equipped with an automatic entrance door that will remain closed during every wash cycle.
- All rooftop mechanical equipment shall be screened from view by intervening rooftop parapets.

In addition to the noise measures recommended above, the proposed project is subject to the noise mitigation measures identified within the 2011 FPASP EIR/EIS and the 2015 Westland-Eagle Specific Plan Amendment Addendum.

Construction of the proposed project would temporarily increase noise levels in the project vicinity during the construction period, which would take approximately 12-15 months. Construction activities, including site clearing, excavation, grading, building construction, and paving, would be considered an intermittent noise impact throughout the construction period of the project. The City's Noise Ordinance excludes construction activities from meeting the General Plan Noise Element standards, provided that all phases of construction are limited to the hours between 7:00 a.m. and 6:00 p.m. on weekdays, and between 8:00 a.m. and 5:00 p.m. on Saturdays. To ensure compliance with the City's Noise Control Ordinance and General Plan Noise Element, staff recommends that hours of construction operation be limited from 7:00 a.m. to 6:00 p.m. on weekdays and 8:00 a.m. to 5:00 p.m. on Saturdays with no construction permitted on Sundays or holidays. In addition, staff recommends that construction equipment be muffled and shrouded to minimize noise levels. Condition No. 54 is included to reflect these requirements.

Walls and Fencing

The proposed project includes construction of a two-foot-tall stone-faced retaining wall in the southwest corner of the project site adjacent to the drive-thru lane associated with Building P4. The proposed project also includes construction of a decorative, six-foot-tall masonry wall along the eastern boundary of the project site. In addition, decorative, three-foot-tall ranch-style fencing is proposed at the northwest corner of the project site. The City typically requires that that masonry or sound walls have a high quality design that features integrally colored split-face concrete masonry units with a distinctive wall cap, treatments, and pilasters. Staff recommends that the final location, design, height, materials, and colors of the retaining wall, sound wall, and decorative fencing be subject to review and approval by the Community Development Department to ensure consistency with the overall design of the proposed shopping center. Condition No. 38 is included to reflect this requirement.

Site Lighting

The applicant is proposing to use a combination of freestanding parking lot lights, landscape and walkway lighting, courtyard festoon lighting, and building-attached lights to illuminate the project

site for safety and aesthetic purposes. The freestanding parking lot light poles will be evenly distributed throughout the parking lot areas. The landscape and walkway lighting, which primarily consist of bollard-style lights, will be situated within the landscaped areas and along pedestrian walkways. The festoon lighting, which are decorative strings of lights that will be strung up in the outdoor courtyard areas, provide ambient lighting on a seasonal basis. The building attached lighting will consist of decorative lighting fixtures intended to illuminate pedestrian walkways adjacent to buildings within the shopping center. To minimize potential lighting-related impacts, staff recommends that all freestanding parking lot lights, landscape and walkway lights, festoon light strings, and building attached lights be screened, shielded, and directed downward to minimize glare towards the surrounding properties. In addition, staff recommends that the final design of all exterior lighting be subject to review and approval by the Community Development Department. Condition No. 27 is included to reflect these requirements.

Existing and Proposed Landscaping

As noted within the project description, the project site has recently been disturbed to a minor extent by construction of Folsom Plan Area backbone infrastructure improvements along Alder Creek Parkway and East Bidwell Street. Existing vegetation on the project site consists of non-native and naturalized grasslands. There are no trees or other types of vegetation present on the project site.

The proposed project includes a 20-foot-wide landscape easement along the frontage of East Bidwell Street, an 18-foot-wide landscape easement along the frontage of Alder Creek Parkway, and a 14-foot-wide landscape easement along the frontage of Old Ranch Way. Pedestrian sidewalks will be located within the aforementioned landscape easement or buffer areas in addition to landscape materials. Proposed landscaping includes a variety of trees, shrubs, and groundcover. The proposed shade and accent trees include Big Leaf Maple, California Bay, California Sycamore, Olive, and Valley Oak. Proposed shrubs and groundcover will feature drought-tolerant plant materials consisting of Boston Ivy, Blue Wildrye, Buckbrush, California Poppy, Chaparral Currant, Common Manzanita, Coyote Brush, Deergrass, Needlegrass, Western Redbud, and Wild Rose. The preliminary landscape plan meets the General Commercial Development Standards (FPASP) requirement by landscaping 22% of the entire project site whereas 20% of the project site is required to be landscaped. In addition, preliminary landscape plan meets the City shade requirement (40%) by providing 46% shade in the parking lot area within fifteen (15) years.

In April, 2015, the City approved the Folsom Plan Area Specific Plan Community Design Guidelines. The Design Guidelines are intended to provide guidance to developers with respect to the level of design quality expected for the “Public Realm” improvements in the Folsom Plan Area. In relation to the proposed project, the Design Guidelines identified the intersection of East Bidwell Street and Alder Creek Parkway as one of the gateways or symbolic entry points into the Plan Area. The Design Guidelines recommend that these gateways include significant design features including but not limited to monuments, water features, lighting, and ornamental plantings. To address the recommendations, the applicant has designed the project to include a number of significant hardscape and landscape design features at the northwest corner of the project site including two large decorative monuments, ranch-style fencing, and a large grove of olive trees. The aforementioned design features, which are intended to recognize the history and agrarian roots of the project area, represent a symbolic entry point into the Plan Area as recommended by the Design Guidelines.

Signage

The applicant has submitted a Uniform Sign Criteria (Attachment 17) for the Shops at Folsom project with the intent of ensuring the uniformity and consistency of signage for the entire center. The Sign Criteria has also been designed to preserve and enhance the overall appearance of the shopping center, prohibit excessive and confusing sign displays, and safeguard and enhance property values. The Sign Criteria provides for a combination of freestanding monument signs, building-attached wall signs, canopy signs, and blade signs to provide identification for the shopping center. The proposed freestanding signs include two project identification monument signs and one multi-tenant identification monument sign. The proposed project identification monument signs, which are highlighted by a corten metal sign supported by a stone veneer base, are located at the northwest and southwest corners of the project site. The multi-tenant monument sign, which features a corten metal background with aluminum finish panels supported by a stone veneer base, is located at the project driveway on East Bidwell Street.

Identification for individual tenants includes a combination of building-attached wall signs, canopy signs, and blade signs. Tenants will be permitted one wall sign or one canopy sign per store frontage, with end-cap tenants being allowed an additional sign for a maximum total of three wall/canopy signs. For the purpose of secondary identification, tenants facing pedestrian walkways are allowed a two-sided blade sign. In terms of design, the Sign Criteria requires that signs utilize internally illuminated channel letters and shapes, illumination may be through the face or “halo lit” where light spills out the back of the sign onto a wall surface. Creative signage is encouraged by the Sign Criteria and allows hand painted wall signage/graphics as well as face-lit metal cut out letters. With regard to sign area, the Sign Criteria allows for 1.5 square feet of sign area per lineal foot of the primary tenant leased frontage length, with the sign length not exceeding 75% of the tenant leased wall length.

The Folsom Municipal Code (FMC, Chapter 17.59 SIGNS) regulates commercial signage including freestanding monument signs and building-attached wall signs. In reviewing the submitted Sign Criteria, staff has determined that the proposed building-attached wall signs, canopy signs, and blade signs are consistent with the requirements of the Folsom Municipal Code in terms of the permitted sign locations and maximum sign area allowed.

The Folsom Municipal Code states that integrated developments with multiple businesses are permitted one freestanding monument sign with a maximum sign area 60 square feet and a 15-foot height limit to identify the name of the center and the tenants within the development. As described previously, the Sign Criteria for the shopping center provides for a total of three monument signs including two project identification signs (6 feet tall/32 S.F. sign area and 3 feet tall/6 S.F. of sign area) and one multi-tenant monument sign (15 feet tall/60 S.F. of sign area), thus exceeding the number of allowed monument signs. As part of the Planned Development Permit that has been submitted with the subject application, the applicant is requesting approval for three monument signs to provide identification for the shopping center. The applicant has indicated that the additional monument signs are necessary to provide adequate identification for the shopping center given that the center has driveway entrances on three different streets. In addition, the applicant notes that the two additional monument signs are more decorative in nature and important to the overall appearance of the center and do not advertise individual businesses, just the shopping center name.

In reviewing the request for the additional monument signs, staff considered past City policy with regard to shopping center monument identification and the specific design of the proposed shopping center. The City of Folsom contains numerous integrated shopping centers whose site design features multiple access points from public roadways including but not limited to Palladio at Broadstone, Broadstone Marketplace, Broadstone Power Center, Folsom Square, and Folsom Central Plaza. With each of the aforementioned shopping centers, the Planning Commission approved a Uniform Sign Criteria through the Planned Development Permit process that allowed those developments to have multiple monument signs to ensure adequate identification. The proposed project is similar to the aforementioned shopping centers in that it is located on a site that will have driveway access from three different street frontages (Alder Creek Parkway, East Bidwell Street, and Folsom Ranch Way). Based on the site design of the proposed project, staff has determined that three monument signs are necessary to provide adequate identification for the shopping center. In addition, staff has determined that the provision of three monument signs is consistent with City policy and prior Planning Commission action.

Trash/Recycling Enclosure

The proposed project includes six trash and recycling enclosures that are evenly distributed throughout the shopping center. In addition to trash and recycling, the enclosures have been designed with separate food waste collection area to meet the needs of restaurants within the shopping center. The proposed six-foot-tall trash/recycling enclosures, which measure 25 feet in width by 10 feet in depth, include a design that features CMU split-face blocks, a CMU wall-cap, and a metal gate. The applicant is proposing to paint the trash-recycling enclosure an earth-tone color to match the colors utilized on the proposed commercial buildings. Staff recommends that the final location, orientation, design, materials, and colors of the trash/recycling enclosures is subject to review and approval by the Community Development Department. Condition No. 37 is included to reflect this requirement.

Grading

As shown on the submitted grading and drainage plan (Attachment 4), the Shops at Folsom Ranch project site is undulating with gently rolling mounds. The maximum elevation is approximately 438 feet in the central portion of the project site, sloping downward to approximately 416 feet in the northeast corner of the site. The project site is currently elevated approximately 10-15 feet above the grade of the adjacent roadways (Alder Creek Parkway and East Bidwell Street).

The applicant is proposing to mass grade the Shops at Folsom Ranch project site in order to accommodate development of the project site. Mass grading of the project site and associated roadway improvements will involve the stockpiling of approximately 78,500 cubic yards of material on the adjacent undeveloped Enclave at Folsom Ranch Subdivision site to the east and the undeveloped commercial site to the north of Alder Creek Parkway (Attachment 11). An additional 7,500 cubic yards of export material will be utilized to complete the required off-site roadway widening of East Bidwell Street and Alder Creek Parkway. The aforementioned mass grading activity will require transportation of large amounts of material onto and across Alder Creek Parkway to the aforementioned spoils site. Staff recommends that the owner/applicant submit a traffic control plan to address the method by which the spoils will be transported and to ensure that the impact to the adjacent public roadways (Alder Creek Parkway and East Bidwell Street) is limited to the greatest extent possible. The traffic control plan will be subject to review and approval by the Community Development Department. Condition No. 17 is included to reflect this requirement.

As described above, development of the project site is anticipated to require significant movement of soils and the compaction of said materials. The applicant will be required to provide a complete geotechnical report before the design of streets and building foundations are finalized. Condition No. 58-43 is included to reflect this requirement. The proposed project includes construction of a two-foot-tall stone-faced retaining wall in the southwest corner of the project site adjacent to Building P4. Staff recommends that the final location, design, materials, and colors of all retaining walls be subject to review and approval by the Community Development Department. Condition No. 38 is included to reflect this requirement.

Drainage

As shown on the submitted grading and drainage plan (Attachment 4) and the off-site utility exhibit (Attachment 10), the Shops at Folsom Ranch project includes a number of on-site and off-site improvements to accommodate stormwater flows generated by the project. In terms of on-site stormwater improvements, the proposed project includes installation of storm drain pipes and appurtenances that will direct flows to Old Ranch Way to the south of the project site. In relation to off-site stormwater improvements, the proposed project will be installing a storm drain main and appurtenances in the following locations: Alder Creek Parkway from East Bidwell Street to the eastern project boundary, East Bidwell Street from Old Ranch Way to tie in at proposed Hydromodification Basin No. 19 to the south of the project site, and Old Ranch Way from East Bidwell Street to the eastern project boundary.

As described previously, the proposed project includes construction of a permanent hydromodification basin (Basin No. 19) on the east side of East Bidwell Street, approximately 1,500 feet to the south of the project site. Proposed Hydromodification Basin No. 19 will tie in to existing Drainage Basin No. 6, which is located approximately 200 feet to the south. Permanent hydromodification basins are approved as part of the FPA Storm Drainage Master Plan. Staff recommends the storm drain improvement plans provide for “Best Management Practices” that meet the requirements of the water quality standards of the City’s National Pollutant Discharge Elimination System Permit issued by the State Regional Water Quality Control Board. Condition 35 is included to reflect this requirement.

Utilities

As provided for in the Development Agreement between the City and the Landowners south of U.S. Highway 50, each individual project within the Plan Area is required to build the portion of the off-site utility infrastructure system (sewer, water, stormwater, dry utilities, etc.) necessary to support the proposed project. The Shops at Folsom Ranch project will be served by existing sewer infrastructure located within the Alder Creek Parkway and East Bidwell Street right-of-way. The applicant is proposing to construct a sewer main and appurtenances within the Old Ranch Way from East Bidwell Street to the eastern project boundary.

The proposed Shops at Folsom Ranch project is located within the Zone 3 water pressure zone in the Folsom Plan Area. The proposed project includes construction of improvements to accommodate potable water and recycled water. The following are proposed potable water improvements to be constructed with development of the project: installation of a pressure reducing station at Alder Creek Parkway and Westwood Drive (Zone 4 to Zone 3), installation of a Zone 4 distribution main and appurtenances within Alder Creek Parkway from Placerville Road to Westwood Drive, installation of a Zone 3 distribution main and appurtenances within Alder Creek Parkway from Westwood Drive to East Bidwell Street, installation of a Zone 3 distribution main

and appurtenances from Old Ranch Way to Mangini Parkway, and installation of a Zone 3 distribution main and appurtenances from East Bidwell Street to the eastern project boundary. To accommodate recycled water, a Zone 3 distribution main and appurtenances are proposed to be constructed from East Bidwell Street to the eastern project boundary.

As shown on the submitted utility plan (Attachment 5) and the off-site utility exhibit (Attachment 10), the proposed project includes construction of on-site and off-site dry utility ties to accommodate electric, gas, telephone, and cable infrastructure. With respect to off-site utility improvements, a dry joint-utility trench will be constructed from Placerville Road to East Bidwell Street in order to serve the proposed project.

The Pacific Gas & Electric Company (PG&E) will provide the entire Plan Area including the Shops at Folsom Ranch project with natural gas service. Peak natural gas demand at build-out of the Plan Area is estimated at approximately 818 thousand cubic feet per hour. PG&E currently has excess capacity in its system to serve a portion of the Plan Area. PG&E will be constructing additional infrastructure (transmission pipelines, gas regulator stations, etc.) within the Plan Area to accommodate the full gas demand. The Sacramento Metropolitan Utility District (SMUD) will supply electric service to the entire Plan Area including the Shops at Folsom Ranch project. Peak electric demand at build-out of the Plan Area is estimated at approximately 87 megavolt amperes. Three electrical substations will be constructed to provide electric service to the Plan Area. The first SMUD electrical substation has been constructed on Placerville Road, within the eastern portion of the project site.

Off-Site Improvements

As discussed within various sections of this report, the proposed project features a number of off-site improvements (as shown on Attachments 9, 10, and 11) including roadway improvements to Alder Creek Parkway, East Bidwell Street, and Old Ranch Way, storm drain improvements within Alder Creek Parkway, East Bidwell Street, and Alder Creek Parkway, storm drain improvements involving construction of an off-site hydromodification basin to the south of the project site, utility improvements within Alder Creek Parkway, East Bidwell Street, and Old Ranch Way, and grading activities that impact adjacent and nearby properties. For any off-site improvements constructed on private property that are not under the ownership or control of the project applicant, staff recommends that the owner/applicant shall obtain all rights-of-entry, and if necessary, a permanent easement shall be obtained and provided to the City. Condition No. 15 is included to reflect these requirements.

Architecture and Design

The Shops at Folsom Ranch features a contemporary California Ranch Style architectural theme that is designed to recognize the history of Folsom, while also complimenting the agrarian roots and rolling hills landscape of the project site. As shown on the submitted building elevations (Attachment 12) and building perspectives (Attachment 14), the design of the buildings features many unique architectural elements including varied roof forms, staggered roof heights and pitches, asymmetrical shapes, and trellis structures. The proposed project utilizes a variety of natural building materials, which accentuate the Ranch Style design theme of the buildings. As shown on the color and materials board (Attachment 15), the proposed color palette for the buildings is a lighter muted range of earth tone colors including light tans, warm wood tones, cool grays, and rustic bronze-colored metals. With regard to site design, the proposed project features a number of gathering places (courtyards and outdoor seating areas) intended to compliment the buildings

through the use of high quality materials (limestone ledge walls, gabion rock walls, corten steel design elements, and ranch wood decking) and site furnishings (wood benches, wood planter boxes, decorative tree grates, bollard lights, and festoon lighting).

Commercial design guidelines were intentionally not established for the Folsom Plan Area in order to provide projects with the opportunity to create innovating and creating design concepts. In addition, design guidelines are not necessary as the applicant has provided specific design details for all of the buildings within the shopping center. As described above, the applicant has chosen a modern California Ranch Style architectural theme in order to complement the natural setting of project site. In reviewing the project design, staff considered design parameters that have been established previously for other high quality commercial developments within the City including the Parkway Shopping Center, Broadstone Marketplace Shopping Center, Prairie City Crossing Shopping Center, each of which have similarities to the proposed project. The aforementioned design guidelines include a variety of recommendations for commercial developments including:

- The architectural design of buildings should consider the site, relationship to other structures, and climatic orientation.
- Strong variations of traditional architecture, massing, and form which create texture and shadow should be a major consideration.
- Openings in buildings should be accentuated architecturally through indentation, framing, and roof variations.
- Buildings with long uninterrupted exterior walls should be avoided. Walls should have varied forms to create shadows which soften the architecture.
- Natural materials such as stone, masonry, wood, and patterned concrete should be used as building materials. Other building materials such as tile, glass, and metal should be utilized in concert with the natural building materials to reflect the area's modernity, diversity, and traditions.
- Finish colors of general wall areas should be of natural earth tones or variations of these tones. Limited accent colors of compatible schemes may be used for trim, window areas, and doors.

In reviewing the architecture and design of the proposed shopping center, staff has determined that the project features a modern and sophisticated design concept that includes a significant number of quality design elements that results in an attractive overall appearance. Staff has also determined that the proposed materials and colors clearly compliment the California Ranch Style design theme of the shopping center. In addition, the proposed design theme of the shopping center compliments the natural setting of the project area. As a result, staff forwards the following design recommendations for the Shops at Folsom project to the Commission for consideration:

1. This approval is for the Shops at Folsom Ranch Shopping Center, which includes development of six, single-story commercial buildings totaling 27,900 feet. The applicant shall submit building plans that comply with this approval and the attached building elevations and building perspectives dated August 31, 2018 and October 5, 2018.
2. The design, materials, and colors of the proposed Shops at Folsom Ranch Shopping Center shall be consistent with the submitted building elevations, building perspectives, materials samples, and color scheme to the satisfaction of the Community Development Department.
3. Brick pavers, stamped asphalt or another type of colored masonry material (ADA compliant) shall be used to designate pedestrian walkways and crosswalks on the project site, in addition to where pedestrian paths cross drive aisles, and shall be incorporated as a design feature at the primary driveway entrances at Alder Creek Parkway (westerly driveway) and East Bidwell Street.
4. All mechanical equipment shall be concealed from view of public streets, neighboring properties and nearby higher buildings where practicable to the satisfaction of the Community Development Department.
5. The final design of the building-attached light fixtures shall be subject to review and approval by the Community Development Department to ensure architectural consistency with the overall building design.

These recommendations are included in the conditions of approval presented for consideration by the Planning Commission (Condition No. 52).

ENVIRONMENTAL REVIEW

The application of CEQA to the approval of a project that is consistent with a previously approved zoning decision, community plan, or general plan, for which an environmental impact report was certified, shall be limited to effects upon the environment which are peculiar to the parcel or to the project and which were not addressed as significant effects in the prior environmental impact report. (Public Resources Code § 21083.3.) This streamlines the review of such projects and reduces the need to prepare repetitive environmental studies (CEQA Guidelines § 15183(a).)

The City, as lead agency, has determined that the Shops at Folsom Ranch project is entirely consistent with the Folsom Plan Area Specific Plan (FPASP) and Westland Eagle Specific Plan Amendment. As a project that is consistent with existing plans and zoning and which would not result in any new or more severe environmental effects that are peculiar to the project or the parcels or which were not previously analyzed as significant effects in the FPASP EIR/EIS and/or the Addendum for the Westland Eagle Specific Plan Amendment, the Shops at Folsom Ranch project qualifies for the streamlining provisions in Public Resources Code section 21083.3 and CEQA Guidelines section 15183. The City provides the attached CEQA Streamlining Analysis and Checklist exploring considerations raised by CEQA Guidelines section 15183 because the checklist, together with the information in this staff report, provides a clear disclosure of the City's evidence and reasoning for determining the project's consistency with the FPASP and eligibility for the claimed CEQA streamlining.

Site specific studies were prepared for this project to analyze noise (Noise Assessment referenced above) and traffic (Attachment 18, Exhibit C). Along with the attached Checklist, these studies document the bases for the conclusion that the proposed project will not have any new significant or substantially more severe environmental impacts which were not addressed as significant effects in the FPASP EIR/EIS and/or the Addendum for the Westland Eagle Specific Plan Amendment and the conclusion that it will not result in any significant impacts that are peculiar to the parcel or to the project and which were not addressed as significant impacts in the FPASP EIR/EIS and/or the Westland Eagle Addendum.

All of the recommended feasible mitigation measures previously adopted for the FPASP Final EIR/EIS, the Westland Eagle Addendum to the FPASP EIR/EIS, the U.S. Highway 50 Backbone Infrastructure Mitigated Negative Declaration, and the Revised Off-Site Water Facility Alternative Mitigation Monitoring and Reporting Program for the FPASP have been included as conditions of approval for this project. The City is not required to formally adopt any analysis under CEQA to make these determinations under Guidelines section 15183, except for a finding regarding the implementation of previously adopted mitigation measures.

RECOMMENDATION/PLANNING COMMISSION ACTION

MOVE TO APPROVE A VESTING TENTATIVE PARCEL MAP CREATING FIVE (5) INDIVIDUAL PARCELS AS ILLUSTRATED IN ATTACHMENT 2 FOR THE SHOPS AT FOLSOM RANCH PROJECT;

AND

MOVE TO APPROVE A PLANNED DEVELOPMENT PERMIT FOR THE DEVELOPMENT OF A 27,900-SQUARE-FOOT SHOPPING CENTER LOCATED AT THE SOUTHEAST CORNER OF THE INTERSECTION OF ALDER CREEK PARKWAY AND EAST BIDWELL STREET FOR THE SHOPS AT FOLSOM RANCH PROJECT AS ILLUSTRATED ON ATTACHMENTS 2 THROUGH 18 WITH THE FOLLOWING FINDINGS AND CONDITIONS (NOS. 1-58):

GENERAL FINDINGS

- A. NOTICE OF HEARING HAS BEEN GIVEN AT THE TIME AND IN THE MANNER REQUIRED BY STATE LAW AND CITY CODE.
- B. THE PROJECT IS CONSISTENT WITH THE GENERAL PLAN AND THE FOLSOM PLAN AREA SPECIFIC PLAN AS AMENDED BY THE WESTLAND-EAGLE GENERAL AND SPECIFIC PLAN AMENDMENT.

CEQA FINDINGS

- C. A FINAL ENVIRONMENTAL IMPACT REPORT AND ENVIRONMENTAL IMPACT STATEMENT WAS PREVIOUSLY CERTIFIED FOR THE FOLSOM PLAN AREA SPECIFIC PLAN IN ACCORDANCE WITH CEQA AND NEPA.

- D. AN ADDENDUM TO THE FOLSOM PLAN AREA SPECIFIC PLAN FINAL ENVIRONMENTAL IMPACT REPORT AND ENVIRONMENTAL IMPACT STATEMENT WAS CERTIFIED IN 2015 FOR THE WESTLAND-EAGLE SPECIFIC PLAN AMENDMENT PROJECT IN ACCORDANCE WITH CEQA.
- E. THE PROPOSED PROJECT IS CONSISTENT WITH THE DEVELOPMENT DENSITY ESTABLISHED BY THE FOLSOM PLAN AREA SPECIFIC PLAN AS AMENDED BY THE WESTLAND EAGLE SPECIFIC PLAN AMENDMENT, FOR WHICH A FINAL EIR/EIS WAS CERTIFIED.
- F. ANY FEASIBLE MITIGATION MEASURES SPECIFIED IN THE FOLSOM PLAN AREA SPECIFIC PLAN FINAL EIR/EIS AND WESTLAND EAGLE ADDENDUM RELEVANT TO A SIGNIFICANT EFFECT THE PROPOSED PROJECT WILL HAVE ON THE ENVIRONMENT WILL BE UNDERTAKEN FOR THE PROPOSED PROJECT, CONSISTENT WITH PUBLIC RESOURCES CODE SECTION 21083.3(c) AND CEQA GUIDELINES SECTION 15183(e).
- G. NO PROJECT-SPECIFIC SIGNIFICANT EFFECTS WHICH ARE PECULIAR TO THE PROJECT OR ITS SITE EXIST.

TENTATIVE PARCEL MAP FINDINGS

- H. THE PROPOSED VESTING TENTATIVE PARCEL MAP TOGETHER WITH THE PROVISIONS FOR THE PROJECT'S DESIGN AND IMPROVEMENTS, ARE CONSISTENT WITH THE CITY'S SUBDIVISION ORDINANCE AND THE SUBDIVISION MAP ACT IN THAT THE PROJECT IS SUBJECT TO CONDITIONS OF APPROVAL THAT WILL ENSURE THAT THE PROJECT IS DEVELOPED IN COMPLIANCE WITH CITY STANDARDS.
- I. AS CONDITIONED, THE DESIGN OF THE VESTING TENTATIVE PARCEL MAP AND THE PROPOSED IMPROVEMENTS ARE NOT LIKELY TO CAUSE SUBSTANTIAL ENVIRONMENTAL DAMAGE OR SUBSTANTIALLY AND AVOIDABLY INJURE FISH OR WILDLIFE OR THEIR HABITAT.
- J. THE DESIGN OF THE VESTING TENTATIVE PARCEL MAP AND THE TYPE OF PROPOSED IMPROVEMENTS ARE NOT LIKELY TO CAUSE SERIOUS PUBLIC HEALTH OR SAFETY PROBLEMS.
- K. THE DESIGN OF THE VESTING TENTATIVE PARCEL MAP AND THE TYPE OF IMPROVEMENTS WILL NOT CONFLICT WITH EASEMENTS FOR ACCESS THROUGH OR USE OF PROPERTY WITHIN THE PROPOSED PROJECT.
- L. THE PROPOSED PROJECT, TOGETHER WITH THE PROVISIONS FOR ITS DESIGN AND IMPROVEMENTS, IS CONSISTENT WITH THE GENERAL PLAN, THE FOLSOM PLAN AREA SPECIFIC PLAN, AND ALL APPLICABLE PROVISIONS OF THE FOLSOM MUNICIPAL CODE.

- M. THE SITE IS PHYSICALLY SUITABLE FOR THE TYPE OF DEVELOPMENT PROPOSED.
- N. THE SITE IS PHYSICALLY SUITABLE FOR THE PROPOSED DENSITY OF THE DEVELOPMENT.
- O. SUBJECT TO SECTION 66474.4 OF THE SUBDIVISION MAP ACT, THE LAND IS NOT SUBJECT TO A CONTRACT ENTERED INTO PURSUANT TO THE CALIFORNIA LAND CONSERVATION ACT OF 1965 (COMMENCING WITH SECTION 51200 OF THE GOVERNMENT CODE).

PLANNED DEVELOPMENT PERMIT FINDINGS

- P. THE PROPOSED PROJECT COMPLIES WITH THE INTENT AND PURPOSES OF CHAPTER 17.38 (PLANNED DEVELOPMENT DISTRICT) OF THE FOLSOM MUNICIPAL CODE, OTHER APPLICABLE ORDINANCES OF THE CITY, AND THE GENERAL PLAN.
- Q. THE PROPOSED PROJECT IS CONSISTENT WITH THE OBJECTIVES, POLICIES, AND REQUIREMENTS OF THE DEVELOPMENT STANDARDS OF THE CITY.
- R. THE PHYSICAL, FUNCTIONAL AND VISUAL COMPATIBILITY BETWEEN THE PROPOSED PROJECT AND NEIGHBORING USES AND NEIGHBORHOOD CHARACTERISTICS IS ACCEPTABLE.
- S. THERE ARE AVAILABLE NECESSARY PUBLIC FACILITIES, INCLUDING BUT NOT LIMITED TO, WATER, SEWER AND DRAINAGE TO ALLOW FOR THE DEVELOPMENT OF THE PROJECT SITE IN A MANNER CONSISTENT WITH THIS PROPOSAL.
- T. THE PROPOSED PROJECT WILL NOT CAUSE UNACCEPTABLE VEHICULAR TRAFFIC LEVELS ON SURROUNDING ROADWAYS, AND THE PROPOSED PROJECT WILL PROVIDE ADEQUATE INTERNAL CIRCULATION, INCLUDING INGRESS AND EGRESS.
- U. THE PROPOSED PROJECT WILL NOT BE DETRIMENTAL TO THE HEALTH, SAFETY AND GENERAL WELFARE OF THE PERSONS OR PROPERTY WITHIN THE VICINITY OF THE PROJECT SITE, AND THE CITY AS A WHOLE.
- V. ADEQUATE PROVISION IS MADE FOR THE FURNISHING OF SANITATION SERVICES AND EMERGENCY PUBLIC SAFETY SERVICES TO THE DEVELOPMENT.
- W. AS CONDITIONED, THE PROPOSED PROJECT WILL NOT CAUSE ADVERSE ENVIRONMENTAL IMPACTS WHICH HAVE NOT BEEN MITIGATED TO AN ACCEPTABLE LEVEL.

Submitted,



PAM JOHNS
Community Development Director

CONDITIONS

See attached tables of conditions for which the following legend applies.

RESPONSIBLE DEPARTMENT		WHEN REQUIRED	
CD	Community Development Department	I	Prior to approval of Improvement Plans
(P)	Planning Division	M	Prior to approval of Final Map
(E)	Engineering Division	B	Prior to issuance of first Building Permit
(B)	Building Division	O	Prior to approval of Occupancy Permit
(F)	Fire Division	G	Prior to issuance of Grading Permit
PW	Public Works Department	DC	During construction
PR	Park and Recreation Department	OG	On-going requirement
PD	Police Department		

CONDITIONS OF APPROVAL FOR THE SHOPS AT FOLSOM RANCH PROJECT (PN 18-179) SOUTHEAST CORNER OF THE INTERSECTION OF EAST BIDWELL STREET AND ALDER CREEK PARKWAY VESTING TENTATIVE PARCEL MAP AND PLANNED DEVELOPMENT PERMIT			
Condition No.	Condition of Approval	When Required	Responsible Department
GENERAL REQUIREMENTS			
1.	<p>Final Development Plans The owner/applicant shall submit final site development plans to the Community Development Department that shall substantially conform to the exhibits referenced below:</p> <ol style="list-style-type: none"> 1. Vicinity Map 2. Vesting Tentative Parcel Map, dated October 25, 2018 3. Preliminary Site Plan, dated October 25, 2018 4. Preliminary Grading and Drainage Plan, dated October 25, 2018 5. Preliminary Utility Plan, dated October 25, 2018 6. Preliminary Landscape Plan and Tree Shading Exhibit, dated October 5, 2018 7. Preliminary Site Furnishings and Courtyard Exhibit, dated September 4, 2018 8. Onsite Bicycle and Pedestrian Circulation Exhibit, dated October 5, 2018 9. Preliminary Off-Site Roadway Configuration Exhibit, dated October, 2018 10. Preliminary Off-Site Utility Exhibit, dated October, 2018 11. Preliminary On-Site and Off-Site Grading Exhibit, dated October, 2018 12. Building Elevations, dated August 31, 2018 and October 5, 2018 13. Building Floor Plans, dated October 5, 2018 14. Building Renderings and Perspectives, dated August 31, 2018 and October 5, 2018 15. Color and Materials Board, dated August 31, 2018 16. Roof Screening and Lighting Exhibit, dated August 31, 2018 17. Uniform Sign Criteria for The Shops at Folsom Ranch, dated August 31, 2018 <p>The Vesting Tentative Parcel Map and Planned Development Permit are approved for the development of a 27,900-square-foot shopping center (Shops at Folsom Ranch). Implementation of the project shall be consistent with the above referenced items and these conditions of approval.</p>	G, I, M, B	CD (P)(E)

**CONDITIONS OF APPROVAL FOR THE SHOPS AT FOLSOM RANCH PROJECT (PN 18-179)
SOUTHEAST CORNER OF THE INTERSECTION OF EAST BIDWELL STREET AND ALDER CREEK PARKWAY
VESTING TENTATIVE PARCEL MAP AND PLANNED DEVELOPMENT PERMIT**

Condition No.	Condition of Approval	When Required	Responsible Department
2.	<p>Plan Submittal All civil engineering, improvement, and landscape and irrigation plans, shall be submitted to the Community Development Department for review and approval to ensure conformance with this approval and with relevant codes, policies, standards and other requirements of the City of Folsom.</p>	G, I, M,	CD (P)(E)(B)
3.	<p>Validity The project approvals granted under this staff report shall remain in effect for two years from final date of approval (November 7, 2020). Failure to obtain a building permit within this time period, without the subsequent extension of this Vesting Tentative Parcel Map and Planned Development Permit, shall result in the termination of this Vesting Tentative Parcel Map and Planned Development Permit.</p>	OG	CD (P)
4.	<p>Improvements in the PFFP The owner/applicant shall be subject to all thresholds, timelines and deadlines for the construction and final completion of various improvements for the entire Folsom Plan Area. The various improvements are outlined and detailed in the Folsom Plan Area Specific Plan Public Facilities Financing Plan (PFFP) dated January 28, 2014 and adopted by City of Folsom Resolution No. 9298. These improvements in the PFFP include, but are not limited to, the backbone infrastructure water (water reservoirs, water transmission mains, booster pump stations, pressure reducing valve stations, etc.), sanitary sewer (lift stations and forced mains) systems, recycled water mains and associated infrastructure, roadway and transportation (future interchanges, major arterial roadways, etc.) improvements, aquatic center (community pool), parks, fire stations, municipal services center, community library, etc The thresholds and timelines included in the PFFP require facilities to be constructed and completed based on number of building permits issued and in some cases, number of residential units that are occupied. The owner/applicant shall be required to address these thresholds and timelines as the project moves forward through the various development stages and shall be subject to the various fair share requirements, subject to the provisions of the PFFP, the ARDA and any amendment thereto.</p>	M, B, O	CD(E)(P)(B), PW, FD, EWR, PR

CONDITIONS OF APPROVAL FOR THE SHOPS AT FOLSOM RANCH PROJECT (PN 18-179) SOUTHEAST CORNER OF THE INTERSECTION OF EAST BIDWELL STREET AND ALDER CREEK PARKWAY VESTING TENTATIVE PARCEL MAP AND PLANNED DEVELOPMENT PERMIT			
Condition No.	Condition of Approval	When Required	Responsible Department
5.	<p><i>Indemnity for City</i> The owner/applicant shall protect, defend, indemnify, and hold harmless the City and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void, or annul any approval by the City or any of its agencies, departments, commissions, agents, officers, employees, or legislative body concerning the project, which claim, action or proceeding is brought within the time period provided therefore in Government Code Section 66499.37 or other applicable statutes of limitation. The City will promptly notify the owner/applicant of any such claim, action or proceeding, and will cooperate fully in the defense. If the City should fail to cooperate fully in the defense, the owner owner/applicant shall not thereafter be responsible to defend, indemnify and hold harmless the City or its agents, officers, and employees, pursuant to this condition. The City may, within its unlimited discretion, participate in the defense of any such claim, action or proceeding if both of the following occur:</p> <ul style="list-style-type: none"> • The City bears its own attorney's fees and costs; and • The City defends the claim, action or proceeding in good faith <p>The owner/applicant shall not be required to pay or perform any settlement of such claim, action or proceeding unless the settlement is approved by the owner/applicant. The owner/applicant's obligations under this condition shall apply regardless of whether a Final Map is ultimately recorded with respect to this project.</p> <p><i>Vesting Tentative Parcel Map and Planned Development Permit</i> The Vesting Tentative Parcel Map and Planned Development Permit are expressly conditioned upon compliance with all applicable environmental mitigation measures in the Folsom Plan Area Specific Plan (FEIR/EIS) as amended by the Westland/Eagle Specific Plan Amendment CEQA Addendum, the Folsom South of U.S. Highway 50 Specific Plan Revised Proposed Off-Site Water Facility Alternative CEQA Addendum, as well as compliance with the mitigation measures in the South of U.S. Highway 50 Backbone Infrastructure Project Mitigated Negative Declaration.</p>	OG	CD (P)(E)(B) PW, PR, FD, PD
6.	<p><i>Vesting Tentative Parcel Map and Planned Development Permit</i> The Vesting Tentative Parcel Map and Planned Development Permit are expressly conditioned upon compliance with all applicable environmental mitigation measures in the Folsom Plan Area Specific Plan (FEIR/EIS) as amended by the Westland/Eagle Specific Plan Amendment CEQA Addendum, the Folsom South of U.S. Highway 50 Specific Plan Revised Proposed Off-Site Water Facility Alternative CEQA Addendum, as well as compliance with the mitigation measures in the South of U.S. Highway 50 Backbone Infrastructure Project Mitigated Negative Declaration.</p>	OG	CD

**CONDITIONS OF APPROVAL FOR THE SHOPS AT FOLSOM RANCH PROJECT (PN 18-179)
SOUTHEAST CORNER OF THE INTERSECTION OF EAST BIDWELL STREET AND ALDER CREEK PARKWAY
VESTING TENTATIVE PARCEL MAP AND PLANNED DEVELOPMENT PERMIT**

Condition No.	Condition of Approval	When Required	Responsible Department
7.	<p>ARDA and Amendments The owner/applicant shall comply with all provisions of Amendments No. 1 and 2 to the First Amended and Restated Tier 1 Development Agreement and any approved amendments thereafter by and between the City and the owner/applicant of the project.</p>	G, I, M, B	CD (E)
8.	<p>Mitigation Monitoring The owner/applicant shall participate in a mitigation monitoring and reporting program pursuant to City Council Resolution No. 2634 and Public Resources Code 21081.6. The mitigation monitoring and reporting measures identified in the Folsom Plan Area Specific Plan FEIR/EIS, the South of 50 Backbone Infrastructure Project MND, the Westland/Eagle Specific Plan Amendment to the FPASP and Addendum to the FPASP EIR/EIS, and the Folsom South of U.S. Highway 50 Specific Plan Project Revised Proposed Off-Site Water Facility Alternative Amendment to the FPASP and Addendum to the FPASP EIR/EIS have been incorporated into these conditions of approval in order to mitigate or avoid significant effects on the environment. These mitigation monitoring and reporting measures are identified in the mitigation measure column. Applicant shall fund on a Time and Materials basis all mitigation monitoring (e.g., staff and consultant time).</p>	OG	CD (P)

DEVELOPMENT COSTS AND FEE REQUIREMENTS			
9.	<p>Taxes and Fees The owner/applicant shall pay all applicable taxes, fees and charges for the project at the rate and amount required by the Public Facilities Financing Plan and Amendment No. 1 to the Amended and Restated Tier 1 Development Agreement.</p>	OG	CD (P)(E)
10.	<p>Assessments If applicable, the owner/applicant shall pay off any existing assessments against the property, or file necessary segregation request and pay applicable fees.</p>	OG	CD (E)
11.	<p>FPASP Development Impact Fees The owner/applicant shall be subject to all Folsom Plan Area Specific Plan Area development impact fees in place at the time of approval or subsequently adopted consistent with the Public Facilities Financing Plan (PFFP), Development Agreement and amendments thereto, unless exempt by previous agreement. The owner/applicant shall be subject to all applicable Folsom Plan Area plan-wide development impact fees in effect at such time that a building permit is issued. These fees may include, but are not limited to, the Folsom Plan Area Specific Plan Fee, Solid Waste Fee, Corporation Yard Fee, Transportation Management Fee, Transit Fee, Highway 50 Interchange Fee, General Park Equipment Fee, Housing Trust Fee, etc.</p> <p>Any protest to such for all fees, dedications, reservations or other exactions imposed on this project will begin on the date of final approval (November 7, 2018), or otherwise shall be governed by the terms of Amendments No. 1 and 2 to ARDA. The fees shall be calculated at the fee rate set forth in the PFFP and the ARDA.</p>	B	CD (P), PW, PK
12.	<p>Legal Counsel The City, at its sole discretion, may utilize the services of outside legal counsel to assist in the implementation of this project, including, but not limited to, drafting, reviewing and/or revising agreements and/or other documentation for the project. If the City utilizes the services of such outside legal counsel, the City shall provide notice to the owner/applicant of the outside counsel selected, the scope of work and hourly rates, and the owner/applicant shall reimburse the City for all outside legal fees and costs incurred and documented by the City for such services. The owner/applicant may be required, at the sole discretion of the City Attorney, to submit a deposit to the City for these services prior to initiation of the services. The owner/applicant shall be responsible for reimbursement to the City for the services regardless of whether a deposit is required.</p>	OG	CD (P)(E)

13.		<p>Consultant Services If the City utilizes the services of consultants to prepare special studies or provide specialized design review or inspection services for the project, the City shall provide notice to the owner/applicant of the outside consultant selected, the scope of work and hourly rates, and the owner/applicant shall reimburse the City for actual costs incurred and documented in utilizing these services, including administrative costs for City personnel. A deposit for these services shall be provided prior to initiating review of the Grading Plan, Final Map, improvement plans, or first inspection, whichever is applicable.</p>	G, I, M, B	CD (P)(E)
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MAJOR INFRASTRUCTURE/GRADING REQUIREMENTS

Development Plan

The owner/applicant shall construct the following improvements as shown on the Vesting Tentative Parcel Map, Preliminary Site Plan, Preliminary Grading and Drainage Plan, Preliminary Utility Plan, Preliminary Off-Site Roadway Configuration Exhibit, and Preliminary Off-Site Utility Exhibit. Roadways shall be to the ultimate horizontal and vertical alignment unless otherwise noted.

▪ **Roads**

• East Bidwell Street

Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure East Bidwell Street as follows:

- On the west side of the existing East Bidwell Street fronting the Project, construct a raised median and one new southbound lane. Realign southbound traffic to be on the west side of the median. The raised median shall extend south from Alder Creek Parkway, past Old Ranch Way, to prevent left turns out from the Project's East Bidwell Street driveway and from Old Ranch Way.
- Construct roadway transitions for the new southbound alignment, north of Alder Creek Parkway and south of Old Ranch Way.
- Restripe pavement on the east side of the new median for exclusive northbound use (Stripe as one U-turn lane in a 100-foot (or longer) pocket plus 58-foot taper, one through lane, a 5-foot bike lane, and one right-turn lane in a 150-foot pocket plus 58-foot taper) as described below for the East Bidwell Street/Alder Creek Parkway, East Bidwell Street/Old Ranch Way, and East Bidwell Street/Project Driveway intersections.
- Frontage improvements including curb, gutter, sidewalk, and landscaping along the east side of East Bidwell Street from Old Ranch Way to Alder Creek Parkway to complete roadway section "1" as shown on the Vesting Tentative Parcel Map.

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14.Cont.	<ul style="list-style-type: none"> ● <u>Alder Creek Parkway</u> Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure Alder Creek Parkway as follows: <ul style="list-style-type: none"> ○ Widen eastbound Alder Creek Parkway between East Bidwell Street and the Eastern Project boundary to two through lanes plus turn pocket as described below for the Alder Creek Parkway/Western Project Driveway intersection. ○ Frontage improvements including curb, gutter, sidewalk, and landscaping along the south side of Alder Creek Parkway from East Bidwell Street to the Eastern project boundary to complete roadway section “B” as shown on the Vesting Tentative Parcel Map. ● <u>Old Ranch Way</u> Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure Old Ranch Way as follows: <ul style="list-style-type: none"> ○ Construct Old Ranch Way within ultimate right-of-way as a two-lane urban street, between East Bidwell Street and the Old Ranch Way Project Driveway. ○ Frontage improvements including curb, gutter, sidewalk, and landscaping along the north side of Old Ranch Way from East Bidwell Street to the Eastern project boundary to complete roadway section “S” as shown on the Vesting Tentative Parcel Map. ● <u>East Bidwell Street/Alder Creek Parkway Intersection</u> Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure the East Bidwell Street/Alder Creek Parkway Intersection as follows: <ul style="list-style-type: none"> ○ Northbound Approach: Stripe as one U-turn lane in a 100-foot (or longer) pocket plus 58-foot taper, one through lane, a 5-foot bike lane, and one 150-foot right-turn lane plus 58-foot taper. Widen the west side of East Bidwell Street at the intersection to accommodate U-turns as shown on the Roadway Configuration Exhibit. ○ Southbound Approach: Construct as one through lane, and one left-turn lane in a 100-foot (or longer) pocket plus 58-foot taper. 	O, I, G	CD (E), EWR, PW, FD
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14.Cont.	<ul style="list-style-type: none"> ○ Westbound Approach: Retain existing westbound geometry. ○ Control: Retain existing westbound stop-control. ○ The southeast corner of the intersection shall include a “bulb-out” into Alder Creek Parkway that facilitates turn pockets described for the Alder Creek Parkway/Western Project Driveway intersection below. ● <u>East Bidwell Street/East Bidwell Street Project Driveway Intersection</u> Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure East Bidwell Street/East Bidwell Street Project Driveway Intersection as follows: <ul style="list-style-type: none"> ○ Northbound Approach: Stripe as one through lane, and one right-turn in a 150-foot pocket plus a 58-foot taper, and a 5-foot bike lane. ○ Southbound Approach: Construct as one through lane on the west side of the median. ○ Westbound Driveway: Construct as a single right-turn lane. ○ Eastbound Driveway: Construct a two-lane entrance as shown on the preliminary site plan ○ A raised median on East Bidwell Street is included to prevent left-turns out from the Project Driveway. ● <u>East Bidwell Street/Old Ranch Way Intersection</u> Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure East Bidwell Street/Old Ranch Way Intersection as follows: <ul style="list-style-type: none"> ○ Northbound Approach: Stripe as shared through-right lane with a 58-foot taper for the right-turn. ○ Southbound Approach: Construct as one through lane, and one left-turn lane in a 90-foot pocket plus 58-foot taper. ○ Westbound Approach: Construct as a single right-turn only lane. 	O, I, G	CD (E), EWR, PW, FD
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14.Cont.	<ul style="list-style-type: none"> ○ A raised median or curb is included on East Bidwell Street to prevent left-turns out from Old Ranch Way. There shall be no westbound left-turn from Old Ranch Way onto East Bidwell Street. ● <u>Alder Creek Parkway/Western Project Driveway Intersection</u> Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure Alder Creek Parkway/Western Project Driveway Intersection as follows: <ul style="list-style-type: none"> ○ Eastbound Approach: Construct as two through lanes and one right-turn deceleration lane in a 130-foot pocket plus 30-foot taper, and 5-foot bike lane. ○ Northbound Driveway: Construct as a single right-turn only lane. ○ Driveway Entrance: Construct as a single lane as shown on the preliminary site plan. ● <u>Alder Creek Parkway/Eastern Project Driveway Intersection</u> Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure Alder Creek Parkway/Eastern Project Driveway Intersection as follows: <ul style="list-style-type: none"> ○ Eastbound Approach: Construct as two through lanes, and 5-foot bike lane. ○ Northbound Driveway: Construct as a single right-turn only lane. ○ Driveway Entrance: Construct as a single lane as shown on the preliminary site plan. ● <u>Old Ranch Way/Old Ranch Way Project Driveway Intersection</u> Prior to issuance of the first certificate of occupancy, the owner/applicant shall configure Old Ranch Way/Old Ranch Way Project Driveway Intersection: <ul style="list-style-type: none"> ○ Eastbound Approach: Construct as one lane ○ Westbound Approach: Construct as one lane. 	CD (E), EWR, PW, FD
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14.Cont.		<ul style="list-style-type: none"> o Southbound Driveway: Construct one shared lane for outgoing left and right turns from the Project. Left-turns out of the driveway will not be permitted until future extension of Old Ranch Way east. o Driveway Entrance: Construct as a single lane as shown on the preliminary site plan. 	O, I, G	CD (E), EWR, PW, FD
15.		<p>Off-site improvements / Rights of Entry For any improvements constructed on private property that are not under the ownership or control of the owner/applicant, all rights-of-entry, and if necessary, a permanent easement shall be obtained and provided to the City. All rights of entry, construction easements, either permanent or temporary and other easements shall be obtained as set forth in Amendments No. 1 and 2 to ARDA, which shall be fully executed by all affected parties and shall be recorded with the Sacramento County Recorder, where applicable, prior to approval of grading and/or improvement plans.</p>	G, I	CD (E)
16.		<p>Mine Shaft Remediation owner/applicant shall locate and remediate all antiquated mine shafts, drifts, open cuts, tunnels, and water conveyance or impoundment structures existing on the project site, with specific recommendations for the sealing, filling, or removal of each that meet all applicable health, safety and engineering standards. Recommendations shall be prepared by an appropriately licensed engineer or geologist. All remedial plans shall be reviewed and approved by the City prior to approval of grading plans.</p>	G	CD (E)

17.	<p>Prepare Traffic Control Plan.</p> <p>Prior to construction, a Traffic Control Plan for roadways and intersections affected by construction shall be prepared. The Traffic Control Plan shall designate haul routes and comply with requirements in the encroachment permits issued by the City of Rancho Cordova, Sacramento County, and Caltrans and any other local agencies, including but not limited to the City, if applicable. The Traffic Control Plan to be prepared by the project construction contractor(s) shall, at minimum, include the following measures:</p> <ul style="list-style-type: none"> • Maintaining the maximum amount of travel lane capacity during non-construction periods, possible, and advanced notice to drivers through the provision of construction signage. • Maintaining alternate one-way traffic flow past the lay down area and site access when feasible. • Heavy trucks and other construction transport vehicles shall avoid the busiest commute hours (7 a.m. to 8 a.m. and 5 p.m. to 6 p.m. on weekdays). • A minimum 72-hour advance notice of access restrictions for residents, businesses, and local emergency response agencies. This shall include the identification of alternative routes and detours to enable for the avoidance of the immediate construction zone. • A phone number and community contact for inquiries about the schedule of the construction throughout the construction period. This information will be posted in a local newspaper, via the City's web site, or at City Hall and will be updated on a monthly basis. 	G, I	CD (E)
18.	<p>State and Federal Permits</p> <p>The owner/applicant shall obtain all required State and Federal permits and provide evidence that said permits have been obtained, or that the permit is not required, subject to staff review prior to approval of any grading or improvement plan.</p>	G, I	CD (P)(E)
19.	<p>Water Quality Certification</p> <p>A water quality certification pursuant to Section 401 of the Clean Water Act is required before issuance of the record of decision and before issuance of the Section 404 permit. Before construction in any areas containing wetland features, the owner/applicant shall obtain water quality certification for the project. Any measures required as part of the issuance of water quality certification shall be implemented pursuant to the permit conditions.</p>	G	CD (E)

20.	Landslide /Slope Failure The owner/applicant shall retain an appropriately licensed engineer during the grading activities to identify existing landslides and potential slope failure hazards. The said engineer shall be notified a minimum of two days prior to any site clearing or grading to facilitate meetings with the grading contractor in the field.	G	CD (E) PW
IMPROVEMENT PLAN REQUIREMENTS			
21.	Improvement Plans The improvement plans for the required public and private improvements, including but not limited to street and frontage improvements on Alder Creek Parkway, East Bidwell Street, and Old Ranch Way shall be reviewed and approved by the Community Development Department prior to issuance of the Building Permit.	B	CD (E)
22.	Required Improvements Required public and private improvements, including but not limited to street and frontage improvements on Alder Creek Parkway, East Bidwell Street, and Old Ranch Way shall be completed prior to the first issuance of a Certificate of Occupancy.	O	CD (E)
23.	Sewer/Water/Drainage Studies The owner/applicant shall submit water, sewer and drainage studies to the satisfaction of the Community Development Department and provide sanitary sewer, water and storm drainage improvements with corresponding easements and quit claims, as necessary, in accordance with these studies and the current edition of the City of Folsom <u>Standard Construction Specifications</u> and the <u>Design and Procedures Manual and Improvement Standards</u> .	I	CD (E)
24.	Lot and Building Configurations Final lot and building configurations may be modified to allow for overland release of storm events greater than the capacity of the underground system.	I, G	CD (E)
25.	Standard Construction Specifications and Details Public and private improvements, including roadways, curbs, gutters, sidewalks, bicycle lanes and trails, streetlights, underground infrastructure and all other improvements shall be provided in accordance with the latest edition of the City of Folsom <u>Standard Construction Specifications and Details</u> and the <u>Design and Procedures Manual and Improvement Standards</u> .	I	CD (P)(E)

26.		<p>Water and Sewer Infrastructure</p> <p>All City-owned water and sewer infrastructure shall be placed within the street right of way or public utility easements as approved by the Community Development Department. In the event that a City-maintained public water or sewer main needs to be placed in an area other than the public right of way or public utility easement, such as through an open space corridor, landscaped area, etc., the following criteria must be met;</p> <ul style="list-style-type: none"> • The owner/applicant shall provide public sewer and water main easements • An access road shall be designed and constructed to allow for the operations, maintenance and replacement of the public water or sewer line by the City along the entire water and/or sewer line alignment. • In no case shall a City-maintained public water or public sewer line be placed on private residential property. • The domestic water and irrigation system owned and maintained by the City shall be separately metered per City of Folsom <u>Standard Construction Specifications and Details</u>. 	I	CD (E)
27.		<p>Lighting Plan</p> <p>Final exterior building and site lighting plans shall be submitted for review and approval by Community Development Department for location, height, aesthetics, level of illumination, glare and trespass prior to the issuance of any building permits. All lighting, including but not limited to freestanding parking lot lights, landscape and walkway lights, festoon light strings, and building attached lights shall be screened, shielded, and directed downward onto the project site and away from adjacent properties and public rights-of-way. The final design of all exterior lighting shall be subject to review and approval by the Community Development Department. Lighting shall be equipped with a timer or photo condenser. In addition, pole-mounted parking lot lights shall utilize a low-intensity, energy efficient lighting method.</p>	I, B	CD (P)
28.		<p>Utility Coordination</p> <p>The owner/applicant shall coordinate the planning, development and completion of this project with the various utility agencies (i.e., SMUD, PG&E, etc.). The owner/applicant shall provide the City with written confirmation of public utility service prior to recording the Parcel Map.</p>	M	CD (P)(E)

29.		<p>Replacing Hazardous Facilities The owner/applicant shall be responsible for replacing any and all damaged or hazardous public sidewalk, curb and gutter, and/or bicycle trail facilities along the site frontage and/or boundaries, including pre-existing conditions and construction damage, to the satisfaction of the Community Development Department.</p>	I, OG	CD (E)
30.		<p>Future Utility Lines All future utility lines lower than 69 KV that are to be built within the project, shall be placed underground within and along the perimeter of the project at the developer's cost. The owner/applicant shall dedicate to SMUD all necessary underground easements for the electrical facilities that will be necessary to service development of the project.</p>	I, O	CD (E)
31.		<p>Water Meter Fixed Network System The owner owner/applicant shall pay for, furnish and install all infrastructure associated with the water meter fixed network system for any City-owned and maintained water meters within the project.</p>	I	CD (E), EWR
32.		<p>Vertical Curb All curbs located adjacent to landscaping, whether natural or manicured, and where parking is allowed shall be vertical.</p>	I	CD (P)(B)
33.		<p>Class II Bike Lanes All Class II bike lanes shall be striped and the legends painted green. No parking shall be permitted within the Class II bike lanes.</p>	I	CD (E)(P)

34.

Master Plan Updates

The City has approved the Folsom Plan Area Storm Drainage Master Plan, Wastewater Master Plan, and Water Master Plan. The owner/applicant shall submit complete updates to the approved master plans, if applicable, for the proposed changes to the master plans as a result of the proposed project. The updates to the master plans for the proposed project shall be reviewed and approved by the City prior to approval of grading and/or improvement plans.

The plans shall be accompanied by engineering studies supporting the sizing, location, and timing of the proposed facilities. Improvements shall be constructed in phases as the project develops in accordance with the approved master plans, including any necessary off-site improvements to support development of a particular phase or phases, subject to prior approval by the City. Off-site improvements may include roadways to provide secondary access, water transmission lines or distribution facilities to provide a looped water system, sewer trunk mains and lift stations, water quality facilities, non-potable water pipelines and infrastructure, and drainage facilities including on or off-site detention. No changes in infrastructure from that shown on the approved master plan shall be permitted unless and until the applicable master plan has been revised and approved by the City. Final lot configurations may need to be modified to accommodate the improvements identified in these studies to the satisfaction of the City.

The owner/applicant shall provide sanitary sewer, water and storm drainage improvements with corresponding easements, as necessary, in accordance with these studies and the latest edition of the City of Folsom Standard Construction Specifications and Details, and the Design and Procedures Manual and Improvement Standards.

The storm drainage design shall provide for no net increase in run-off under post-development conditions.

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35.	<p>Best Management Practices The storm drain improvement plans shall provide for "Best Management Practices" that meet the requirements of the water quality standards of the City's National Pollutant Discharge Elimination System Permit issued by the State Regional Water Quality Control Board.</p> <p>In addition to compliance with City ordinances, the owner/applicant shall prepare a Stormwater Pollution Prevention Plan (SWPPP), and implement Best Management Practices (BMPs) that comply with the General Construction Stormwater Permit from the Central Valley RWQCB, to reduce water quality effects during construction. Detailed information about the SWPPP and BMPs are provided in Chapter 3A.9, "Hydrology and Water Quality."</p> <p>Each proposed project development shall result in no net change to peak flows into Alder Creek and associated tributaries, or to Buffalo Creek, Carson Creek, and Coyote Creek. The owner/applicant shall establish a baseline of conditions for drainage on-site. The baseline-flow conditions shall be established for 2-, 5-, and 100-year storm events. These baseline conditions shall be used to develop monitoring standards for the stormwater system on the Specific Plan Area. The baseline conditions, monitoring standards, and a monitoring program shall be submitted to USACE and the City for their approval. Water quality and detention basins shall be designed and constructed to ensure that the performance standards, which are described in Chapter 3A.9, "Hydrology and Water Quality," are met and shall be designed as off-stream detention basins.</p> <p>Discharge sites into Alder Creek and associated tributaries, as well as tributaries to Carson Creek, Coyote Creek, and Buffalo Creek, shall be monitored to ensure that project conditions are being met. Corrective measures shall be implemented as necessary. The mitigation measures will be satisfied when the monitoring standards are met for 5 consecutive years without undertaking corrective measures to meet the performance standard.</p>	G, I	CD (E)
36.	<p>Litter Control During Construction, the owner/applicant shall be responsible for litter control and sweeping of all paved surfaces in accordance with City standards. All on-site storm drains shall be cleaned immediately before the commencement of the rainy season (October 15).</p>	OG	CD (E)

37.		<p>Trash/Recycling The final location, orientation, design, materials, and colors of the trash/recycling enclosures is subject to review and approval by the Community Development Department.</p>	I, B	CD (P)
38.		<p>Walls/Fencing The final location, design, height, materials, and colors of the retaining wall, sound wall, and decorative fencing be subject to review and approval by the Community Development Department to ensure consistency with the overall design of the proposed shopping center.</p>	B, G	CD (P)(E)

FIRE DEPT REQUIREMENTS

39.

All-Weather Access and Fire Hydrants

The owner/applicant shall provide all-weather access and fire hydrants before combustible materials are allowed on any project site or other approved alternative method as approved by the Fire Code Official/Fire Chief. All-weather emergency access roads and fire hydrants (tested and flushed) shall be provided before combustible material or vertical construction is allowed on any project site or other approved alternative method as approved by the Fire Code Official/Fire Chief. (All-weather access is defined as six inches of compacted aggregate base from May 1 to September 30 and two inch asphalt concrete over six inch aggregate base from October to April 30). The building shall have illuminated addresses visible from the street or drive fronting the property. Size and location of address identification shall be reviewed and approved by the Fire Marshal.

- Commercial Fire-Flow with Automatic Fire Sprinkler System: The required fire-flow for the general commercial portion of the project is determined to be 750 GPM for three hours. The reduced fire-flow shall not be less than 1,000 GPM for commercial buildings with automatic sprinkler systems per Section 903.1.1 of the CFC, and shall not be less than 1,500 GPM for commercial buildings with automatic sprinkler systems per Section 903.3.1.2 of the CFC.
- All public streets shall meet City of Folsom Street Standards unless an alternative is specifically included within this approval.
- The maximum length of any dead end street shall not exceed 500 feet in accordance with the Folsom Fire Code (unless approved by the Fire Department). Several streets indicated on the plans are dead ends greater than 500 feet. In such cases, a second emergency access will be required.
- All-weather emergency access roads and fire hydrants (tested and flushed) shall be provided before combustible material storage or vertical construction is allowed. All-weather access is defined as 6" of compacted AB from May 1 to September 30 and 2"AC over 6" AB from October 1 to April 30
- The first Fire Station planned for the Folsom Plan Area shall be completed and operational at the time that the threshold of 1,500 occupied homes within the Folsom Plan Area is met.

G, I, M, B

CD (P), FD

LANDSCAPE REQUIREMENTS

40.	<p>Landscaping Plans Final landscape plans and specifications shall be prepared by a registered landscape architect and approved by the City prior to the approval of the first Building Permit or Parcel Map, whichever occurs first. Said plans shall include all on-site landscape specifications and details, off-site detention basin landscaping specifications and details, and shall comply with all State and local rules, regulations, Governor's declarations and restrictions pertaining to water conservation and outdoor landscaping.</p>	I, OG	CD(P), PW
41.	<p>Landscaping shall meet shade requirements as outlined in the Folsom Plan Area Specific Plan where applicable. The landscape plans shall comply and implement water efficient requirements as adopted by the State of California (Assembly Bill 1881) (State Model Water Efficient Landscape Ordinance) until such time the City of Folsom adopts its own Water Efficient Landscape Ordinance at which time the owner/applicant shall comply with any new ordinance. Shade and ornamental trees shall be maintained according to the most current American National Standards for Tree Care Operations (ANSI A-300) by qualified tree care professionals (Tree topping for height reduction, view protection, light clearance or any other purpose shall not be allowed). Specialty-style pruning, such as pollarding, shall be specified within the approved landscape plans and shall be implemented during a 5-year establishment and training period. Furthermore, the owner/applicant shall comply with city-wide landscape rules or regulations on water usage. The Owner/applicant shall comply with any state or local rules and regulations relating to landscape water usage and landscaping requirements necessitated to mitigate for drought conditions on all landscaping in the Shops at Folsom Ranch project.</p>	I, OG	CD (P), PW
	<p>Right of Way Landscaping Landscaping along all road rights of way (directly adjacent to the project site) shall be installed when the adjoining road or lots are constructed.</p>		

MAP REQUIREMENTS

42.	<p>Public Utility Easements The owner/applicant shall dedicate public easements for water, sewer, and sidewalks within the private streets, as well as public utility easements for underground public facilities on properties adjacent to the streets. Twelve and one-half-foot (12.5') wide Public Utility Easements for underground public facilities shall be dedicated adjacent to all private and public streets for other public utilities (i.e., SMUD, Pacific Gas and Electric, cable television, telephone). The width of the public utility easements adjacent to public and private streets may be reduced with prior approval from public utility companies. The owner/applicant shall dedicate additional width to accommodate extraordinary facilities as determined by the City. The width of the public utility easements adjacent to public and private right of way may be reduced with prior approval from public utility companies.</p>	M	CD (E)
43.	<p>Reciprocal Easements The owner/applicant shall dedicate all reciprocal access easements for access, parking, sewer, water, landscape, and fire protection systems on the Parcel Map.</p>	M	CD (E)
44.	<p>Backbone Infrastructure As provided for in the ARDA and the Amendment No. 1 thereto, the owner/applicant shall provide fully executed grant deeds, legal descriptions, and plats for all necessary Backbone Infrastructure to serve the project, including but not limited to lands, public rights of way, public utility easements, public water main easements, public sewer easements, irrevocable offers of dedication and temporary construction easements. All required easements as listed necessary for the Backbone Infrastructure shall be reviewed and approved by the City and recorded with the Sacramento County Recorder pursuant to the timing requirements set forth in Section 3.8 of the ARDA, and any amendments thereto.</p>	I	CD (E)
45.	<p>New Permanent Benchmarks The owner/applicant shall provide and establish new permanent benchmarks on the (NAVD 88) datum in various locations within the project site or at any other locations in the vicinity of the off-site Backbone Infrastructure as directed by the City Engineer. The type and specifications for the permanent benchmarks shall be provided by the City. The new benchmarks shall be placed by the owner/applicant within 6 months from the date of approval of the Parcel Map.</p>	M	CD (E)

46.	Recorded Parcel Map The owner/applicant shall provide a digital copy of the recorded Parcel Map (in AutoCAD format) to the Community Development Department.	O	CD (E)
47.	Deferred Improvement Agreement Prior to the recording of each Parcel Map, the owner/applicant shall enter into a deferred improvement agreement with the City, identifying public improvements, if any, to be constructed with each phase. The owner/applicant shall provide security acceptable to the City, guaranteeing construction of the improvements.	M	CD (E)
48.	Project Phasing If the proposed project is approved in phases, the phasing of improvements shall be to the satisfaction of the Community Development Department. Construction of all public and/or frontage improvements shall be completed with the initial phase.	M, I	CD (E)
49.	SPIF Fees The owner/applicant shall be subject to all Specific Plan Infrastructure Fees (SPIF) prior to recordation of the Parcel Map.	M	CD (E)

TRAFFIC, ACCESS, CIRCULATION, AND PARKING REQUIREMENTS

Traffic Impact Analysis

In accordance with the traffic impact analysis dated, October 24, 2018, prepared by T.KEAR Transportation Planning & Management, the following traffic measures shall be implemented to the satisfaction of the Community Development Department:

- The owner/applicant shall construct an eastbound Left-Turn Lane that provides for U-Turn movement on Alder Creek Parkway at the future location of Westwood Drive as shown in Figure ES-2 of the Traffic Study. The Left-Turn Lane shall be constructed with a 100-foot-long (or longer) turn pocket plus 58-foot taper. Widening on the north side of Alder Creek Parkway is required to accommodate the U-turn movement. The area to be widened is shown conceptually in Figure ES-2 of the Traffic Study, widening shall accommodate turning radius for emergency vehicles and delivery trucks serving the Project.
- If not already constructed, the owner/applicant shall construct a northbound U-turn at the East Bidwell Street/Alder Creek Parkway intersection with a 100-foot long (or longer) pocket plus 58-foot taper.
- If neighboring projects construct a raised median within East Bidwell Street, the owner/applicant shall construct a southbound left-turn through the raised median at the East Bidwell Street/Old Ranch Way intersection with a 90-foot long (or longer) pocket plus 58-foot taper.
- The owner/applicant shall construct a 150-foot left turn pocket with 58-foot taper on eastbound Alder Creek Parkway to facilitate future access into the 50-acre commercial property located on the north side of Alder Creek Parkway as shown on the Offside Roadway Configuration Exhibit (Attachment 9). The left-turn pocket shall be barricaded and striped to prevent left or U-turns prior to development of the 50-acre commercial property (Parcel 85 A), and further analyses of its site access.
- In the event that the fuel station is not developed within the shopping center, the Eastern Driveway on Alder Creek Parkway shall be eliminated, unless authorized by the City Engineer, in order to reduce the number of driveways on Alder Creek Parkway (major arterial roadway) and to reduce the number of driveways in close proximity to the intersection of East Bidwell Street and Alder Creek Parkway.

50.

B

CD (E)

50. Con	<ul style="list-style-type: none"> If the Community Development Director determines that either the P3 or P4 building pad users are likely to have queues of more than 10 cars, analysis of drive through queuing shall be required prior to approval of grading or improvement plans for that parcel. 	G, I	CD (E)(P)
51.	<p>Vehicle and Bicycle Parking</p> <p>A minimum of 207 vehicle parking spaces shall be provided for the project including 13 electric vehicle charging spaces and stations. In addition, a minimum of 21 bicycle parking spaces shall be provided to serve residents.</p>	B	CD (E)
52.	ARCHITECTURE/SITE DESIGN REQUIREMENTS		
	<p>Architectural Requirements</p> <p>The project shall comply with the following architecture and design requirements:</p>		
1.	<p>This approval is for the Shops at Folsom Ranch Shopping Center, which includes development of six, single-story commercial buildings totaling 27,900 feet. The applicant shall submit building plans that comply with this approval and the attached building elevations and building perspectives dated August 31, 2018 and October 5, 2018.</p>		
2.	<p>The design, materials, and colors of the proposed Shops at Folsom Ranch Shopping Center shall be consistent with the submitted building elevations, building perspectives, materials samples, and color scheme to the satisfaction of the Community Development Department.</p>	B	CD (P)
3.	<p>Brick pavers, stamped asphalt or another type of colored masonry material (ADA compliant) shall be used to designate pedestrian walkways and crosswalks on the project site, in addition to where pedestrian paths cross drive aisles, and shall be incorporated as a design feature at the primary driveway entrances at Alder Creek Parkway (westerly driveway) and East Bidwell Street.</p>		
4.	<p>All mechanical equipment shall be concealed from view of public streets, neighboring properties and nearby higher buildings where practicable to the satisfaction of the Community Development Department.</p>		
5.	<p>The final design of the building-attached light fixtures shall be subject to review and approval by the Community Development Department to ensure architectural consistency with the overall building design.</p>		

POLICE/SECURITY REQUIREMENT

55.

Police Requirements

The owner/applicant shall consult with the Police Department in order to incorporate all reasonable crime prevention measures. The following security/safety measures shall be considered:

- A security guard on-duty at all times at the site or a six-foot security fence shall be constructed around the perimeter of construction areas.
- Security measures for the safety of all construction equipment and unit appliances.
- Landscaping shall not cover exterior doors or windows, block line-of-sight at intersections or screen overhead lighting.

G, J, B

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Mitigation Measures

56. **Westland/Eagle Specific Plan Amendment (W/E SPA) Mitigation Monitoring Reporting Program (MMRP) for the Westland/Eagle Area of the Folsom Plan Area Specific Plan (FPASP).** Table 1 below describes the mitigation measures from the FPASP (May 2011) MMRP, as amended by the Revised Proposed Water Supply Facility Alternative (November 2012) and by W/E SPA (September 2015).

Table 1.

FPASP EIR/EIS, as amended by the W/E SPA Addendum, Mitigation Measures Applicable to the Shops at Folsom Ranch Project.*

*The MMRP for the W/E SPA Addendum to the FPASP EIR/EIS is included as Attachment 19.

Condition	Mitigation Measures Applicable to the Project	Timing	Responsible Agency	Notes/Compliance
Aesthetics				
56-1	3A.1-4: Screen Construction Staging Areas.	G	CD (P)(E)(B)	
56-2	3A.1-5: Establish and Require Conformance to Lighting Standards and Prepare and Implement a Lighting Plan.	B	CD (P)	
Air Quality				
56-3	3A.2-1a: Implement Measures to Control Air Pollutant Emissions Generated by Construction of On-Site Elements.	G, C	CD	
56-4	3A.2-1b: Pay Off-site Mitigation Fee to SMAQMD to Off-Set NOX Emissions Generated by Construction of On-Site Elements.	G, C	CD (P)(E) SMAQMD	
56-5	3A.2-1c: Analyze and Disclose Projected PM10 Emission Concentrations at Nearby Sensitive Receptors Resulting from Construction of On-Site Elements.	G	CD (E)(P)	
56-6	3A.2-1d: Implement SMAQMD's Basic Construction Emission Control Practices during Construction of all Off-site Elements located in Sacramento County	G	SMAQMD	
56-7	3A.2-1f: Implement SMAQMD's Enhanced Exhaust Control Practices during Construction of all Off-site Elements.	G	Sacramento County El Dorado County or Cal Trans	
56-8	3A.2-1g: Pay Off-site Mitigation Fee to SMAQMD to Off-Set NOX Emissions Generated by Construction of Off-site Elements.	G	SMAQMD	

56-9	3A.2-1h: Analyze and Disclose Projected PM10 Emission Concentrations at Nearby Sensitive Receptors Resulting from Construction of Off-site Elements	G		
56-10	3A.2-2: Implement All Measures Prescribed by the Air Quality Mitigation Plan to Reduce Operational Air Pollutant Emissions.	M, I	CD	
56-11	3A.2-4a: Develop and Implement a Plan to Reduce Exposure of Sensitive Receptors to Construction-Generated Toxic Air Contaminant Emissions.	G, C	CD	
56-12	3A.2-4b: Implement Measures to Reduce Exposure of Sensitive Receptors to Operational Emissions of Toxic Air Contaminants.	G, C	CD SMAQMD	
56-13	3A.2-5: Implement A Site Investigation to Determine the Presence of NOA and, if necessary, Prepare and Implement an Asbestos Dust Control Plan.	G, C	CD	
56-14	3A.2-6: Implement Measures to Control Exposure of Sensitive Receptors to Operational Odorous Emissions	B, C	CD	
Biological Resources				
56-15	3A.3-1a (as amended by W/E SPA): Mitigation for erosion impacts.	I, OG	PW USACE	
56-16	3A.3-1b (as amended by W/E SPA): Implement Clean Water Act Section 404 Permits and Section 401 Water Quality Certifications.	G, I, OG	CD USACE	Comply with Carpenter Ranch and Backbone 401 and 404 permits and any amendments thereto
56-17	3A.3-2a (as amended by W/E SPA): Avoid Direct Loss of Swainson's Hawk and Other Raptor Nests.	G, I, C	CD CDFW	See Swainson's Hawk and Tricolored Blackbird Mitigation Plans, dated 5/2/17 and 2/21/17
56-18	W/E SPA Mitigation Measure 4.4-4: Conduct preconstruction Swainson's Hawk and other raptor surveys	G	CD (P)(E) CDFW	See Swainson's Hawk and Tricolored Blackbird Mitigation Plans, dated 5/2/17 and 2/21/17
56-19	3A.3-2b: Prepare and Implement a Swainson's Hawk Mitigation Plan.	G, I, C	CD	See Swainson's Hawk and Tricolored Blackbird Mitigation Plans, dated 5/2/17 and 2/21/17

56-20	W/E SPA Mitigation Measure 4.4-5: Prepare and Implement Swainson's hawk mitigation plan.	G	CD (P)(E) CDFW	See Swainson's Hawk and Tricolored Blackbird Mitigation Plans, dated 5/2/17 and 2/21/17
56-21	3A.3-2c (as amended by W/E SPA): Conduct preconstruction Tricolored Blackbird Nesting surveys.	G	CD CDFW	See Swainson's Hawk and Tricolored Blackbird Mitigation Plans, dated 5/2/17 and 2/21/17
56-22	W/E SPA Mitigation Measure 4.4-6: Conducting preconstruction burrowing owl survey.	G	CD (P)(E) CDFW	
56-23	W/E SPA Mitigation Measure 4.4-7: Preconstruction nesting bird survey.	G	CD (P)(E) CDFW	
56-24	3A.3-2d (as amended by W/E SPA): Conduct preconstruction bat roosting survey.		CD	
56-25	3A.3-2e: Obtain an Incidental Take Permit under Section 10(a) of ESA; Develop and Implement a Habitat Conservation Plan to Compensate for the Loss of Vernal Pool Habitat.	G, I, OG	USFWS CD	
56-26	3A.3-2f: Obtain an Incidental Take Permit under Section 10(a) of ESA; Develop and Implement a Habitat Conservation Plan to Compensate for the Loss of VELB Habitat.	G, I, OG	USFWS USACE CD	
56-27	3A.3-2g: Secure Take Authorization for Federally Listed Vernal Pool Invertebrates and Implement All Permit Conditions.	G, I, C	USACE CD	
56-28	3A.3-2h (as amended by W/E SPA): Valley Elderberry Longhorn Beetle avoidance and minimization measures.	G, I, OG	USACE USFWS CD	
56-29	3A.3-3: Conduct Special-Status Plant Surveys; Implement Avoidance and Mitigation Measures or Compensatory Mitigation.	G, I, B	USFWS CDFW CD	
56-30	3A.3-4a (as amended by W/E SPA): Implement Section 1602 Master Streambed Alteration Agreement.	G, I, C	CDFW CD	Comply with approved 1602 Master Streambed Alteration Agreement
56-31	3A.3-4b (as amended by W/E SPA): Valley Needlegrass Grassland Avoidance and Minimization Measures.	G, I, B	CDFW CD	Comply with the Needlegrass Plan, dated 10/6/16

56-32	3A.3-5 (as amended by W/E SPA): Oak woodlands mitigation.	G, I, C	CD, PW	Comply with the Oak Tree Mitigation and Monitoring Plan, dated 1/24/17, and the Off-Site Oak Woodland Operations and Management Plan, dated March 17, 2017
56-33	W/E SPA Mitigation Measure 4.4-1: Conduct environmental awareness training for construction employees.	G, C	CD	
56-34	W/E SPA Mitigation Measure 4.4-2: Conduct preconstruction western spade/foot survey	G	CD (E) (P) CDFW	
56-35	W/E SPA Mitigation Measure 4.4-3: Conduct preconstruction western pond turtle survey.	G	CD (E)(P) CDFW	
Climate Change				
56-36	3A.4-1: Implement Additional Measures to Control Construction-Generated GHG Emissions.	M, B, C	CD SMAQMD	
56-37	3A.4-2a: Implement Additional Measures to Reduce Operational GHG Emissions.	M, B	CD SMAQMD	
56-38	3A.4-2b: Participate in and Implement an Urban and Community Forestry Program and/or Off-Site Tree Program to Off-Set Loss of On-Site Trees.	M, B	CD	
Cultural Resources				
56-39	3A.5-1a (as amended by W/E SPA): Comply with the Programmatic Agreement.	G	CD USACE	
56-40	3A.5-1b (as amended by W/E SPA): Perform an Inventory and Evaluation of Cultural Resources for the California Register of Historic Places, Minimize or Avoid Damage or Destruction, and Perform Treatment Where Damage or Destruction Cannot be Avoided.	G	CD USACE	
56-41	3A.5-2 (as amended by W/E SPA): Conduct Construction Personnel Education, Conduct On-Site Monitoring if Required, Stop Work if Cultural Resources are Discovered, Assess the Significance of the Find, and Perform Treatment or Avoidance as Required.	G, C	CD USACE	

56-42	3A.5-3 (as amended by W/E SPA): Suspend Ground-Disturbing Activities if Human Remains are Encountered and Comply with California Health and Safety Code Procedures.	OG	CD (P)(E) Sacramento County Coroner Native American Heritage Commission
Geology, Soils, Minerals, And Paleontological Resources			
56-43	3A.7-1a: Prepare Site-Specific Geotechnical Report per CBC Requirements and Implement Appropriate Recommendations.	B	CD (E)
56-44	3A.7-1b: Monitor Earthwork during Earthmoving Activities	B	CD (P)(E)(B)
56-45	3A.7-3: Prepare and Implement the Appropriate Grading and Erosion Control Plan.	G	CD (E)
56-46	3A.7-5: Divert Seasonal Water Flows Away from Building Foundations.	B	CD (B)(P)
56-47	3A.7-10: Conduct Construction Personnel Education, Stop Work if Paleontological Resources are Discovered, Assess the Significance of the Find, and Prepare and Implement a Recovery Plan as Required.	C	CD
Hazards and Hazardous Materials			
56-48	3A.8-2: Complete Investigations Related to the Extent to Which Soil and/or Groundwater May Have Been Contaminated in Areas Not Covered by the Phase I and II Environmental Site Assessments and Implement Required Measures	G, C	CD
56-49	3A.8-5: Prepare and Implement a Blasting Safety Plan in Consultation with a Qualified Blaster.	M	CD
56-50	Mitigation Measure 3A.8-6: Prudent Avoidance and Notification of EMF Exposure.	M	CD Folsom Cordova Unified School District
56-51	3A.8-7: Prepare and Implement a Vector Control Plan in Consultation with the Sacramento-Yolo Mosquito and Vector Control District.	G	CD Sacramento- Yolo Mosquito and Vector Control District
Hydrology and Water Quality			
56-52	3A.9-1: Acquire Appropriate Regulatory Permits and Prepare and Implement SWPPP and BMPs.	G, C	CD (E) CVRWQB

56-53	3A.9-2: Prepare and Submit Final Drainage Plans and Implement Requirements Contained in Those Plans.	G, B	CD (E)	
56-54	3A.9-3: Develop and Implement a BMP and Water Quality Maintenance Plan.	G, C	CD (E), PW	
56-55	3A.9-4: Inspect and Evaluate Existing Dams Within and Upstream of the Project Site and Make Improvements if Necessary.	M, I	PW	
Noise				
56-56	3A.11-1: Implement Noise-Reducing Construction Practices, Prepare and Implement a Noise Control Plan, and Monitor and Record Construction Noise near Sensitive Receptors.	G, C	CD	
56-57	3A.11-3: Implement Measures to Prevent Exposure of Sensitive Receptors to Groundborne Noise or Vibration from Project Generated Construction Activities.	C	CD (E)(P)	See the Folsom Ranch Commercial Development Environmental Noise Assessment dated May 22, 2018
56-58	3A.11-4: Implement Measures to Prevent Exposure of Sensitive Receptors to Increases in Noise from Project-Generated Operational Traffic on Off-site and On-Site Roadways.	C	CD (E)(P)	See the Folsom Ranch Commercial Development Environmental Noise Assessment dated May 22, 2018
56-59	3A.11-5: Implement Measures to Reduce Noise from Project-Generated Stationary Sources.	I, OG	CD (E)	See the Folsom Ranch Commercial Development Environmental Noise Assessment dated May 22, 2018
56-60	W/E SPA Mitigation Measure 4.12-1: Implement measures to prevent exposure of sensitive receptors to increases in noise from project-generated operational traffic on offsite and onsite roadways.	M	CD	
Public Services				
56-61	3A.14-1 (as amended by W/E SPA): Prepare and Implement a Construction Traffic Control Plan.	G, I, B, C	PW	

56-62	3A.14-2: Incorporate California Fire Code Requirements; and EDHFD Requirements, if Necessary, into Project Design and Submit Project Design to the City of Folsom Fire Department for Review and Approval.	B, O	CD, FD
56-63	3A.14-3: Incorporate Fire Flow Requirements into Project Designs.	B, O	CD, FD
Traffic and Transportation			
<p>It should be noted that many of the Transportation, Traffic, and Circulation mitigation measures identified below will be satisfied through the payment of fees. Below is a brief summary of the fee types and their purpose. The acronyms for each fee type noted below are further noted in the Implementation Schedule column of each applicable mitigation measure to clarify how each mitigation measure is anticipated to be satisfied.</p>			
<p>Public Facilities Financing Plan (PFFP): In January of 2014, the City of Folsom adopted the PFFP for the Folsom Plan Area which detailed all the infrastructure components to address full build out of the Plan Area. The PFFP includes various techniques including development fees to fund the necessary infrastructure. The City is currently in the process of preparing and adopting implementing ordinances and a nexus study required by State law to impose the associated development fees.</p>			
<p>Included in the PFFP are a number roadway projects including the Highway Interchanges that the Shops at Folsom Ranch project will have cumulative impacts on within the Folsom Plan Area. The PFFP was designed to satisfy the "fair share" financing of all the Plan Area's backbone roadway system. Participating in this fee program will satisfy numerous roadway mitigation measures as shown in the MMRP table.</p>			
<p>Sacramento County Transportation Development Fee (SCTDF) contribution: The City is establishing a "fair share" fee to mitigate roadway impacts outside the project boundaries and within unincorporated Sacramento County. This fee will be included in the City Facilities portion of the Public Facilities Financing Plan program and will be collected at the time of building permit issuance. The basis for the calculation of the fee is a report entitled, "Fair Share Cost Allocation Sacramento County & City of Folsom" dated January 2, 2014.</p>			
<p>Cal Trans/City Memorandum of Understanding (Cal Trans MOU): The City of Folsom and Cal Trans entered into an MOU on December 17, 2014 to establish a fee mechanism to address the "fair share" impacts to Highway 50. The MOU identifies all the highway improvements for which there are mitigation measures and potential construction projects to address them. The City will establish a fee in the City Facilities portion of the Public Facilities Financing Plan and it will be collected at the time of building permit issuance</p>			
56-64	3A.15-1: Project Participation in Funding Transportation Improvements	B	CD (E), PW
56-65	3A.15-1a: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Folsom Boulevard/Blue Ravine Road Intersection (Intersection 1).	B (pay PFFP fee)	CD (E), PW
56-66	3A.15-1b: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements at the Sibley Street/ Blue Ravine Road Intersection (Intersection 2).	B (pay PFFP fee)	CD (E), PW

56-67	3A.15-1c: The Applicant Shall Fund and Construct Improvements to the Scott Road (West)/White Rock Road Intersection (Intersection 28).	B (pay SCTDF)	CD (E), PW
56-68	3A.15-1e: Fund and Construct Improvements to the Hillside Drive/Easton Valley Parkway Intersection (Intersection 41).	B (pay PFFP fee)	CD (E), PW
56-69	3A.15-1f: Fund and Construct Improvements to the Oak Avenue Parkway/Middle Road Intersection (Intersection 44).	B (pay PFFP fee)	CD (E), PW
56-70	3A.15-1h: Participate in Fair Share Funding of Improvements to Reduce Impacts to the Hazel Avenue/Folsom Boulevard Intersection (Sacramento County Intersection 2).	B (pay SCTDF)	CD (E), PW
56-71	3A.15-1j: Participate in Fair Share Funding of Improvements to Reduce Impacts on Hazel Avenue between Madison Avenue and Curragh Downs Drive (Roadway Segment 10).	B (pay SCTDF)	CD (E), PW
56-72	3A.15-1o: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 as an alternative to improvements at the Folsom Boulevard/U.S. 50 Eastbound Ramps Intersection (Caltrans Intersection 4).	B (Caltrans MOU)	CD (E), PW
56-73	3A.15-1p: Participate in Fair Share Funding of Improvements to Reduce Impacts on the Grant Line Road/ State Route 16 Intersection (Caltrans Intersection 12).	B (Caltrans MOU/pay SCTDF)	CD (E), PW
56-74	3A.15-1q: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1).	B (Caltrans MOU)	CD (E), PW
56-75	3A.15-1r: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Hazel Avenue and Folsom Boulevard (Freeway Segment 3).	B (Caltrans MOU)	CD (E), PW
56-76	3A.15-1s: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Folsom Boulevard and Prairie City Road (Freeway Segment 4).	B (Caltrans MOU)	CD (E), PW
56-77	3A.15-1u: Participate in Fair Share Funding of Improvements to Reduce Impacts on Westbound U.S. 50 between Prairie City Road and Folsom Boulevard (Freeway Segment 16).	B (Caltrans MOU)	CD (E), PW
56-78	3A.15-1v: Participate in Fair Share Funding of Improvements to Reduce Impacts on Westbound U.S. 50 between Hazel Avenue and Sunrise Boulevard (Freeway Segment 18).	B (Caltrans MOU)	CD (E), PW
56-79	3A.15-1w: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Folsom Boulevard Ramp Merge (Freeway Merge 4).	B (Caltrans MOU)	CD (E), PW

56-80	3A.15-1x: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Diverge (Freeway Diverge 5).	B (Caltrans MOU)	CD (E), PW
56-81	3A.15-1y: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Direct Merge (Freeway Merge 6).	B (Caltrans MOU)	CD (E), PW
56-82	3A.15-1z: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Flyover On-Ramp to Oak Avenue Parkway Off-Ramp Weave (Freeway Weave 8).	B (pay PFFP fee)	CD (E), PW
56-83	3A.15-1aa: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Oak Avenue Parkway Loop Merge (Freeway Merge 9)	B (Caltrans MOU)	CD (E), PW
56-84	3A.15-1dd: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Empire Ranch Road Loop Ramp Merge (Freeway Merge 23).	B (Caltrans MOU)	CD (E), PW
56-85	3A.15-1ee: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Oak Avenue Parkway Loop Ramp Merge (Freeway Merge 29).	B (Caltrans MOU)	CD (E), PW
56-86	3A.15-1ff: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Prairie City Road Loop Ramp Merge (Freeway Merge 32).	B (Caltrans MOU)	CD (E), PW
56-87	3A.15-1gg: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Prairie City Road Direct Ramp Merge (Freeway Merge 33).	B (Caltrans MOU)	CD (E), PW
56-88	3A.15-1hh: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Folsom Boulevard Diverge (Freeway Diverge 34).	B (Caltrans MOU)	CD (E), PW
56-89	3A.15-1ii: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Hazel Avenue Direct Ramp Merge (Freeway Merge 38).	B (Caltrans MOU)	CD (E), PW
56-90	3A.15-2a: Develop Commercial Support Services and Mixed-use Development Concurrent with Housing Development, and Develop and Provide Options for Alternative Transportation Modes.	I (pay PFFP fee and Transit fee)	PW
56-91	3A.15-2b: Participate in the City's Transportation System Management Fee Program.	B (pay City fee)	CD (E), PW
56-92	3A.15-2c: Participate with the 50 Corridor Transportation Management Association	B (pay City fee)	PW

56-93	3A.15-3: Pay Full Cost of Identified Improvements that Are Not Funded by the City's Fee Program.	B (Caltrans MOU, PFFP fee, SCTDF)	CD (E), PW	
56-94	3A.15-4a: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Sibley Street/Blue Ravine Road Intersection (Folsom Intersection 2).	B (pay PFFP fee)	CD (E), PW	
56-95	3A.15-4b: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Oak Avenue Parkway/East Bidwell Street Intersection (Folsom Intersection 6).	B (pay PFFP fee)	CD (E), PW	
56-96	3A.15-4c: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the East Bidwell Street/College Street Intersection (Folsom Intersection 7).	B (pay PFFP fee)	CD (E), PW	
56-97	3A.15-4d: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the East Bidwell Street/Iron Point Road Intersection (Folsom Intersection 21).	B (pay PFFP fee)	CD (E), PW	
56-98	3A.15-4e: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Serpa Way/ Iron Point Road Intersection (Folsom Intersection 23)	B (pay PFFP fee)	CD (E), PW	
56-99	3A.15-4f: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Empire Ranch Road/Iron Point Road Intersection (Folsom Intersection 24).	B (pay PFFP fee)	CD (E), PW	
56-100	3A.15-4g: The Applicant Shall Fund and Construct Improvements to the Oak Avenue Parkway/Easton Valley Parkway Intersection (Folsom Intersection 33).	B (pay SCTDF)	CD (E), PW	
56-101	3A.15-4i: Participate in Fair Share Funding of Improvements to Reduce Impacts on the Grant Line Road/White Rock Road Intersection (Sacramento County Intersection 3).	B (pay SCTDF)	CD (E), PW	
56-102	3A.15-4j: Participate in Fair Share Funding of Improvements to Reduce Impacts on Grant Line Road between White Rock Road and Kiefer Boulevard (Sacramento County Roadway Segments 5-7).	B (pay SCTDF)	CD (E), PW	
56-103	3A.15-4k: Participate in Fair Share Funding of Improvements to Reduce Impacts on Grant Line Road between Kiefer Boulevard and Jackson Highway (Sacramento County Roadway Segment 8).	B (pay SCTDF)	CD (E), PW	
56-104	3A.15-4l: Participate in Fair Share Funding of Improvements to Reduce Impacts on Hazel Avenue between Curragh Downs Drive and U.S. 50 Westbound Ramps (Sacramento County Roadway Segment s 12-13).	B (pay SCTDF)	CD (E), PW	

56-105	3A.15-4m: Participate in Fair Share Funding of Improvements to Reduce Impacts on White Rock Road between Grant Line Road and Prairie City Road (Sacramento County Roadway Segment 22).	B (pay SCTDF)	CD (E), PW
56-106	3A.15-4n: Participate in Fair Share Funding of Improvements to Reduce Impacts on White Rock Road between Empire Ranch Road and Carson Crossing Road (Sacramento County Roadway Segment 28).	B (pay SCTDF)	CD (E), PW
56-107	3A.15-4o: Participate in Fair Share Funding of Improvements to Reduce Impacts on the White Rock Road/Carson Crossing Road Intersection (El Dorado County 1).	B (pay SCTDF)	CD (E), PW
56-108	3A.15-4p: Participate in Fair Share Funding of Improvements to Reduce Impacts on the Hazel Avenue/U.S. 50 Westbound Ramps Intersection (Caltrans Intersection 1).	B (pay SCTDF)	CD (E), PW
56-109	3A.15-4q: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1).	B (pay SCTDF)	Capitol Southeast Connector JPA
56-110	3A.15-4r: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Rancho Cordova Parkway and Hazel Avenue (Freeway Segment 3).	B (pay SCTDF)	Capitol Southeast Connector JPA
56-111	3A.15-4s: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Folsom Boulevard and Prairie City Road (Freeway Segment 5).	B (pay SCTDF)	Capitol Southeast Connector JPA
56-112	3A.15-4t: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Prairie City Road and Oak Avenue Parkway (Freeway Segment 6).	B (pay PFFP/Interchange fee)	CD (E), PW
56-113	3A.15-4u: Participate in Fair Share Funding of Improvements to Reduce Impacts on the U.S. 50 Eastbound / Prairie City Road Slip Ramp Merge (Freeway Merge 6).	B (pay PFFP fee)	CD (E), PW
56-114	3A.15-4v: Participate in Fair Share Funding of Improvements to Reduce Impacts on the U.S. 50 Eastbound / Prairie City Road Flyover On Ramp to Oak Avenue Parkway Off Ramp Weave (Freeway Weave 7).	B (pay PFFP fee)	CD (E), PW
56-115	3A.15-4w: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound / Oak Avenue Parkway Loop Ramp Merge (Freeway Merge 8).	B (pay PFFP fee)	CD (E), PW
56-116	3A.15-4x: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound / Empire Ranch Road Loop Ramp Merge (Freeway Merge 27).	B (pay PFFP fee)	CD (E), PW

56-117	3A.15-4y: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound / Prairie City Road Loop Ramp Merge (Freeway Merge 35).	B (pay PFFP fee)	CD (E), PW
56-118	W/E SPA Mitigation Measure 4.16-1: Participate in Fair Share Funding of modification of the Iron Point Road/East Bidwell Street Intersection.	B (pay PFFP fee)	CD (E), PW
56-119	W/E SPA Mitigation Measure 4.16-2: Participate in Fair Share Funding of improvements to the Scott Road/Easton Valley Parkway Intersection.	B (pay PFFP fee)	CD (E), PW
Utilities and Service Systems			
56-120	3A.16-1: Submit Proof of Adequate On- and Off-Site Wastewater Conveyance Facilities and Implement On- and Off-Site Infrastructure Service Systems or Ensure That Adequate Financing Is Secured	M, B	CD, PW
56-121	3A.16-3: Demonstrate Adequate SRWTP Wastewater Treatment Capacity	M, B	CD, PW
Water Supply			
56-122	3A.18-1: Submit Proof of Surface Water Supply Availability	M, B	CD, PW
56-123	3A.18-2a: Submit Proof of Adequate Off-Site Water Conveyance Facilities and Implement Off-Site Infrastructure Service System or Ensure That Adequate Financing Is Secured.	M, B	CD, PW
Cumulative			
56-124	AIR-1-Land: Implement East Sacramento Regional Aggregate Mining Truck Management Plan or Other Measures to Reduce Exposure of Sensitive Receptors to Operational Emissions of Toxic Air Contaminants from Quarry Truck Traffic.	M	CD
56-125	NOISE-1-Land: Implement East Sacramento Regional Aggregate Mining Truck Management Plan or Other Measures to Reduce Exposure of Sensitive Receptors to Operational Noise from Quarry Truck Traffic.	M	CD
57.	Folsom South of U.S. Highway 50 Backbone Infrastructure Mitigated Negative Declaration (Backbone MND) Mitigation Monitoring Reporting Program (MMRP) for the Folsom Plan Area Specific Plan (FPASP). Table 2 Below describes the mitigation measures from the Backbone MND (December 2014) MMRP		

Table 2.
Backbone MND Mitigation Measures Applicable to the Shops at Folsom Ranch Project.*

*The mitigation measures specific to the 2014 Backbone Infrastructure MND (designated by roman numerals, e.g., IV-1) apply only to the portions of the Shops at Folsom Ranch Project that are included as part of the South of Highway 50 Backbone Infrastructure Project. The MMRP for the Backbone Infrastructure Project is included as Attachment 20.

Condition	Mitigation Measures Applicable to the Project	Timing	Responsible Agency	Notes/Compliance
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Aesthetics				
57-1	Backbone MND Mitigation Measure I-1: Design above ground pump station and storage tank facilities to reduce visual impacts.	I	CD, EWR	
57-2	Backbone MND Mitigation Measure I-2: Develop and implement a landscaping plan for pump station and storage tank facilities to reduce visual impacts.	I	CD, EWR	
Air Quality				
57-3	Backbone MND Mitigation Measure III-1: Prepare and Implement NOX Reduction Plan	G, C	CD SMAQMD	
57-4	Backbone MND Mitigation Measure III-2: Pay Off-site Mitigation Fee to SMAQMD to off-set NOX Emissions Generated by Construction.	G, C	CD SMAQMD	
57-5	Backbone MND Mitigation Measure III-4: Implement A Site Investigation to Determine the Presence of NOA and, if necessary, Prepare and Implement an Asbestos Dust Control Plan.	G, C	CD SMAQMD	
Biological Resources				
57-6	Backbone MND Mitigation Measure IV-1: Conduct Special-Status Plant Surveys; Implement Avoidance and Mitigation Measures or Compensatory Mitigation	G, C	CD	
57-7	Backbone MND Mitigation Measure IV-2: Implement Conditions of the Biological Opinion (BO) for Federally Listed Vernal Pool Invertebrates.	G, C	CD USFWS	
57-8	Backbone MND Mitigation Measure IV-3: Implement Conditions of the Biological Opinion for Impacts on Valley Elderberry Longhorn Beetle.	G, C	CD USFWS	
57-9	Backbone MND Mitigation Measure IV-4: Western Spadefoot Toad	G, C	CD CDFW	
57-10	Backbone MND Mitigation Measure IV-5: Western Pond Turtle	G, C	CD CDFW	
57-11	Backbone MND Mitigation Measure IV-6(a): Swainson's Hawk Nesting Habitat	G, I, C	CD	See Swainson's Hawk and Tricolored Blackbird Mitigation Plans, dated 5/2/17 and 2/21/17
57-12	Backbone MND Mitigation Measure IV-6(b): Swainson's Hawk Foraging Habitat	G, I, C	CD	See Swainson's Hawk and Tricolored Blackbird Mitigation Plans, dated 5/2/17 and 2/21/17
57-13	Backbone MND Mitigation Measure IV-7: Tricolored Blackbird	G, C	CD CDFW	See Swainson's Hawk and Tricolored Blackbird Mitigation Plans, dated 5/2/17 and 2/21/17
57-14	Backbone MND Mitigation Measure IV-8: Nesting Raptors	G, I	CD CDFW	
57-15	Backbone MND Mitigation Measure IV-9: Nesting Special Status Birds and	G, C	CD	

	Migratory Birds		CDFW	
57-16	Backbone MND Mitigation Measure IV-10: Special-Status Bats		CD CDFW	
57-17	Backbone MND Mitigation Measure IV-12: Implement Section 1602 Master Streambed Alteration Agreement	G, C	CD CDFW	Comply with the approved Master Streambed Alteration Agreement
57-18	Backbone MND Mitigation Measure IV-13: Conduct Surveys to Identify and Map Valley Needlegrass Grassland; Implement Avoidance and Minimization Measures or Compensatory Mitigation, if necessary	G, C	CD CDFW	Comply with the Needlegrass Plan, dated 10/6/16
57-19	Backbone MND Mitigation Measure IV-14: Secure Amended Clean Water Act Section 404 Permit and Section 401 Permit and Implement All Permit Conditions; Ensure No Net Loss of Functions of Wetlands, Other Waters of the U.S., and Waters of the State	G, I, C	CD USACE	Comply with Carpenter Ranch and Backbone 401 and 404 permits and any amendments thereto.
57-20	Backbone MND Mitigation Measure IV-15: Conduct Tree Survey, Prepare and Implement an Oak Woodland Mitigation Plan, Replace Native Oak Trees Removed, and Implement Measures to Avoid and Minimize Indirect Impacts on Oak Trees and Oak Woodland Habitat Retained On-Site.	G, I, C	CD, PW	Comply the Oak Tree Mitigation and Monitoring Plan, dated 1/24/17, and the Off-Site Oak Woodland Operations and Management Plan, dated March 17, 2017
57-21	Backbone MND Mitigation Measure IV-11: American Badger	G, C	CD CDFW	
Cultural Resources				
57-22	Backbone MND Mitigation Measure V-1: Comply with the applicable procedures in the FAPA and implementation of applicable historic property treatment plans	G, C	CD USACE	
57-23	Backbone MND Mitigation Measure V-2: Conduct Construction Personnel Education, Conduct On-Site Monitoring if Required, Stop Work if Cultural Resources are Discovered, Assess the Significance of the Find, and Perform Treatment or Avoidance as Required.	G, C	CD USACE	
57-24	Backbone MND Mitigation Measure V-3: Suspend Ground-Disturbing Activities if Human Remains are Encountered and Comply with California Health and Safety Code Procedures.	OG	CD Sacramento County Coroner Native American Heritage Commission	

Geology, Soils, and Paleontological Resources				
57-25	Backbone MND Mitigation Measure VI-1: Prepare Site-Specific Geotechnical Report per CBC Requirements and Implement Appropriate Recommendations.	G, C	CD EWR	
57-26	Backbone MND Mitigation Measure VI-3: Monitor Earthwork during Earthmoving Activities.	G, C	CD	
57-27	Backbone MND Mitigation Measure VI-5(a): Prepare and Implement the Appropriate Grading and Erosion Control Plan.	G	PW	
Hydrology and Water Quality				
57-28	Backbone MND Mitigation Measure VI-5(b): Prepare and Implement the appropriate Grading and Erosion Control Plan for the detention basin West of Prairie City Road.	G	PW	
57-29	Backbone MND Mitigation Measure IX-1: Acquire Appropriate Regulatory Permits and Prepare and Implement SWPPP and BMPs.	G	CD (E) Central Valley Regional Water Quality Control Board.	
Climate Change				
57-30	Backbone MND Mitigation Measure VII-1: Greenhouse Gas Emissions	Prior to releasing RFB to contractors	SMAQMD	
Public Services				
57-31	Backbone MND Mitigation Measure XVI-1: Prepare and Implement a Construction Traffic Control Plan.	G, I, B, C	PW	
Water Improvements				
57-32	Backbone MND Mitigation Measure III-3: North of U.S. Highway 50 Water Improvements	G, I	SMAQMD CD (E)	
57-33	Backbone MND Mitigation Measure V-4 North of U.S. Highway 50 Water Improvements	G, I	CD (E)	
57-34	Backbone MND Mitigation Measure VI-2 North of U.S. Highway 50 Water Improvements	G, I	CD (E)	
57-35	Backbone MND Mitigation Measure VI-4 North of U.S. Highway 50 Water Improvements	G, I	CD (E)	
57-36	Backbone MND Mitigation Measure XII-1 North of U.S. Highway 50 Water Improvements	G, I, B	PW	

58. Revised Proposed Off-Site Water Facility Alternative Mitigation Monitoring Reporting Program (MMRP) for the Folsom Plan Area Specific Plan (FPASP). Table 3 below describes the mitigation measures from the Revised Water Alternative (November 2012) MMRP.

Table 3.
Revised Proposed Off-Site Water Facility Alternative Addendum to the FPASP EIR/EIS Mitigation Measures Applicable to the Shops at Folsom Ranch Project.*

*The Folsom South of U.S. Highway 50 Specific Plan Project: Revised Proposed Off-Site Water Facility Alternative was approved December 11, 2012. The November 2012 MMRP for the Revised Proposed Off-Site Water Facility Alternative is included as Attachment 21.

Condition	Mitigation Measures Applicable to the Project	Timing	Responsible Agency	Notes/Compliance
Aesthetics				
58-1	3B.1-2a: Enhance Exterior Appearance of Structural Facilities.	G, B	CD	
58-2	3B.1-2b: Prepare Landscaping Plan.	G, B	CD	
58-3	3B.1-3a: Conformance to Construction Lighting Standards.	G, B	CD	
58-4	3B.1-3b: Prepare and Submit a Lighting Master Plan.	G, B	CD	
Air Quality				
58-5	3B.2-1a: Develop and Implement a Construction NOX Reduction Plan.	G, C	CD SMAQMD	
58-6	3B.2-1c: Implement Fugitive Dust Control Measures and a Particulate Matter Monitoring Program during Construction.	G, C	CD SMAQMD	
58-7	3B.2-3a: Cite Pump Siting Buffers Away from Sensitive Receptors.	G, B	CD SMAQMD	
58-8	3B.2-3b: Conduct Project-Level DPM Screening and Implement Measures to Reduce Annual DPM to Acceptable Concentrations.	G, B	CD SMAQMD	
Climate Change				
58-9	3B.4-1a: Implement GHG Reduction Measures during Construction.	G, B	CD SMAQMD	
58-10	3B.4-1b Prepare and Implement an Off-site Water Facilities Climate Action Plan.	G, B	CD SMAQMD	
Cultural Resources				
58-11	3A.5-1a: Comply with the Programmatic Agreement.	G	CD (E) USACE	This Mitigation Measure was revised in the W/E SPA Addendum. See Condition No. 56-39
58-12	3A.5-1b: Perform an Inventory and Evaluation of Cultural Resources for the California Register of Historic Places, Minimize or Avoid Damage or Destruction, and Perform Treatment Where Damage or Destruction Cannot be Avoided.	B, C	CD	This Mitigation Measure was revised in the W/E SPA Addendum. See Condition No. 56-40

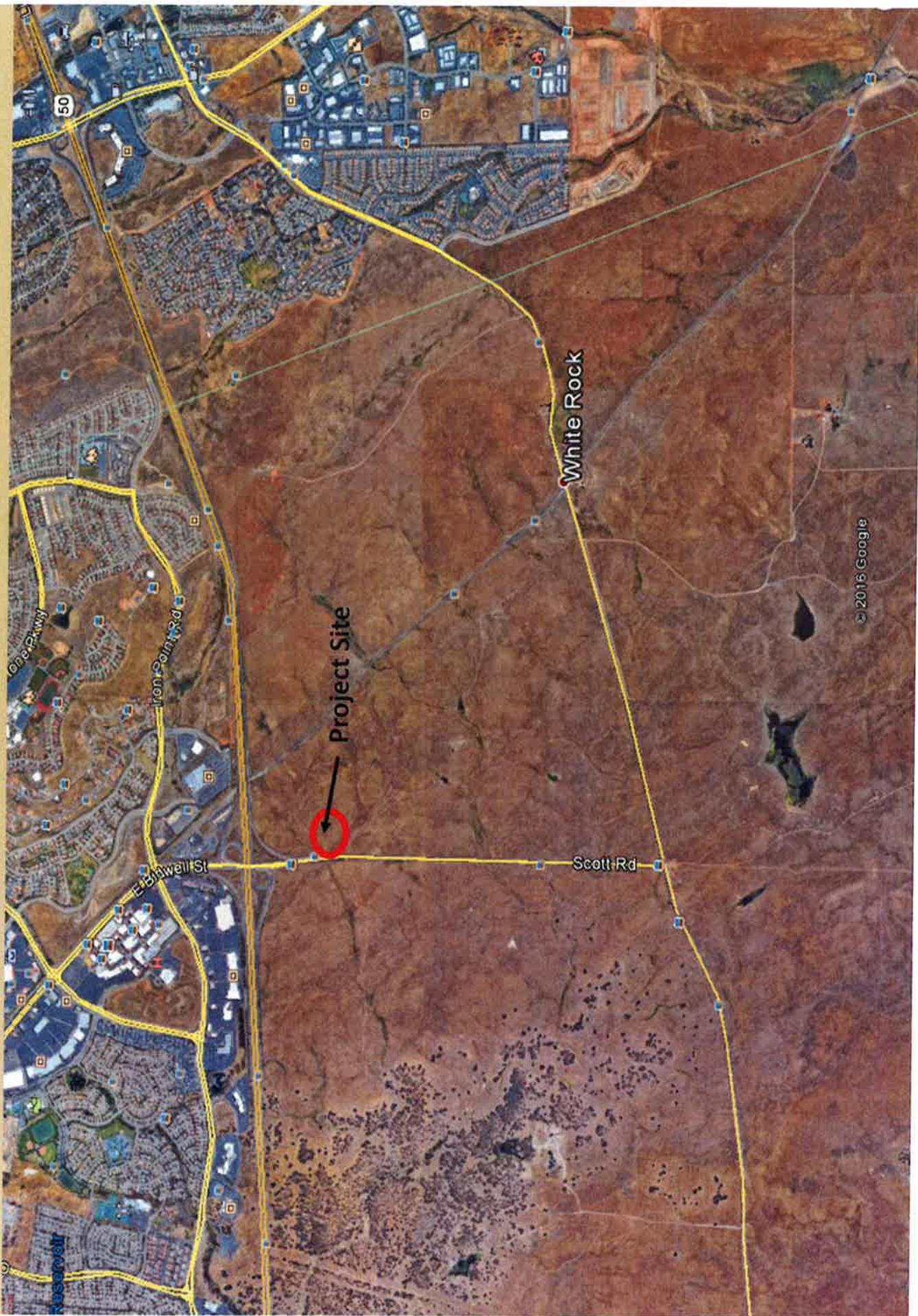
58-13	3A.5-2: Conduct Construction Personnel Education, Conduct On-Site Monitoring if Required, Stop Work if Cultural Resources are Discovered, Assess the Significance of the Find, and Perform Treatment or Avoidance as Required.	G, C	CD USACE	This Mitigation Measure was revised in the W/E SPA Addendum. See Condition No. 56-41
58-14	3A.5-3: Suspend Ground-Disturbing Activities if Human Remains are Encountered and Comply with California Health and Safety Code Procedures.	C	CD USACE	This Mitigation Measure was revised in the W/E SPA Addendum. See Condition No. 56-42
Geology, Soils, Minerals, and Paleontological Resources				
58-15	3B.7-1a: Prepare Geotechnical Report(s) for the Revised Proposed Off-site Water Facilities and Implement Required Measures.	engineering plans	CD	
58-16	3B.7-1b: Incorporate Pipeline Failure Contingency Measures Into Final Pipeline Design.	engineering plans	CD	
58-17	3B.7-4: Implement Corrosion Protection Measures.	engineering plans	CD	
58-18	3B.7-5: Conduct Construction Personnel Education, Stop Work if Paleontological Resources are Discovered, Assess the Significance of the Find, and Prepare and Implement a Recovery Plan as Required.	C	CD	
Hazards and Hazardous Materials				
58-19	3B.8-1a: Transport, Store, and Handle Construction-Related Hazardous Materials in Compliance with Relevant Regulations and Guidelines.	G, C	CD	
58-20	3B.8-1b: Prepare and Implement a Hazardous Materials Management Plan.	G, C	CD	
58-21	3B.8-5a: Conduct Phase 1 Environmental Site Assessment for Selected Alignment.	G, C	CD	
58-22	3B.8-5b: Develop and Implement a Remediation Plan.	G, C	CD	
58-23	3B.8-7a: Keep Construction Area Clear of Combustible Materials.	G, C	CD	
58-24	3B.8-7b: Provide Accessible Fire Suppression Equipment.	G, C	CD	
Hydrology and Water Quality				
58-25	3B.9-1a: Acquire Appropriate Regulatory Permits and Prepare and Implement SW/PPP and BMPs.	G, C	CD CVRWQB	
58-26	3B.9-1b: Properly Dispose of Hydrostatic Test Water and Construction Dewatering in Accordance with the Central Valley Regional Water Quality Control Board.	G, C	CD CVRWQB	
58-27	3B.9-3a: Prepare and Implement Drainage Plan(s) for Structural Facilities.	G, C	CD CVRWQB	
58-28	3B.9-3b: Ensure the Provision of Sufficient Outlet Protection and On-site Containment.	G, C	CD CVRWQB	
Noise				
58-29	3B.11-1a: Limit Construction Hours.	C	CD	

58-30	3B.11-1b: Minimize Noise from Construction Equipment and Staging.	C	CD
58-31	3B.11-1c: Maximize the Use of Noise Barriers.	G, C	CD
58-32	3B.11-1d: Prohibit Non-Essential Noise Sources During Construction.	G, C	CD
58-33	3B.11-1e: Monitor Construction Noise and Provide a Mechanism for Filing Noise Complaints.	G, C	CD
58-34	3B.11-3: Implement Operational Noise Minimization Measures.	engineering plans	CD
Parks and Recreation			
58-35	3B.12-1: Provide for Continued Recreational Access as Identified in Mitigation Measure 3.14-1a.	G, C	CD
Traffic and Transportation			
58-36	3B.15-1a: Prepare Traffic Control Plan.	G, C	CD
58-37	3B.15-1b: Assess Pre-Off-site Water Facilities Roadway Conditions.	G, C	CD
Utilities and Service Systems			
58-38	3B.16-3a: Minimize Utility Conflicts by Implementing an Underground Services Alert.	G, C	CD
58-39	3B.16-3b: Coordinate with Utility Providers and Implement Appropriate Installation Methods to Minimize Potential Utility Service Disruptions.		CD
Groundwater			
58-40	3B.17-1a: Implement Construction Dewatering Best Management Practices.	G, C	CD CDFW or CVRWQB
58-41	3B.17-1b: Implement a Dewatering Discharge Monitoring Program.	G, C	CD CDFW or CVRWQB
Water Supply			
58-42	3A.18-1: Submit Proof of Surface Water Supply Availability.	M, B	CD, PW
58-43	3A.18-2a: Submit Proof of Adequate Off-Site Water Conveyance Facilities and Implement Off-Site Infrastructure Service System or Ensure That Adequate Financing Is Secured.	M, B	CD, PW

Attachment 1

Vicinity Map

Vicinity Map



Attachment 2

Vesting Tentative Parcel Map, dated October 25, 2018

Attachment 3

Preliminary Site Plan, dated October 25, 2018

Attachment 4

**Preliminary Grading and Drainage Plan
Dated October 25, 2018**

Attachment 5

Preliminary Utility Plan, dated October 25, 2018

Attachment 6

**Preliminary Landscape Plan and Tree Shading Exhibit
Dated October 5, 2018**

ALDER CREEK PARKWAY

LEGEND

- A** Development Monument Markers
- B** Project Monument Sign
- C** Outdoor Seating
- D** Courtyard
- E** Trash Enclosure Wall with Vines
- F** Entry Sign
- G** Bike Racks
- H** Screen Hedge
- I** Landscape Wall



EAST BIDWELL STREET

ENCLAVE AT FOLSOM RANCH

OLD RANCH WAY

PLANT LEGEND

SPECIES	TYPE
<i>Acer macrophyllum</i> (Big-Leaf Maple)	Specimen Tree
<i>Olea europaea</i> 'Swan Hill' (Olive)	Monument Tree
<i>Platanus racemosa</i> (California Sycamore)	Parking Lot Tree
<i>Quercus lobata</i> (Valley Oak)	Street Tree
<i>Umbellularia californica</i> (California Bay)	Screening Tree

GROUNDCOVER & GRASSES

- Elymus glaucus* (Blue Wildrye)
- Eschscholzia californica* (California Poppy)
- Festuca californica* (California Fescue)
- Lupinus nanus* (Sky Lupine)
- Muhlenbergia rigens* (Deergrass)
- Nassella lepida* (Foothill Needlegrass)
- Sisyrinchium bellum* (Blue-Eyed Grass)

SHRUBS

- Arctostaphylos manzanita* (Common Manzanita)
- Arctostaphylos viscidula* (Whiteleaf Manzanita)
- Baccharis pilularis* (Coyote Brush)
- Ceanothus cuneatus* (Buckbrush)
- Cercis occidentalis* (Western Redbud)
- Ribes malvaceum* (Chaparral Currant)
- Rosa californica* (Wild Rose)

VINES

- Parthenocissus tricuspidata* (Boston Ivy)



Landscape Site Plan
Planned Development Permit

The Shops
at Folsom Ranch

Folsom, California October 3, 2018

ALDER CREEK PARKWAY



EAST BIDWELL STREET

ENCLAVE AT FOLSOM RANCH

OLD RANCH WAY

LEGEND

- Un-Shaded Parking Lot
- Shaded Parking Lot

	Total General Parking Number	Total Parking Stall and Drive Aisle Area (SF)	Minimum Area (SF) required for shade (40%)	Shade Area Coverage (SF)	Complies
TOTAL	215	58579	23431.6	27238	YES

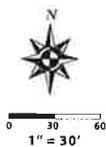
Note: Folsom Improvement Standards and FMC Title 17 Parking require 40% coverage at 15 years. PPASP Policy 10.56 also states this as "40% of the parking lot will be in shade at high noon".

F
FOLSOM RANCH

PCCP

Mackay & Somps
ENGINEERS PLANNERS SURVEYORS

ima **SGPA**
ARCHITECTURE AND PLANNING

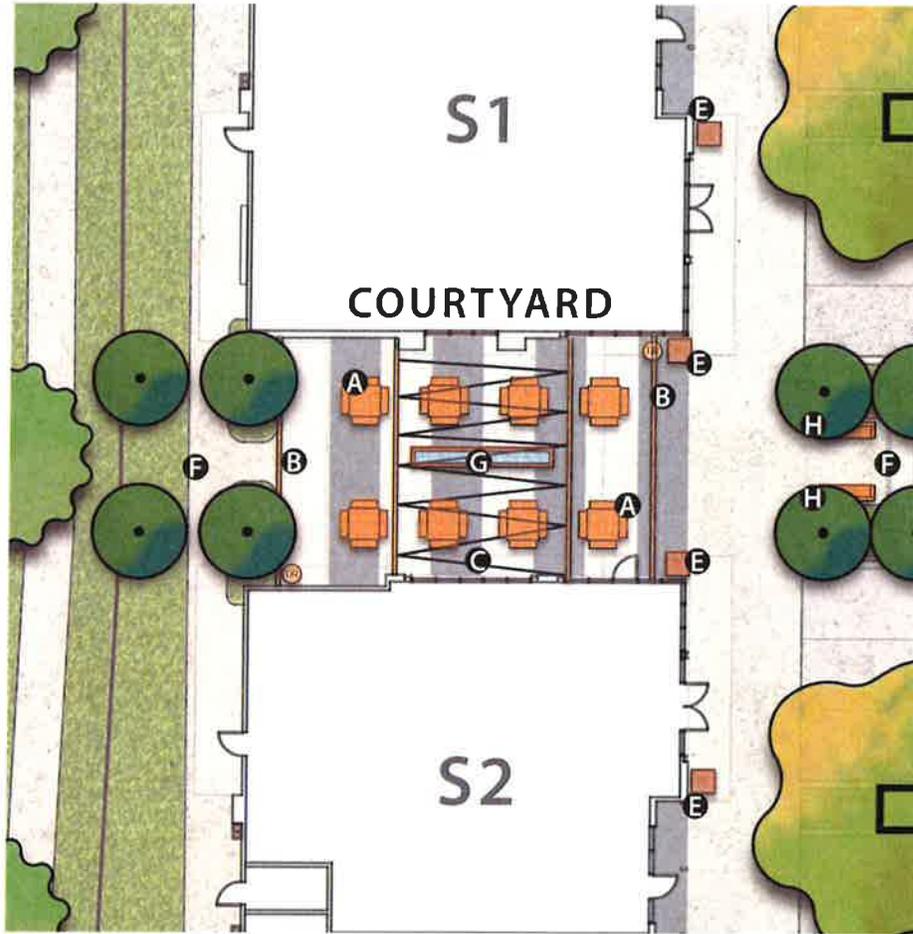


Tree Shading Exhibit
Planned Development Permit
The Shops at Folsom Ranch
Folsom, California October 5, 2018

Attachment 7

**Preliminary Site Furnishings and Courtyard Exhibit
Dated September 4, 2018**

COURTYARD ENLARGEMENT



LEGEND

- A** Seating Area
- B** Trellis
- C** Festoon Lights
- D** Landscape Buffer
- E** Decorative Pots
- F** Olive Allee
- G** Fire or Water Feature
- H** Bench

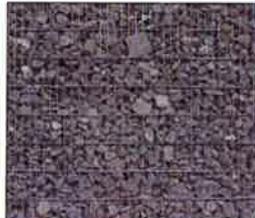
GENERAL MATERIALS AND FINISHES



LIMESTONE LEDGE



CORTEN STEEL



GABION WALL



RANCH WOOD



Courtyard
Enlargement & Materials
Planned Development Permit

**The Shops
at Folsom Ranch**

Folsom, California September 4, 2018

SITE FURNISHINGS

Bench



DESCRIPTION:
Neoromantico
MANUFACTURER: Landscape Forms
COLOR: TBD

Bike Rack



DESCRIPTION:
Bike Rack
MANUFACTURER: Maglin
COLOR: TBD

Trash / Waste Receptacle



DESCRIPTION:
Trash Receptacle
MANUFACTURER: Maglin
COLOR: TBD

Festoon Lights



DESCRIPTION:
Festoon Lighting
MANUFACTURER: Tivoli
COLOR: -

Bollards



DESCRIPTION:
Security / Pedestrian Lighted Bollards
MANUFACTURER: Creative Pipe
COLOR: TBD

Planters



DESCRIPTION:
Plaza Planter
MANUFACTURER: Landscape Forms
COLOR: TBD

Tree Grate



DESCRIPTION:
Tree Grate
MANUFACTURER: Urban Accessories
COLOR: Cast iron



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ima **SGPA**
ARCHITECTURE
AND PLANNING

Site Furnishings Exhibit
Planned Development Permit

The Shops
at Folsom Ranch

Folsom, California September 5, 2018

Attachment 8

**Onsite Bicycle and Pedestrian Circulation Exhibit
Dated October 5, 2018**

EAGLE COMMERCIAL PARTNERS LLC
 072-3130-046
 (GC)
 ALDER CREEK PARKWAY

EAGLE COMMERCIAL PARTNERS LLC
 072-3130-030
 (GC)

EAST BIDWELL STREET



ENCLAVE AT FOLSOM RANCH
 (APPROVED TENTATIVE MAP)
 (IMLD)

OLD RANCH WAY
 MANGINI RANCH PHASE 2
 (APPROVED TENTATIVE MAP)
 (IMLD)

-  PERIMETER SIDEWALK
-  INTERNAL PEDESTRIAN CIRCULATION
-  BICYCLE PARKING
-  ENHANCED PEDESTRIAN CROSSING



0 40 80 160
 SCALE: 1" = 80'

On-site Bike and Pedestrian Circulation Diagram

The Shops at Folsom Ranch

Folsom, California

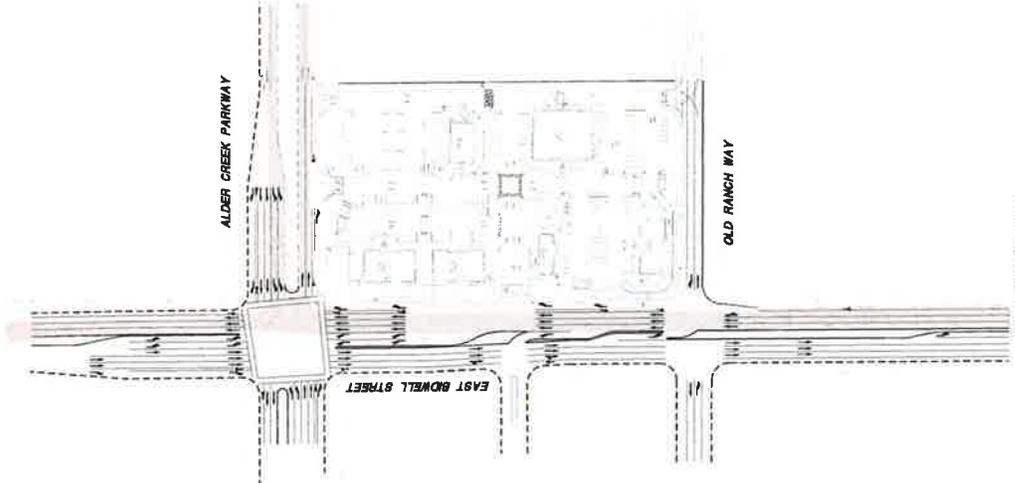
October 5, 2018

MACKAY & SOMPS
 ENGINEERS PLANNERS SURVEYORS

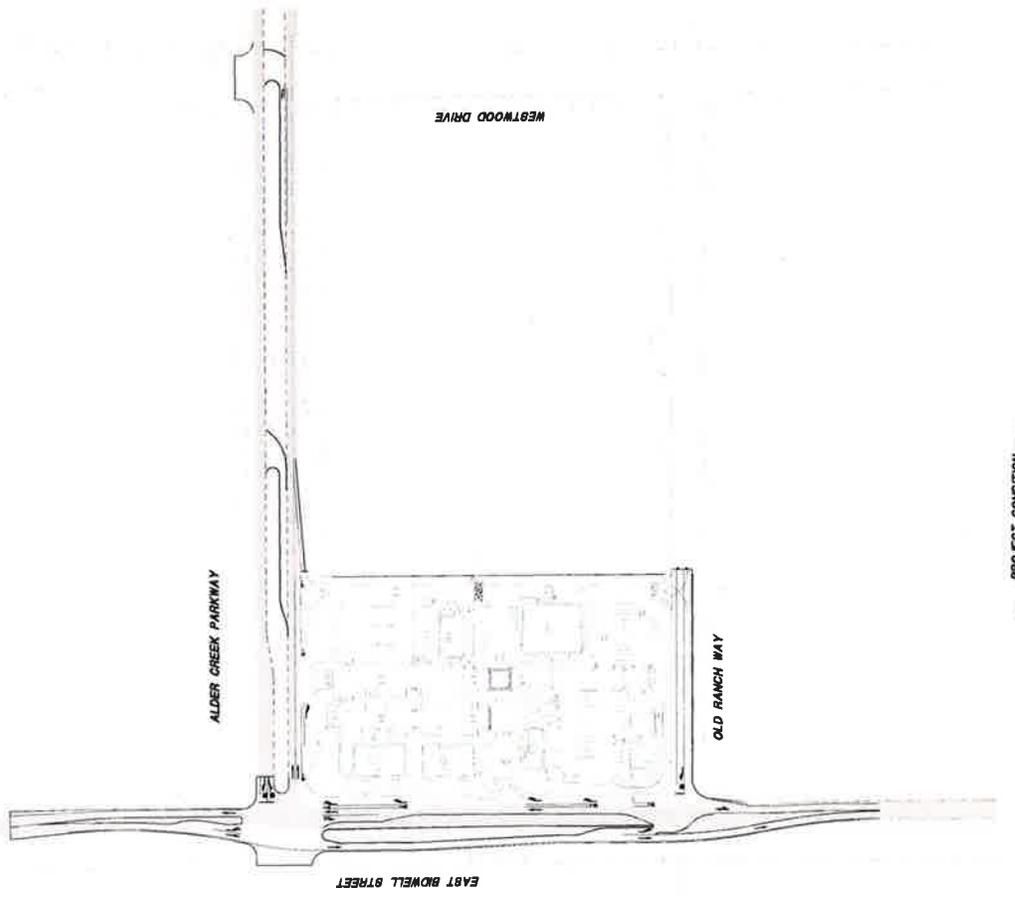
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Attachment 9

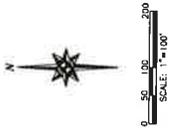
Preliminary Off-Site Roadway Configuration Exhibit
Dated October, 2018



CUMULATIVE CONDITION



PROJECT CONDITION



Roadway Configuration Exhibit

The Shops at Folsom Ranch

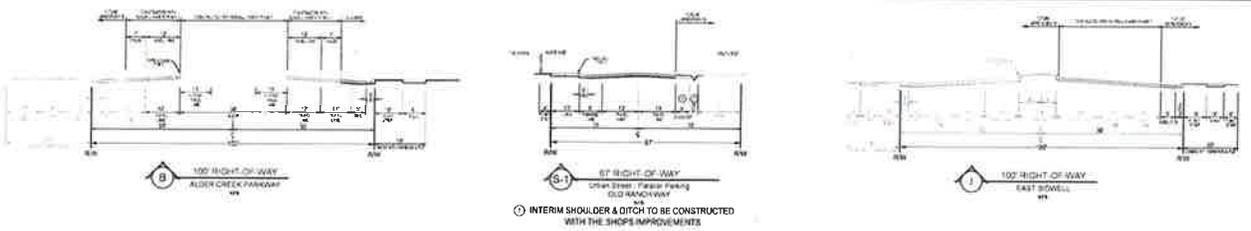
City of Folsom
 California
 October 2016
 2016.176

Mackay & Samps
 ENGINEERS ARCHITECTS PLANNERS
 1000 Folsom Blvd., Suite 100
 Folsom, CA 95630
 (916) 992-1174

PRELIMINARY - Subject to Revision

Attachment 10

**Preliminary Off-Site Utility Exhibit
Dated October, 2018**



LEGEND

	PROPOSED ZONE 4 NON-POTABLE WATER DISTRIBUTION PIPE		EXISTING ZONE 4 NON-POTABLE WATER DISTRIBUTION PIPE		EXISTING ROADWAY IMPROVEMENTS CONSTRUCTED WITH RUSSELL RANCH AND MANGINI RANCH
	EXISTING ZONE 4 NON-POTABLE WATER DISTRIBUTION PIPE		PROPOSED ZONE 3 POTABLE WATER DISTRIBUTION PIPE		PROPOSED ROADWAY IMPROVEMENTS
	EXISTING ZONE 3 POTABLE WATER DISTRIBUTION PIPE		EXISTING ZONE 3 POTABLE WATER DISTRIBUTION PIPE		
	EXISTING ZONE 3 POTABLE WATER TRANSMISSION MAIN				
	PROPOSED ZONE 4 POTABLE WATER DISTRIBUTION PIPE				
	EXISTING ZONE 4 POTABLE WATER DISTRIBUTION PIPE				
	EXISTING ZONE 5 POTABLE WATER DISTRIBUTION PIPE				
	EXISTING ZONE 3 POTABLE WATER TRANSMISSION PIPE				
	PROPOSED SANITARY SEWER PIPE				
	EXISTING SANITARY SEWER PIPE				
	PROPOSED STORM DRAIN PIPE				
	EXISTING STORM DRAIN PIPE				
	PROPOSED DRY UTILITY TRENCH				



SCALE: 1" = 200'

Off-Site Infrastructure
Utilities Exhibit

Folsom Plan Area

City of Folsom



California
October 2015
3022.P19

PRELIMINARY - Subject to Revision

Attachment 11

**Preliminary On-Site and Off-Site Grading Exhibit
Dated October, 2018**



ESTIMATED EARTHWORK QUANTITIES

CUT	FILL	EXPORT*
108,560	31,000	75,560

*EXPORT ASSUMED IN SWELL. EXCESS PLACED ON GRADE FOR 50% FINE STOCKPILE (2,000 CY)



Grading Exhibit

The Shops at Folsom Ranch

City of Folsom
Scale: 1"=100'

MACKAY & SODPS
California
October 2010
10/27/10

PRELIMINARY - Subject to Revision

Attachment 12

Building Elevations, dated August 31, 2018 and
October 5, 2018



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MACKAY & SOMPS
ENGINEERS
PLANNERS
SURVEYORS

SGPA
ARCHITECTURE
AND PLANNING
ima

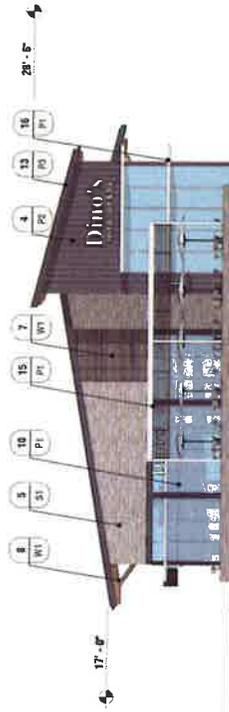
Materials & Colors

1. Cement plaster, paint finish
2. Fiber cement board siding, paint finish
3. Wood siding, stained finish
4. Board and Batten siding
5. Manufactured Stone Veneer
6. Metal Panel
7. Wood screen, stained finish
8. Wood structure, stained finish
9. Standing seam metal roof
10. Aluminum storefront
11. Steel structure, paint finish
12. Metal screen, paint finish
13. Metal fascia, paint finish
14. Precast coping
15. Metal / wood trellis, painted & stained
16. Metal / wood sunscreen, painted & stained
- P1. Benjamin Moore 2121-50 - Iced Cube Silver
- P2. Benjamin Moore 2127-40 - Wolf Gray
- P3. Benjamin Moore 2126-30 - Anchor Gray
- P4. Benjamin Moore 2108-50 - Silver Fox
- P5. Benjamin Moore 2111-40 - Taos Taupe
- P6. Benjamin Moore 2111-20 - Grizzly Bear Brown
- P7. Benjamin Moore HC91 - Danville Tan
- P8. Benjamin Moore HC151 - Buckland Blue
- W1. Olympic-Semi-transparent - Drift
- S1. Eldorado Stone - Stacked stone - Daybreak

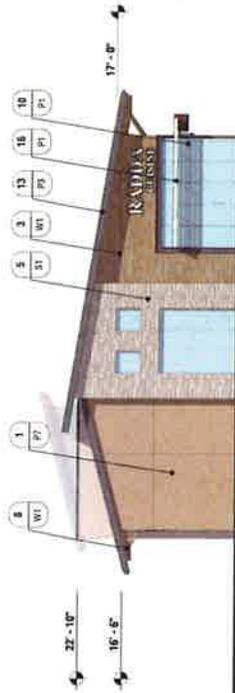
Elevations - BLDG S1 Planned Development Permit

The Shops at Folsom Ranch

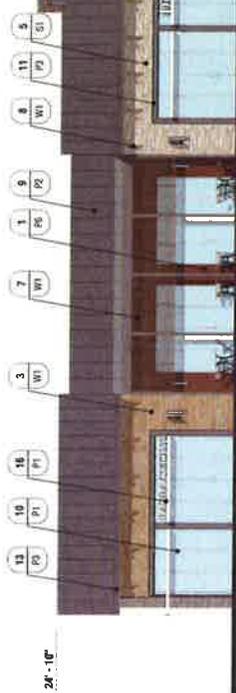
Folsom, California
November 2014
Sheet 1 of 14



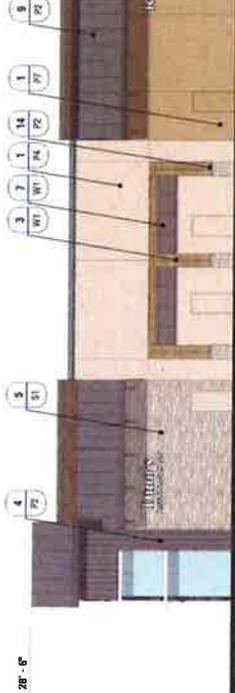
North Elevation



South Elevation



East Elevation



West Elevation



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MACKAY & SOMPS
ENGINEERS PLANNERS ARCHITECTS

SGPA ima
ARCHITECTURE AND PLANNING

Materials & Colors

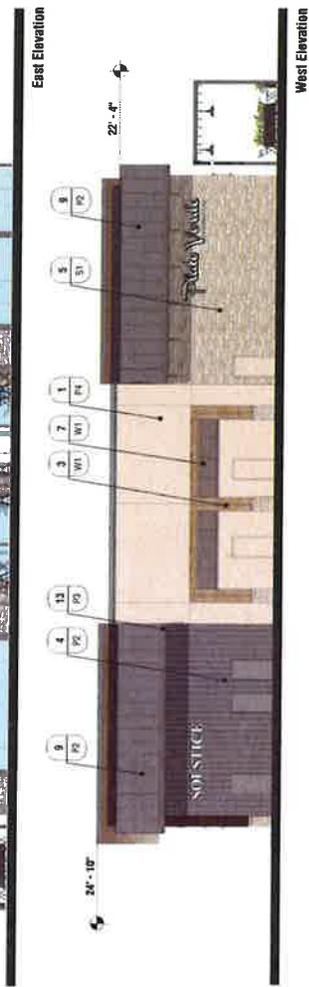
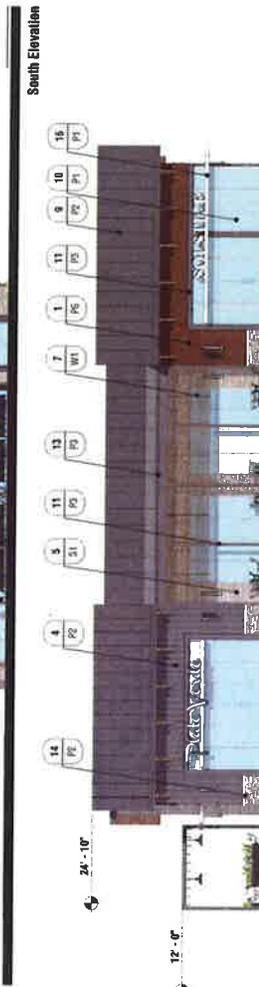
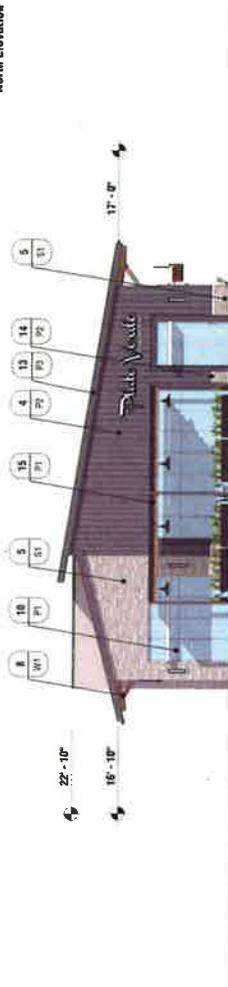
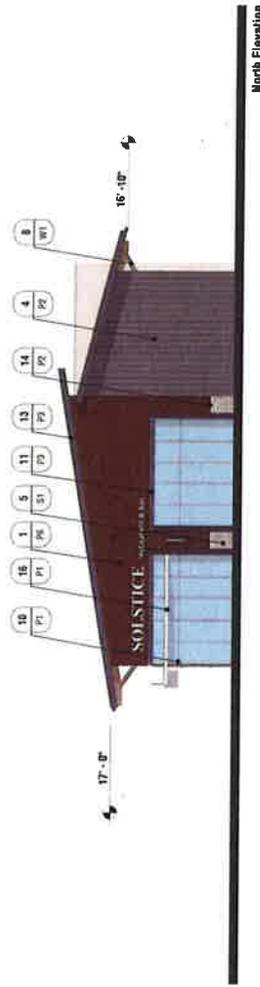
1. Cement plaster, paint finish
2. Fiber cement board siding, paint finish
3. Wood siding, stained finish
4. Board and Batten siding
5. Manufactured Stone Veneer
6. Metal Panel
7. Wood screen, stained finish
8. Wood structure, stained finish
9. Standing seam metal roof
10. Aluminum storefront
11. Steel structure, paint finish
12. Metal screen, paint finish
13. Metal fascia, paint finish
14. Precast coping
15. Metal / wood trellis, painted & stained
16. Metal / wood sunscreen, painted & stained
- P1. Benjamin Moore 2121-50 - Iced Cube Silver
- P2. Benjamin Moore 2127-40 - Wolf Gray
- P3. Benjamin Moore 2126-30 - Anchor Gray
- P4. Benjamin Moore 2108-50 - Silver Fox
- P5. Benjamin Moore 2111-40 - Tans Taupe
- P6. Benjamin Moore 2111-20 - Grizzly Bear Brown
- P7. Benjamin Moore HC91 - Danville Tan
- P8. Benjamin Moore HC151 - Buckland Blue
- W1. Olympic-Semi-transparent - Drift
- S1. Eldorado Stone - Stacked stone - Day/break

Elevations - BLDG S2 Planned Development Permit

The Shops at Folsom Ranch

Folsom, California August 11, 2018

Sheet 5 of 14





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R.A.N.C.H



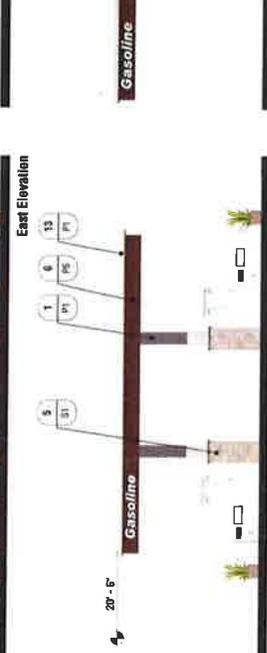
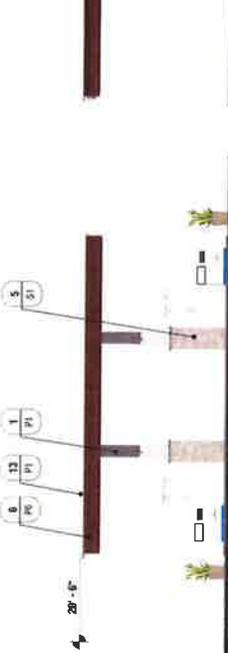
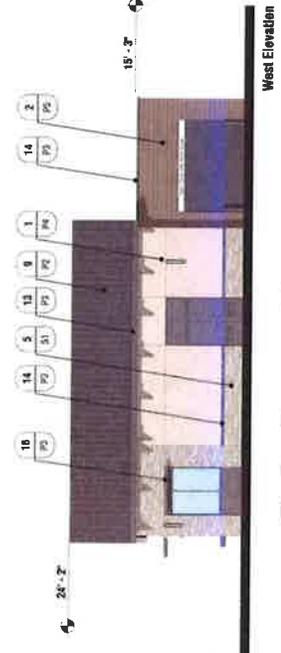
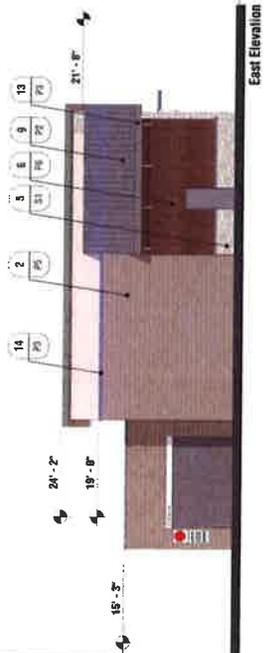
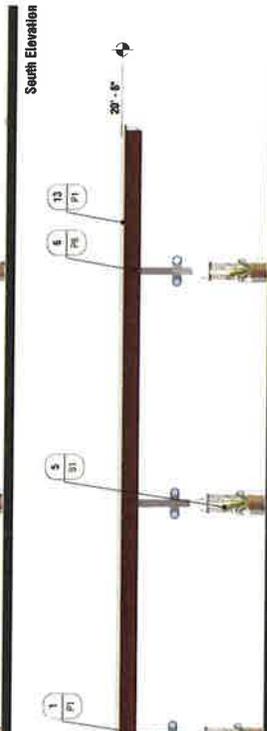
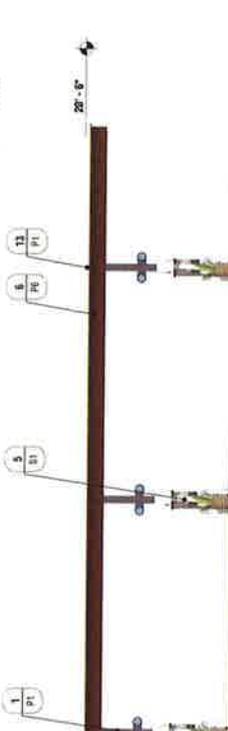
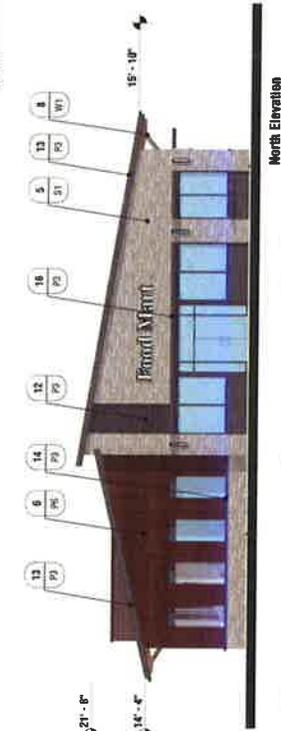
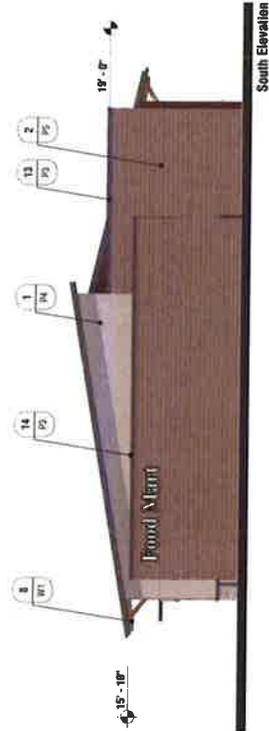
Materials & Colors

1. Cement plaster, paint finish
2. Fiber cement board siding, paint finish
3. Wood siding, stained finish
4. Board and Batten siding
5. Manufactured Stone Veneer
6. Metal Panel
7. Wood screen, stained finish
8. Wood structure, stained finish
9. Standing seam metal roof
10. Aluminum storefront
11. Steel structure, paint finish
12. Metal screen, paint finish
13. Metal fascia, paint finish
14. Precast coping
15. Metal / wood trellis, painted & stained
16. Metal / wood sunscreen, painted & stained
- P1. Benjamin Moore 2121-50 - Iced Cube Silver
- P2. Benjamin Moore 2127-40 - Wolf Gray
- P3. Benjamin Moore 2126-30 - Anchor Gray
- P4. Benjamin Moore 2106-50 - Silver Fox
- P5. Benjamin Moore 2111-40 - Taos Taupe
- P6. Benjamin Moore 2111-20 - Grizzly Bear Brown
- P7. Benjamin Moore HC91 - Danville Tan
- P8. Benjamin Moore HC151 - Backland Blue
- W1. Olympic-Semi-transparent - Drift
- S1. Eldorado Stone - Stacked stone - Daybreak

Elevations - BLDG P1 & Gas Station Planned Development Permit

The Shops at Folsom Ranch

Folsom, California August 31, 2018





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MACKAY & SOMPS
ENGINEERS PLANNERS SURVEYORS

SGPA ima
ARCHITECTURE AND PLANNING

Materials & Colors

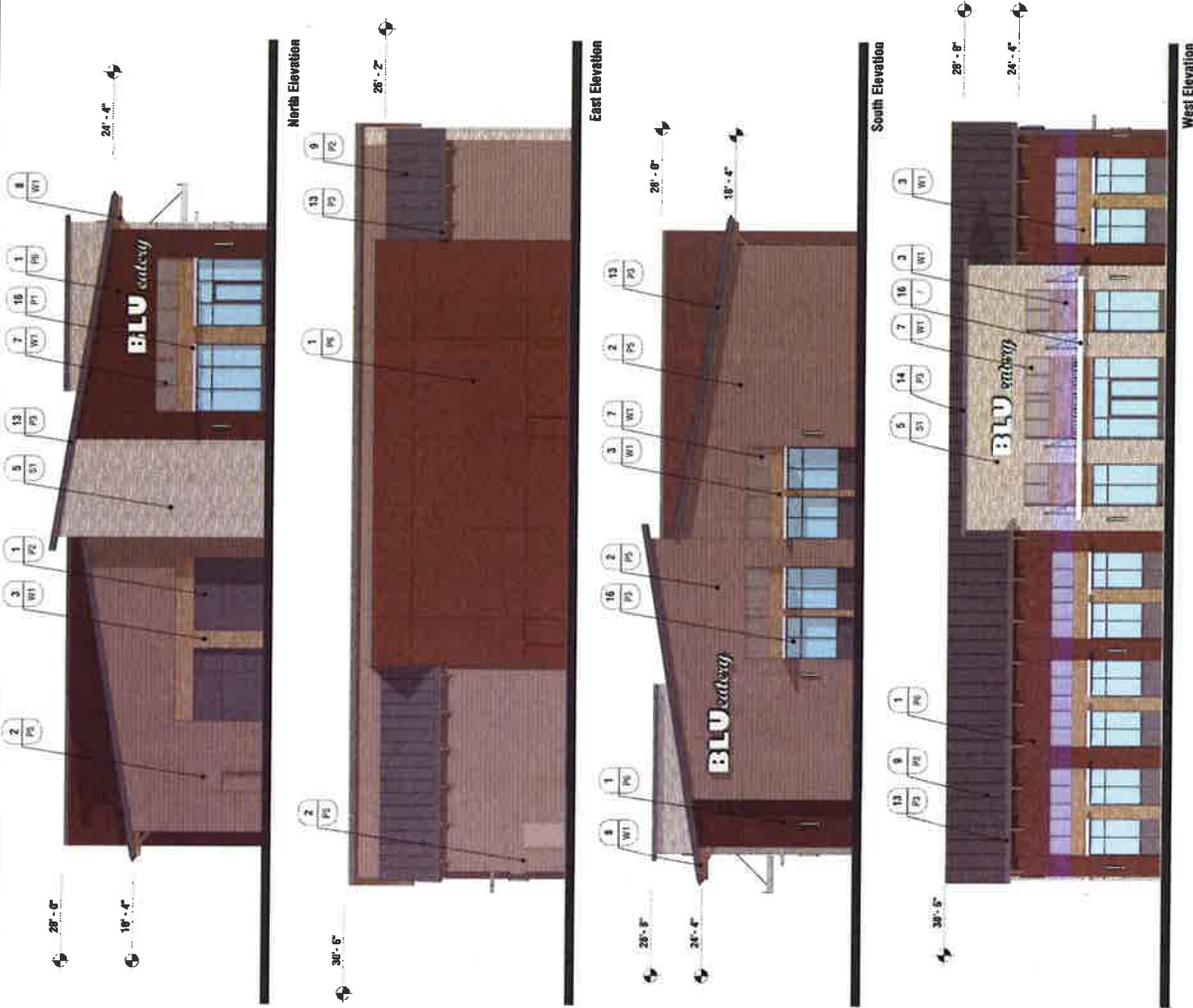
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4. Board and Batten siding
5. Manufactured Stone Veneer
6. Metal Panel
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8. Wood structure, stained finish
9. Standing seam metal roof
10. Aluminum storefront
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12. Metal screen, paint finish
13. Metal fascia, paint finish
14. Precast coping
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16. Metal / wood sunscreen, painted & stained
- P1. Benjamin Moore 2121-50 - Iced Cube Silver
- P2. Benjamin Moore 2127-40 - Wolf Gray
- P3. Benjamin Moore 2126-30 - Anchor Gray
- P4. Benjamin Moore 2108-50 - Silver Fox
- P5. Benjamin Moore 2111-40 - Taos Taupe
- P6. Benjamin Moore 2114-20 - Grizzly Bear Brown
- P7. Benjamin Moore HC51 - Danville Tan
- P8. Benjamin Moore HC151 - Buckland Blue
- WT1. Olympic-Semi-transparent - Drift
- S1. Eldorado Stone - Stacked stone - Daybreak

Elevations - BLDG P2 Planned Development Permit

The Shops at Folsom Ranch

Folsom, California
March 11, 2018

Sheet 8 of 18





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SURVEYORS

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AND PLANNING
ima

Materials & Colors

1. Cement plaster, paint finish
2. Fiber cement board siding, paint finish
3. Wood siding, stained finish
4. Board and Batten siding
5. Manufactured Stone Veneer
6. Metal Panel
7. Wood screen, stained finish
8. Wood structure, stained finish
9. Standing seam metal roof
10. Aluminum structure
11. Steel structure, paint finish
12. Metal screen, paint finish
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- P1. Benjamin Moore 2121-50 - Iced Cube Silver
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- P3. Benjamin Moore 2126-30 - Anchor Gray
- P4. Benjamin Moore 2108-50 - Silver Fox
- P5. Benjamin Moore 2111-40 - Taos Taupe
- P6. Benjamin Moore 2111-20 - Grizzly Bear Brown
- P7. Benjamin Moore HC31 - Danville Tan
- P8. Benjamin Moore HC151 - Buckland Blue
- W1. Olympic-Semi-transparent - Drift
- S1. Eldorado Stone - Stacked stone - Daybreak

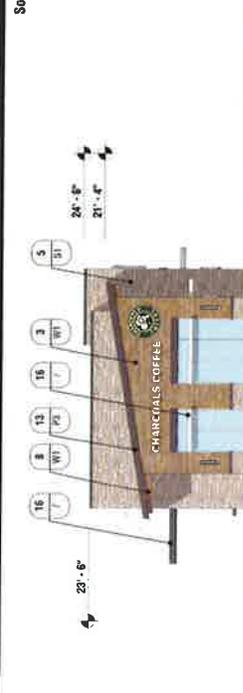
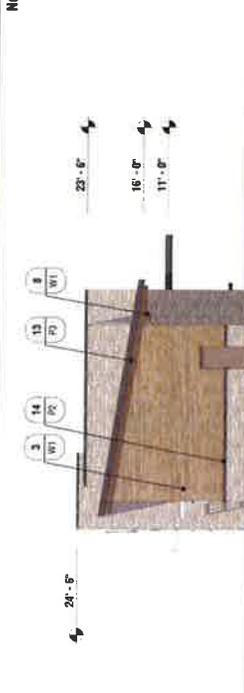
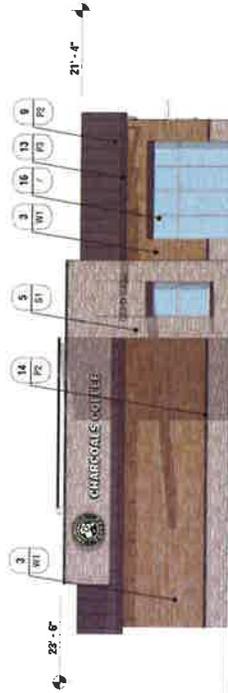
Elevations - BLDG P3

Planned Development Permit

The Shops at Folsom Ranch

Folsom, California 95630
Scale: 1/8" = 1'-0"

Sheet 3 of 18





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MACKAY & SOMPS
ENGINEERS PLANNERS SURVEYORS

SGPA
ARCHITECTURE
AND PLANNING



Materials & Colors

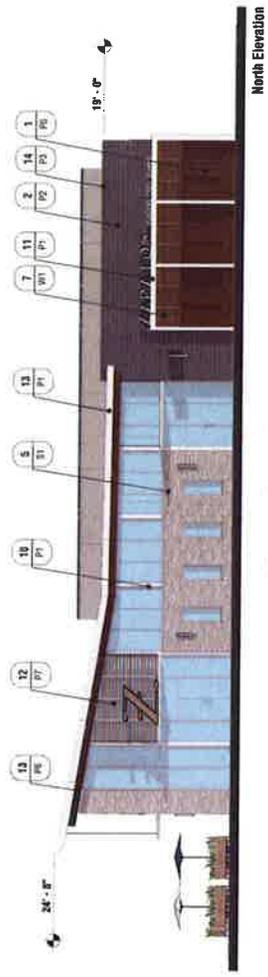
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- P4. Benjamin Moore 2108-50 - Silver Fox
- P5. Benjamin Moore 2111-40 - Teas Taine
- P6. Benjamin Moore 2111-20 - Grizzly Bear Brown
- P7. Benjamin Moore HC31 - Danville Tan
- P8. Benjamin Moore HC151 - Buckard Blue
- W1. Olympic-Semi-transparent - Drift
- S1. Eldorado Stone - Stacked stone - Daybreak

Elevations - BLDG P4

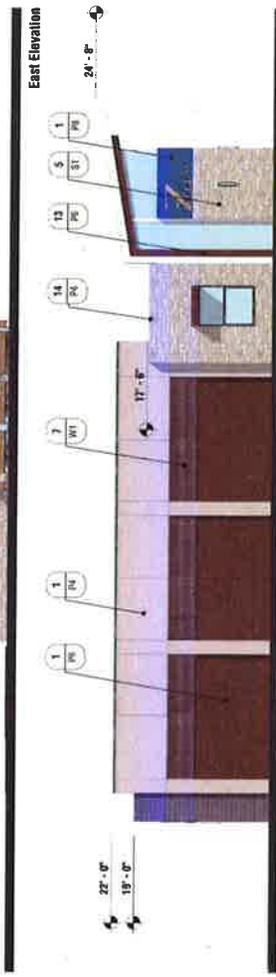
Planned Development Permit

The Shops at Folsom Ranch

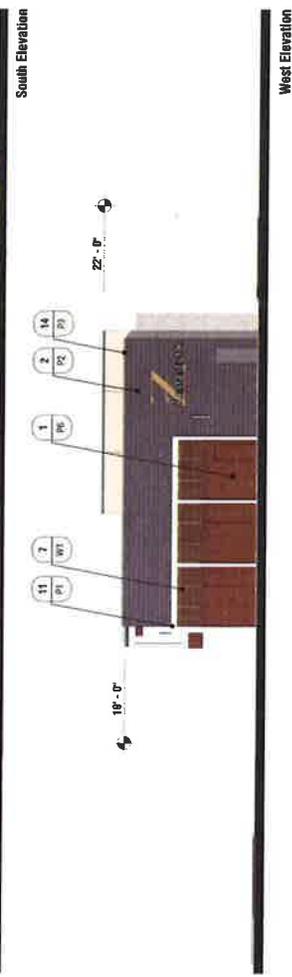
Folsom, California August 13, 2018



North Elevation



East Elevation



South Elevation



West Elevation

Attachment 13

Building Floor Plans, dated October 5, 2018



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R · A · N · C · H

PCCP

MACKAY & SOMPS
ENGINEERS
PLANNERS
SURVEYORS

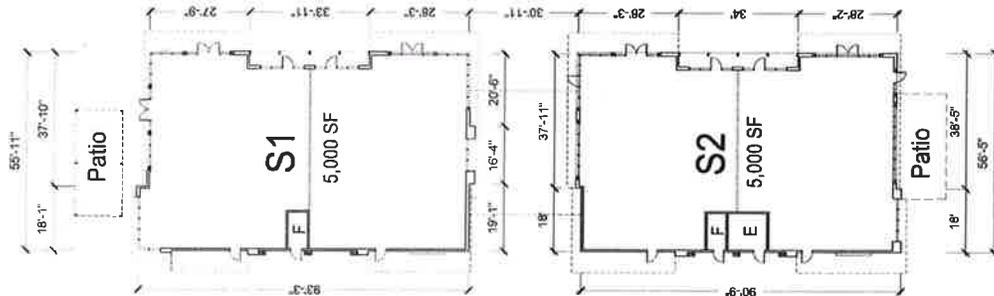
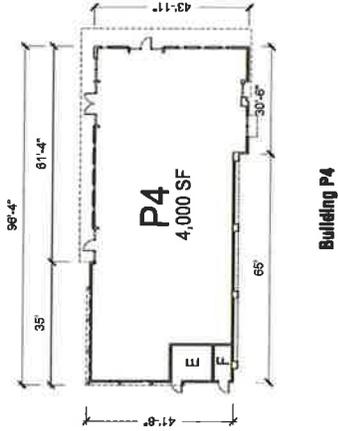
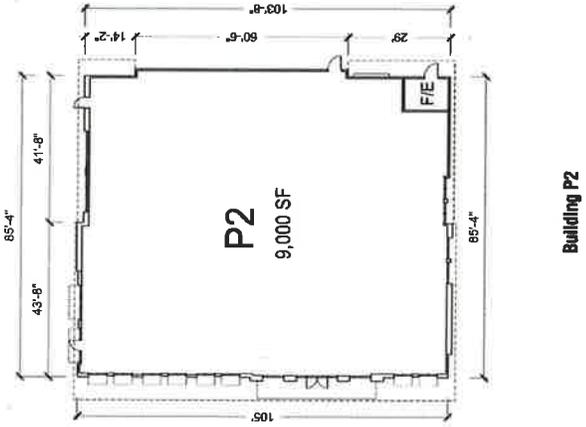
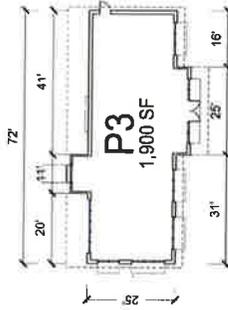
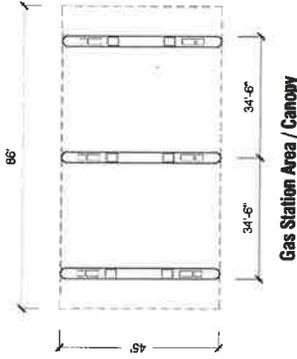
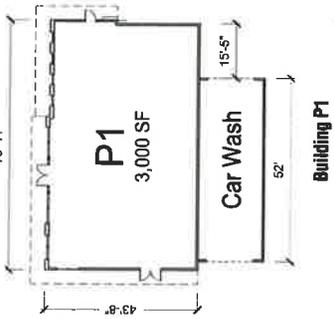
SGPA
ARCHITECTURE
AND PLANNING



Floor Plans - All Buildings
Planned Development Permit

The Shops
at Folsom Ranch

Folsom, California
October 14, 2014



Attachment 14

Building Renderings and Perspectives
Dated August 31, 2018 and October 5, 2018



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PCCP

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ENGINEERS PLANNERS SURVEYORS

SGPA *inua*
ARCHITECTURE
AND PLANNING



Perspectives
Planned Development Permit

The Shops
at Folsom Ranch

Note: Subject Perspectives in some cases remove proposed trees to afford views of the proposed buildings.



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SURVEYORS

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AND PLANNING



7



6



9



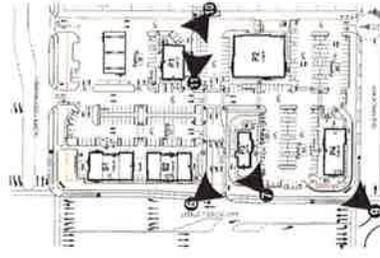
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11



10



Perspectives
Planned Development Permit

The Shops
at Folsom Ranch

Folsom, California
October 26, 2018

Note: Subject Perspectives in some cases remove proposed trees to afford views of the proposed buildings

Attachment 15

Color and Materials Board, dated August 31, 2018

Materials



1



5



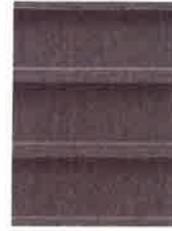
2



6



3



4

Paints



P1



P5



P2



P6



P3



P7



P4



P8



FOLSOM
R.A.N.C.H.

PCCP

MACKAY & SOMPS
BUSINESS PLANNERS SURVEYORS

SGPA *ima*
ARCHITECTURE AND PLANNING

Materials & Colors

1. Cement plaster, paint finish
2. Fiber cement board siding, paint finish
3. Wood siding, stained finish
4. Board and Batten siding
5. Manufactured Stone Veneer
6. Metal Panel
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- P1. Benjamin Moore 2121-50 - Iced Cube Silver
- P2. Benjamin Moore 2127-40 - Wolf Gray
- P3. Benjamin Moore 2126-30 - Anchor Gray
- P4. Benjamin Moore 2108-50 - Silver Fox
- P5. Benjamin Moore 2111-40 - Teas Taupe
- P6. Benjamin Moore 2111-20 - Grizzly Bear Brown
- P7. Benjamin Moore HC91 - Darville Tan
- P8. Benjamin Moore HC151 - Buckland Blue
- W1. Olympic-Semi-transparent - Drift
- S1. Eldorado Stone - Stacked stone - Daybreak

Color and Material Board
Planned Development Permit

The Shops
at Folsom Ranch

Attachment 16

**Roof Screening and Lighting Exhibit
Dated August 31, 2018**



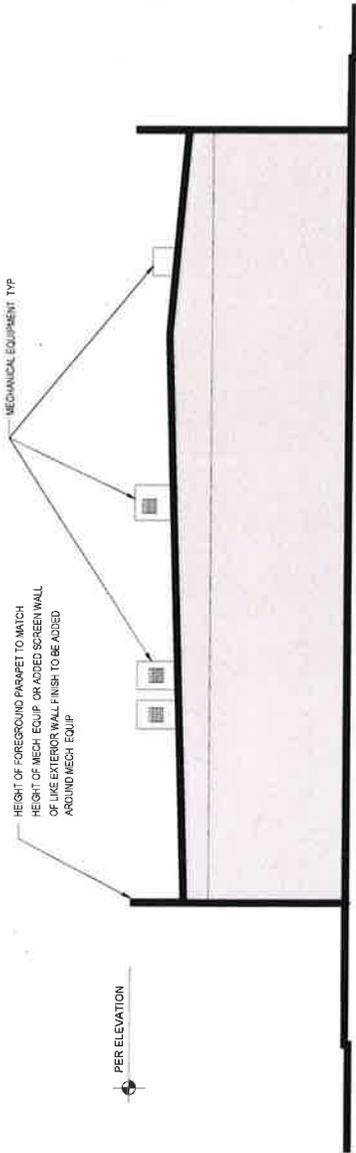
FOLSON
R.A.N.C.H.

PCCP

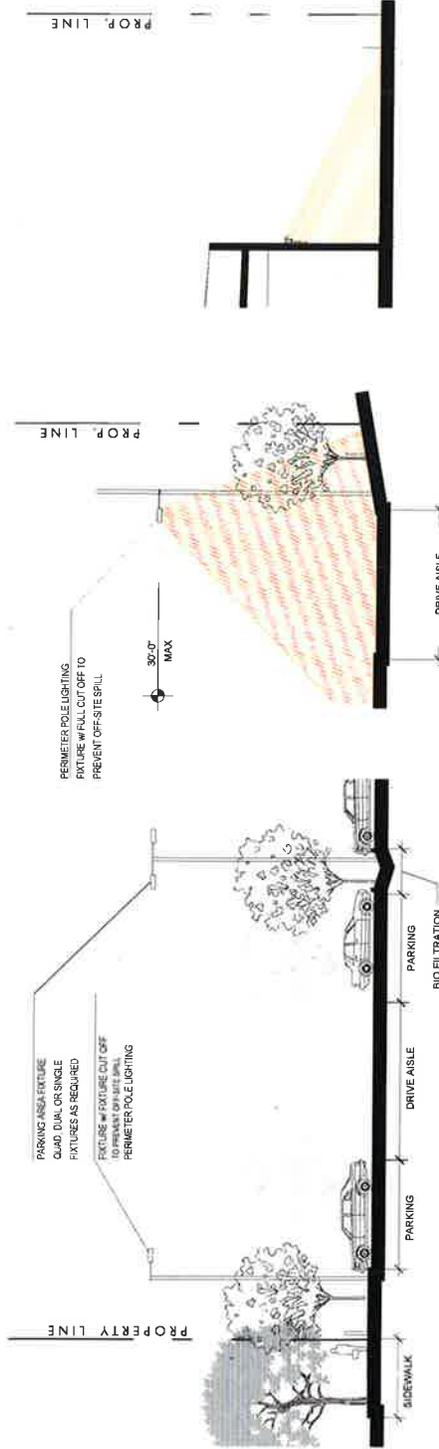
MACKAY & SOMPS
ENGINEERS PLANNERS SURVEYORS

SGPA
ARCHITECTURE AND PLANNING

ima



ROOF SCREENING CONCEPT



Roof Screening and Lighting Concept Exhibit
Planned Development Permit

The Shops
at Folsom Ranch

Folsom, California
August 11, 2014

Attachment 17

**Sign Criteria for The Shops at Folsom Ranch
Dated August 31, 2018**



SIGN CRITERIA
FOR
THE SHOPS AT FOLSOM RANCH
FOLSOM, CA

- I. General Guidelines
 - A. General Requirements
 - B. Restrictions
 - C. Construction Requirements
 - D. Leasing Sign (Temporary)
- II. Free Standing Signs
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 - C. Free Standing Project Signs
 - D. Free Standing Tenant Sign
- III. Building Signs
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 - C. Tenant Canopy Signage
 - D. Tenant Blade Signage
- IV. Material and Color

August 31, 2018



I. General Guidelines

The Goal of this sign program is to ensure the uniformity and consistency of the signage for the entire center.

A sign program shall be included with each individual project and submitted to City Planning staff for review and approval prior to issuance of sign permit. Any sign application not in compliance with the sign criteria shall be modified to comply with the overall sign criteria approved by the Planning Commission.

The following criteria will aid in eliminating excessive and confusing sign displays, preserve and enhance the appearance of the shopping center development, safeguard and enhance property values, and will encourage the use of quality signage that will be harmonious to the buildings and sites that it occupies.

A. General Requirements

1. Tenant shall submit before fabrication, four (4) copies of drawings of the proposed signs, including one (1) copy which is to be colored, to the landlord for approval. These drawings must include total number of signs, location, size, style of lettering, material, type of illumination, installation details, color selection, logo design, and wall graphics, and must comply with The Shops at Folsom Ranch Sign Criteria. Each tenant-building or tenant will submit to the landlord the proposed signage for their prospective space or building and will be viewed and approved on a case-by-case basis.
2. All permits for signs shall be obtained and paid for by the tenant. The tenant shall pay for all signs and their installation and maintenance.
3. Tenant shall be responsible for the fulfillment of all requirements and specifications.
4. The size, location, design, color, texture, lighting and materials of these signs shall in no way detract from the design of the shopping center and the surrounding properties. Signage face colors, returns & trims are subject to review and approval by landlord. White colored face signage is more desirable. Use Pantone Snow White 11-0602 TCX.
5. All signs, permits, drawings, and related expenses to be at tenant's sole cost and expense.

B. Restrictions

1. Logos or manufacturer's decals, hours of business, telephone numbers, etc., are limited to a total of 144 square inches per single door entrance. All "sale" signs, special announcements, etc. are not permitted on interior face of exterior glass. Such advertising materials must be set back 24 inches from glass surface, and all window signs are not to exceed a maximum total of 25% of total window area.
2. Advertising devices such as attraction boards, banners and flags will not be permitted. Promotional banners to be utilized on a temporary basis must be approved by the property landlord of the retail center and be consistent with The Shops at Folsom Ranch Sign Criteria.
3. Flashing, animated, audible, revolving or other signs that create the illusion of animation are not permitted.
4. No exposed junction boxes, lamps, tubing, conduits, raceways or neon cross-overs of any type are permitted.
5. There shall be no roof top signs, or signs which extend above the parapet wall of the roofline of the building to which they are attached.
6. No "Can Signs" are allowed except for corporate logos.

C. Construction Requirements

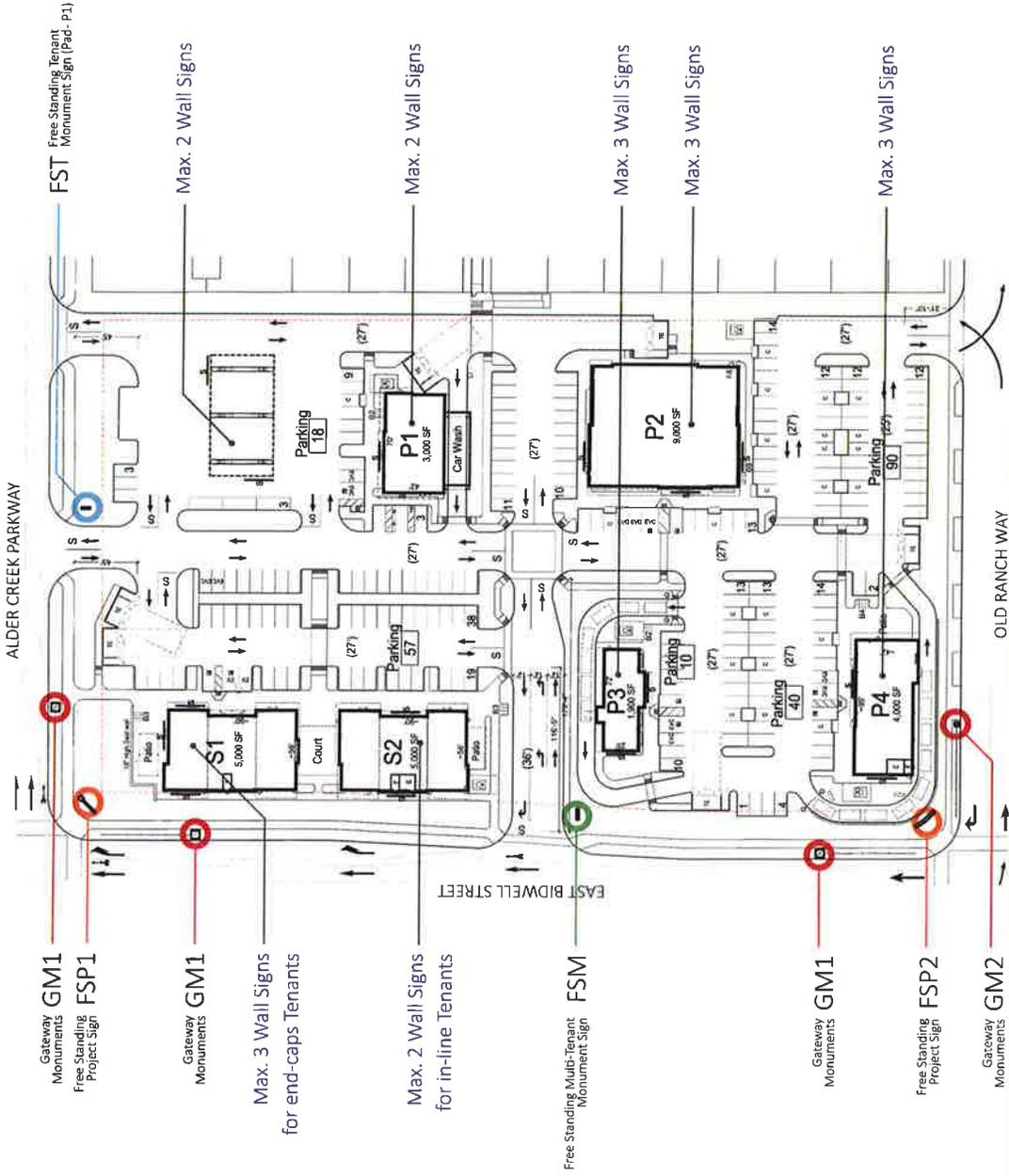
1. Tenant is required to obtain from the City of Folsom Building Department, any and all required sign, building and electrical permits.
2. Location of all openings for conduit in sign panels of building walls shall be indicated by the sign contractor on drawings submitted to landlord.
3. Each sign contractor must seal off (watertight) and touch up all mounting holes and leave premises free of debris after installation. The general contractor or landlord is authorized to correct all such work at the expense of tenant.
4. All signs must bear the "U.L.L." label, and the installation must comply with the County of Sacramento Building and Electrical Codes.
5. Tenant shall be responsible for the operations of their sign contractor.

D. Leasing Signage (Temporary)

1. Landlord will have the option to place a maximum of two (2) portable leasing signs at any location within the property boundaries at their discretion for advertising vacancies as long as signs do not block any traffic or pedestrian views.
2. Leasing Signs will be constructed in a professional manner and will not exceed 7' tall x 5' wide.

II. Free Standing Signs

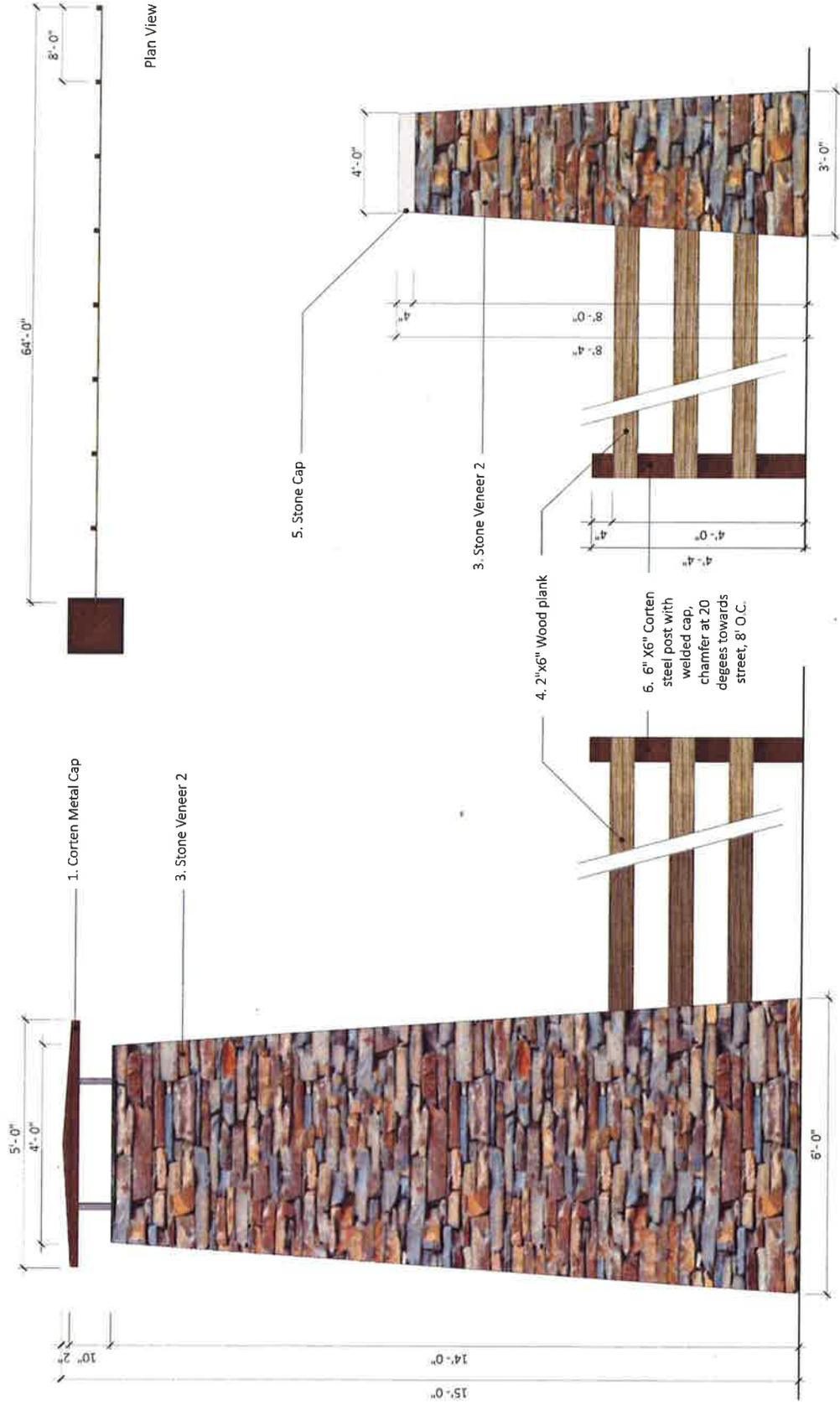
A. Sign Location Map



Note: See additional information on Tenant Wall Signage on pages 8, 9 & 10, Tenant Blade Signage on page 11 and Tenant Storefront signage on pages 2 & 8.

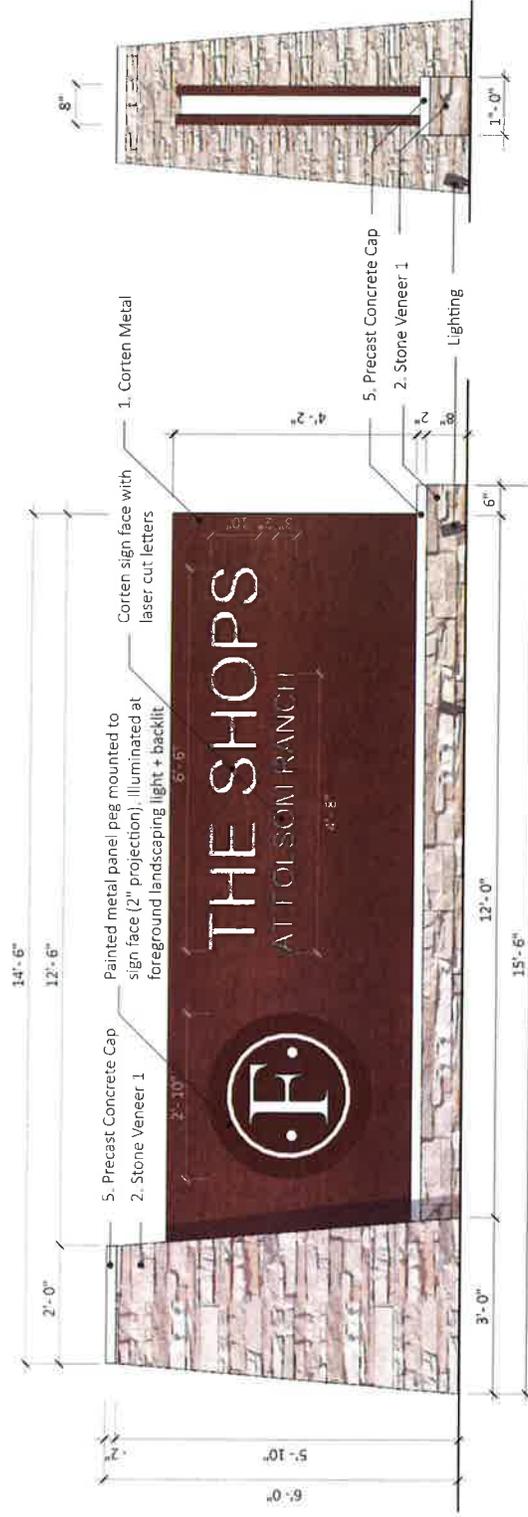


B. GM- Gateway Monuments (Per sign concepts by Hart Towernton)



C. FSP- Free Standing Project Signs

- Freestanding Signs**
1. Two free standing project signs shall be allowed, installed at northwest and southwest corners of the site, not to exceed 60 sq. ft. in area and 6' in height measured per Shops at Folsom Ranch Sign Criteria.
 2. Signs shall be designed to match overall scheme and colors of the shopping center.



Front View

Side View

Front View

Plan View

FSP1 Free Standing Project Sign



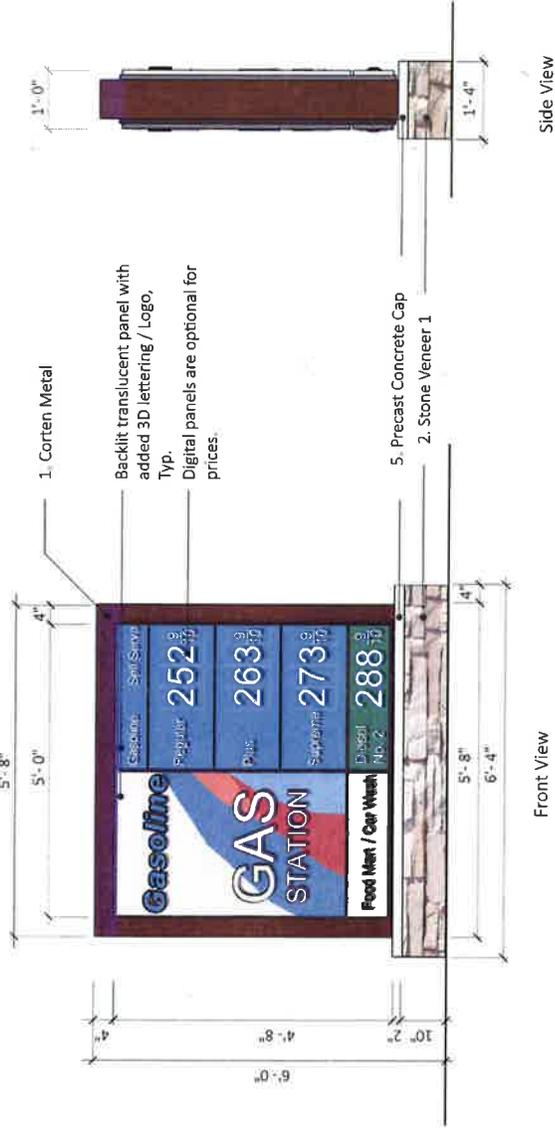
FSP2 Free Standing Project Sign

D. FST - Free Standing Tenant Monument Sign

Freestanding Signs

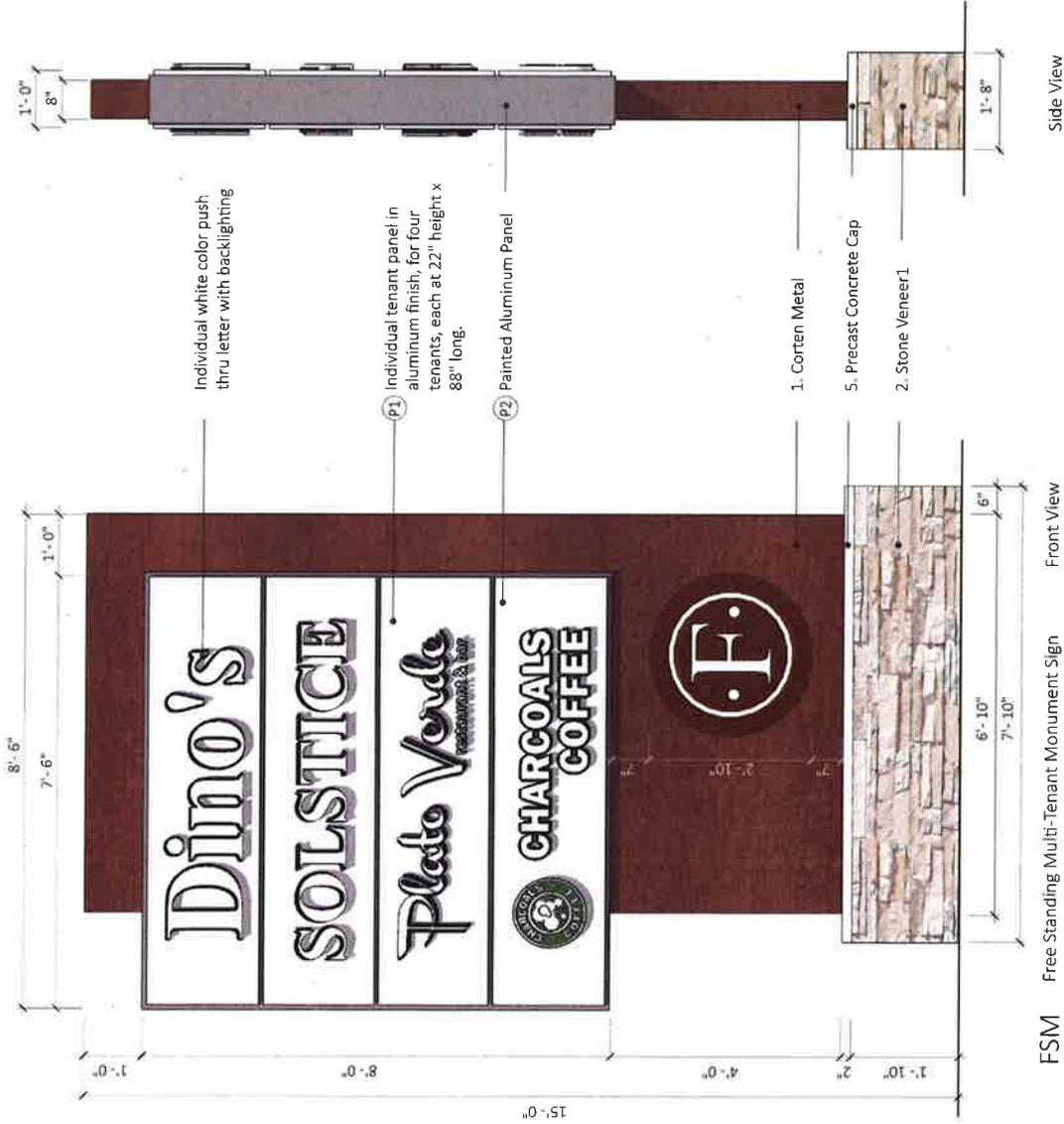
Freestanding signs shall be set back 5 feet from the public right-of-way, located outside required clear vision triangles in a landscaped planting area. However, signs attached to a fence or soundwall may be located within the required setback area. Along arterial roads designated in the General Plan, freestanding signs shall be externally lit, nonilluminated, or internally illuminated with opaque (nontranslucent) backgrounds. Freestanding signs may include changeable copy, provided, that the sign area for changeable copy does not exceed 75 percent of the sign's area. Corporate flags used to identify a business or entity shall be calculated into the maximum allowable freestanding sign area.

Building Pad - P1 is allowed one (1) free standing monument sign at Alder Creek Parkway Frontage, not to exceed a maximum sign area of 24 square feet and a maximum height of 6 feet, including a maximum 2-foot tall base.



FST Free Standing Tenant Monument Sign (Pad-P1)

D. FSM - Free Standing Multi-tenant Monument Sign



FSM Free Standing Multi-Tenant Monument Sign

Side View

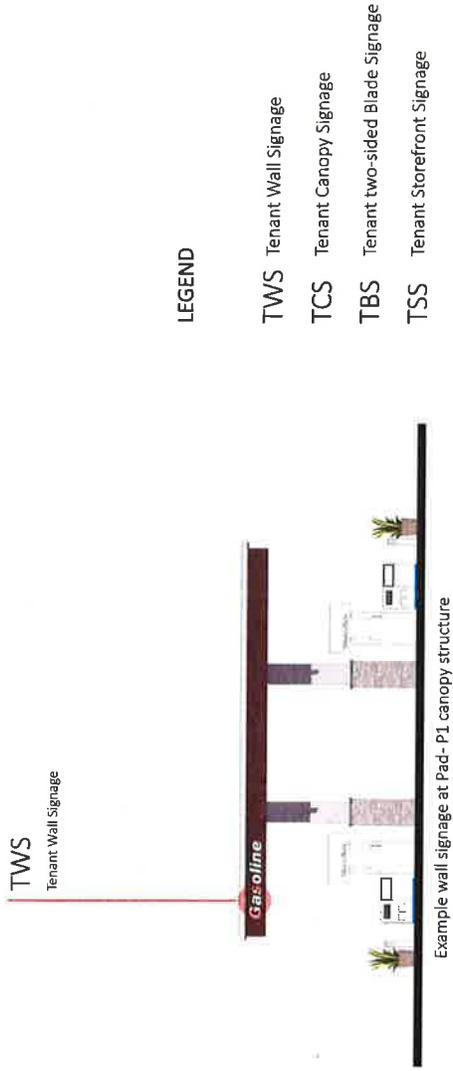
Freestanding Signs

Freestanding signs shall be set back 5 feet from the public right-of-way, located outside required clear vision triangles in a landscaped planting area. However, signs attached to a fence or soundwall may be located within the required setback area. Along arterial roads designated in the General Plan, freestanding signs shall be externally lit, nonilluminated, or internally illuminated with opaque (nontranslucent) backgrounds. Freestanding signs may include changeable copy; provided, that the sign area for changeable copy does not exceed 75 percent of the sign's area. Corporate flags used to identify a business or entity shall be calculated into the maximum allowable freestanding sign area.

Integrated Developments: Integrated developments with multiple businesses are permitted 1 freestanding monument sign (exclusive of allowable building attached signage) with a maximum sign area of 60 square feet and a 15-foot height limit to identify the name of the center or development, address, and tenants within the development.

III. Building Signs

A. Building Sign Location

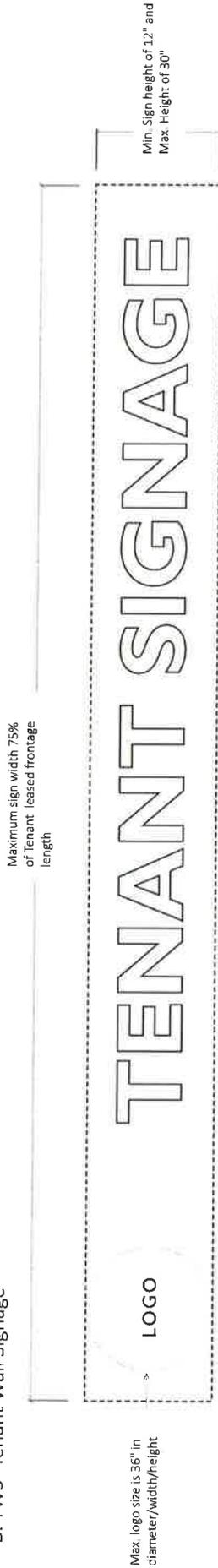


LEGEND

- TWS Tenant Wall Signage
- TCS Tenant Canopy Signage
- TBS Tenant two-sided Blade Signage
- TSS Tenant Storefront Signage



B. TWS- Tenant Wall Signage



DESCRIPTION

Wall mounted signage for the purpose of primary retail tenant identification facing the street and/or parking area.

GENERAL GUIDELINES

1. Signage shall be internally illuminated fabricated channel letters and shapes. Illumination may be through the face or "halo lit" where light spills out the back of the sign onto the wall surface. Creative signage is encouraged and allows hand painted wall signage/ graphics, face lit metal cut out letters.
2. Tenants may use their corporate logos, colors and layouts.
3. Letter/logo return (edge) color may vary.
4. Two lines of text (except where specifically prohibited) may be used but the total height shall not exceed maximum letter height set forth in the following sections.
5. All signs shall be U.L. approved. All wiring, conduits and raceways are to be concealed.
6. Light fixtures must utilize LED lamps.

NUMBER OF SIGNS, LETTER SIZES & AREAS

(Tenant Under 15,000 SF)

SIGN AREA: Maximum Sign Area: One and a half square feet of signage for each 1 lineal foot of primary tenant leased frontage length up to a maximum of 150 square feet.

LETTER HEIGHT: 12" Min, 30" Max, (16" Min. and 36" Max. with double line sign)

SIGN LOCATION & SIGN: Building signs may be placed on building facades facing a street, public parking lot, but may not be located above the roof line. Wall signs shall not project more than 18 inches from the building wall and the sign length shall not exceed 75 percent of the tenant leased frontage length.

NUMBER OF SIGNS: One (1) wall or canopy sign per frontage. End Cap tenants with unique architectural features at their storefront that present more than one frontage, may have one (1) additional wall sign, for a maximum total of three (3) signs.



Example of Hand Painted Letters

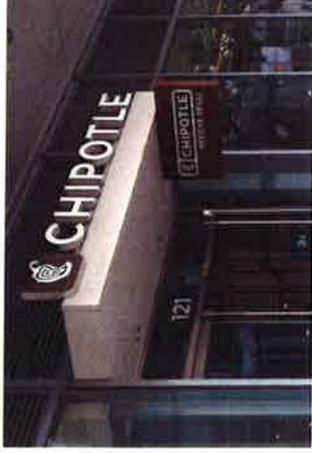
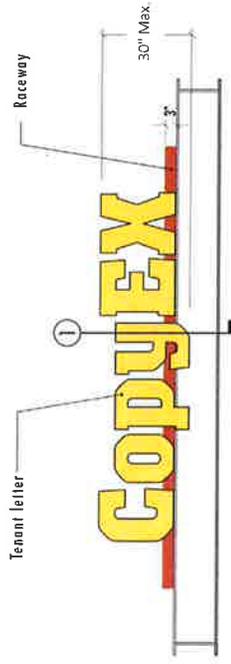


Example of Individual Illuminated Letters



Example of Non-Illuminated Letters

C. TCS- Tenant Canopy Signage (Option to Wall Signage)



Example of Canopy Signage

DESCRIPTION

At storefronts where space is not available to mount a sign to a wall surface, canopy mounted signage may be utilized for the purpose of primary retail tenant identification facing the street and/or parking area.

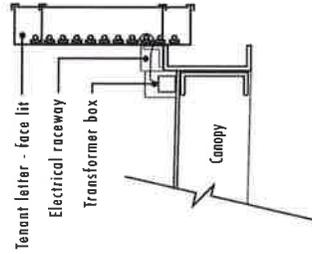
GENERAL GUIDELINES

1. Signage shall be internally illuminated fabricated channel letters/shapes, detailed as shown in examples on this page. If a backer panel is utilized, a reverse channel letter with halo lighting may be utilized.
2. Tenants may use their corporate logos, colors and layouts.
3. Letter/logo return (edge) color may vary.
4. Single line of copy only
5. All signs shall be U.L. approved. All wiring, conduits and raceways are to be concealed.

6. Electrical components and wiring shall be concealed within raceway which is visually integrated with the canopy design.
7. Light fixtures must utilize LED lamps.
8. The raceway for channel letter signs, electrical raceway must be placed on top of canopy face channel as shown in examples on this page. Raceway shall be 3" tall by 80% of the width of the canopy. Raceway to be painted color to match canopy.
9. The sign location is to be placed on top of canopy structure. If channel letters used, no part of sign may extend below bottom edge of canopy.

NUMBER OF SIGNS:

One (1) canopy sign per entry frontage. End Cap tenants not identified on monument signs and/or tenants with unique architectural features at their storefront that present more than one frontage, may have one (1) additional wall sign, for a maximum total of two (2) signs.



LED Channel Letter Section

D. TBS- Tenant Blade Signage



Example of Blade Signage

DESCRIPTION

Signage for the purpose of secondary retail tenant identification facing the pedestrian walkway areas.

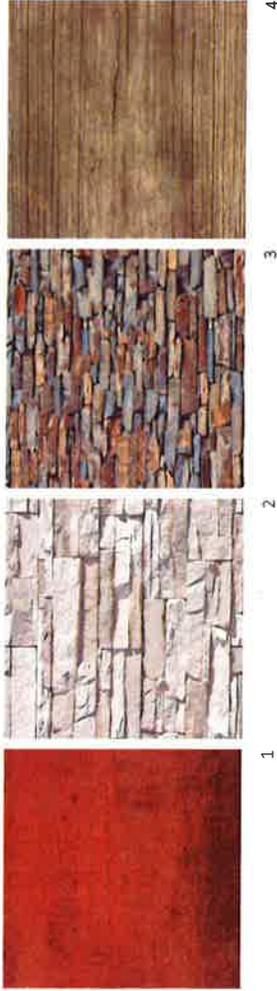
QUANTITY: One sign per tenant storefront.

SIGN AREA : 4 S.F. Max

GENERAL GUIDELINES

1. Signage shall be framed, LED illuminated panels with cutout letters mounted to face.
2. Tenants may use their corporate logos and layouts.
3. Sign panel, bracket and frame must be standard size and colors as indicated in the sign specifications.
4. Sign shall not extend beyond the canopy fascia plane.
5. Sign lettering appears on two sides of panel.
6. The sign must maintain 8' min clearance over the walkway.

IV. Material and Color



Material

1. Corten Metal
2. Stone Veneer 1 - Eldorado Stone- Stacked stone- Daybreak
3. Stone Veneer 2 - Ledge stone, dry-stack appearance
4. Wood plank- Saw-cut finish, to complement stone selection
5. Precast Concrete Cap



Color / Paint

- P1 Benjamin Moore 2121-50- Iced Cube Silver
- P2 Benjamin Moore 2127-40- Wolf Gray

Attachment 18

**Shops at Folsom Ranch CEQA Exemption and
Streamlining Analysis (Bound Separately)**

Attachment 19

**Mitigation Monitoring and Reporting Program for
Westland-Eagle Specific Plan Amendment**

Table 1: Mitigation Monitoring and Reporting Plan for the Westland/Eagle Area of the Folsom Plan Area Specific Plan
 (The Westland/Eagle Specific Plan Amendment to the Folsom Plan Area Specific Plan was approved in September 2015)

Mitigation Measure	Timing	Implementation	Enforcement	Compliance/Notes
<p>Mitigation Measure 3A.1-5: Establish and Require Conformance to Lighting Standards and Prepare and Implement a Lighting Plan.</p> <p>To reduce impacts associated with light and glare, the City shall:</p> <ul style="list-style-type: none"> Establish standards for on-site outdoor lighting to reduce high-intensity nighttime lighting and glare as part of the Folsom Specific Plan design guidelines/standards. Consideration shall be given to design features, namely directional shielding for street lighting, parking lot lighting, and other substantial light sources, that would reduce effects of nighttime lighting. In addition, consideration shall be given to the use of automatic shutoffs or motion sensors for lighting features to further reduce excess nighttime light. Use shielded or screened public lighting fixtures to prevent the light from shining off of the surface intended to be illuminated. <p>To reduce impacts associated with light and glare, the project applicant(s) of all project phases shall:</p> <ul style="list-style-type: none"> Shield or screen lighting fixtures to direct the light downward and prevent light spill on adjacent properties. Flood and area lighting needed for construction activities, nighttime sporting activities, and/or security shall be screened or aimed no higher than 45 degrees above straight down (half-way between straight down and straight to the side) when the source is visible from any off-site residential property or public roadway. For public lighting in residential neighborhoods, prohibit the use of light fixtures that are of unusually high intensity or brightness (e.g., harsh mercury vapor, low-pressure sodium, or fluorescent bulbs) or that blink or flash. Use appropriate building materials (such as low-glare glass, low-glare building glaze or finish, neutral, earth-toned colored paint and roofing materials), shielded or screened lighting, and appropriate signage in the office/commercial areas to prevent light and glare from adversely affecting motorists on nearby roadways. Design exterior on-site lighting as an integral part of the building and landscape design in the Folsom Specific Plan area. Lighting fixtures shall be architecturally consistent with the overall site design. Lighting of off-site facilities within the City of Folsom shall be consistent with the City's General Plan standards. Lighting of the off-site detention basin shall be consistent with Sacramento County General Plan standards. 	<p>Before approval of building permits</p>	<p>Project applicant(s) for any particular discretionary development application.</p>	<p>1. For all on-site and off-site facilities that would be located within the City of Folsom: City of Folsom Neighborhood Services Department and City of Folsom Community Development Department. 2. For the off-site detention basin: Sacramento County Planning Department. 3. For the two local roadways off-site into El Dorado Hills: El Dorado County Community Services Department.</p>	

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<ul style="list-style-type: none"> Lighting of the two local roadway connections from Folsom Heights off-site into El Dorado Hills shall be consistent with El Dorado County General Plan standards. A lighting plan for all on- and off-site elements within the each agency's jurisdictional boundaries (specified below) shall be submitted to the relevant jurisdictional agency for review and approval, which shall include the above elements. The lighting plan may be submitted concurrently with other improvement plans, and shall be submitted before the installation of any lighting or the approval of building permits for each phase. The project applicant(s) for any particular discretionary development application shall implement the approved lighting plan. Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties). 				
<p>3A.2 – Air Quality</p> <p>Mitigation Measure 3A.2-1a: Implement Measures to Control Air Pollutant Emissions Generated by Construction of On-Site Elements.</p> <p>To reduce short-term construction emissions, the project applicant(s) for any particular discretionary development application shall require their contractors to implement SMAQMD's list of Basic Construction Emission Control Practices, Enhanced Fugitive PM Dust Control Practices, and Enhanced Exhaust Control Practices (list below) in effect at the time individual portions of the site undergo construction. In addition to SMAQMD-recommended measures, construction operations shall comply with all applicable SMAQMD rules and regulations.</p> <p>Basic Construction Emission Control Practices</p> <ul style="list-style-type: none"> Water all exposed surfaces two times daily. Exposed surfaces include, but are not limited to soil piles, graded areas, unpaved parking areas, staging areas, and access roads. Cover or maintain at least two feet of free board space on haul trucks transporting soil, sand, or other loose material on the site. Any haul trucks that would be traveling along freeways or major roadways should be covered. Use wet power vacuum street sweepers to remove any visible trackout mud or dirt onto adjacent public roads at least once a day. Use of dry power sweeping is prohibited. Limit vehicle speeds on unpaved roads to 15 miles per hour (mph). 	<p>Before the approval of all grading plans by the City and throughout project construction, where applicable, for all project phases.</p>	<p>The project applicant(s) of all project phases.</p>	<p>City of Folsom Community Development Department</p>	

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<ul style="list-style-type: none"> • All roadways, driveways, sidewalks, parking lots to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used. • Minimize idling time either by shutting equipment off when not in use or reducing the time of idling to 5 minutes (as required by the state airborne toxics control measure [Title 13, Section 2485 of the California Code of Regulations]). Provide clear signage that posts this requirement for workers at the entrances to the site. • Maintain all construction equipment in proper working condition according to manufacturer's specifications. The equipment must be checked by a certified mechanic and determine to be running in proper condition before it is operated. <p>Enhanced Fugitive PM Dust Control Practices – Soil Disturbance Areas</p> <ul style="list-style-type: none"> • Water exposed soil with adequate frequency for continued moist soil. However, do not overwater to the extent that sediment flows off the site. • Suspend excavation, grading, and/or demolition activity when wind speeds exceed 20 mph. • Plant vegetative ground cover (fast-germinating native grass seed) in disturbed areas as soon as possible. Water appropriately until vegetation is established. <p>Enhanced Fugitive PM Dust Control Practices – Unpaved Roads</p> <ul style="list-style-type: none"> • Install wheel washers for all exiting trucks, or wash off all trucks and equipment leaving the site. • Treat site accesses to a distance of 100 feet from the paved road with a 6 to 12-inch layer of wood chips, mulch, or gravel to reduce generation of road dust and road dust carryout onto public roads. • Post a publicly visible sign with the telephone number and person to contact at the construction site regarding dust complaints. This person shall respond and take corrective action within 48 hours. The phone number of SMAQMD and the City contact person shall also be posted to ensure compliance. <p>Enhanced Exhaust Control Practices</p> <ul style="list-style-type: none"> • The project shall provide a plan, for approval by the City of Folsom Community Development Department and SMAQMD, demonstrating that the heavy-duty (50 horsepower [hp] or more) off-road vehicles to be used in the construction project, including owned, leased, and subcontractor vehicles, will achieve a project wide fleet-average 20% NOX reduction and 45% particulate reduction compared to the most current California Air Resources Board (ARB) fleet average that exists at the time of construction. Acceptable options for reducing emissions may include use of late-model engines, low-emission diesel products, 				

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<p>alternative fuels, engine retrofit technology, after-treatment products, and/or other options as they become available. The project applicant(s) of each project phase or its representative shall submit to the City of Folsom Community Development Department and SMAQMD a comprehensive inventory of all off-road construction equipment, equal to or greater than 50 hp, that would be used an aggregate of 40 or more hours during any portion of the construction project. The inventory shall include the horsepower rating, engine production year, and projected hours of use for each piece of equipment. The inventory shall be updated and submitted monthly throughout the duration of the project, except that an inventory shall not be required for any 30-day period in which no construction activity occurs. At least 48 hours prior to the use of heavy-duty off-road equipment, the project representative shall provide SMAQMD with the anticipated construction timeline including start date, and name and phone number of the project manager and on-site foreman. SMAQMD's Construction Mitigation Calculator can be used to identify an equipment fleet that achieves this reduction (SMAQMD 2007a). The project shall ensure that emissions from all off-road diesel powered equipment used on the SPA do not exceed 40% opacity for more than three minutes in any one hour. Any equipment found to exceed 40 percent opacity (or Ringelmann 2.0) shall be repaired immediately, and the City and SMAQMD shall be notified within 48 hours of identification of non-compliant equipment. A visual survey of all in-operation equipment shall be made at least weekly, and a monthly summary of the visual survey results shall be submitted throughout the duration of the project, except that the monthly summary shall not be required for any 30-day period in which no construction activity occurs. The monthly summary shall include the quantity and type of vehicles surveyed as well as the dates of each survey. SMAQMD staff and/or other officials may conduct periodic site inspections to determine compliance. Nothing in this mitigation measure shall supersede other SMAQMD or state rules or regulations.</p> <ul style="list-style-type: none"> • if at the time of construction, SMAQMD has adopted a regulation or new guidance applicable to construction emissions, compliance with the regulation or new guidance may completely or partially replace this mitigation if it is equal to or more effective than the mitigation contained herein, and if SMAQMD so permits. <p>Mitigation Measure 3A.2-1b: Pay Off-site Mitigation Fee to SMAQMD to Off-Set NOX Emissions Generated by Construction of On-site Elements.</p>	<p>Before the approval of all grading plans by the City and</p>	<p>The project applicant(s) of</p>	<p>The City of Folsom Community Development</p>	

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<p>Implementation of the Proposed Project or the other four other action alternatives would result in construction-generated NOX emissions that exceed the SMAQMD threshold of significance, even after implementation of the SMAQMD Enhanced Exhaust Control Practices (listed in Mitigation Measure 3A.2-1a). Additionally, Mitigation Measure 3A.4-1 (Implement Additional Measures to Control Construction-Generated GHG Emissions, pages 3A.4-14 to 15) has the potential to both reduce and increase NOX emissions, depending on the types of alternative fuels and engine types employed.</p> <p>Therefore, the project applicant(s) shall pay SMAQMD an off-site mitigation fee for implementation of any of the five action alternatives for the purpose of reducing NOX emissions to a less-than-significant level (i.e., less than 85 lb/day). All NOX emission reductions and increases associated with GHG mitigation shall be added to or subtracted from the amount above the construction threshold to determine off-site mitigation fees, when possible. The specific fee amounts shall be calculated when the daily construction emissions can be more accurately determined: that is, if the City/USACE select and certify the EIR/EIS and approves the Proposed Project or one of the other four other action alternatives, the City and the applicants must establish the phasing by which development would occur, and the applicants must develop a detailed construction schedule. Calculation of fees associated with each project development phase shall be conducted by the project applicant(s) in consultation with SMAQMD staff before the approval of grading plans by the City. The project applicant(s) for any particular discretionary development application shall pay into SMAQMD's off-site construction mitigation fund to further mitigate construction-generated emissions of NOX that exceed SMAQMD's daily emission threshold of 85 lb/day. The calculation of daily NOX emissions shall be based on the cost rate established by SMAQMD at the time the calculation and payment are made. At the time of writing this EIR/EIS the cost rate is \$16,000 to reduce 1 ton of NOX plus a 5% administrative fee (SMAQMD 2008c). The determination of the final mitigation fee shall be conducted in coordination with SMAQMD before any ground disturbance occurs for any project phase. Based on information available at the time of writing this EIR/EIS, and assuming that construction would be performed at a consistent rate over a 19-year period (and averaging of 22 work days per month), it is estimated that the off-site construction mitigation fees would range from \$517,410 to \$824,149, depending on which alternative is selected. Because the fee is based on the mass quantity of emissions that exceed SMAQMD's daily threshold of significance of 85 lb/day, total fees would be substantially greater if construction activity is more intense during some phases and less intense during other phases of the 19-year build out period, and in</p>	<p>throughout project construction for all project phases.</p>	<p>all project phases.</p>	<p>Department shall not grant any grading permits to the respective project applicant(s) until the respective project applicant(s) have paid the appropriate off-site mitigation fee to SMAQMD.</p>	

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any event, based on the actual cost rate applied by SMAQMD. (This fee is used by SMAQMD to purchase off-site emissions reductions. Such purchases are made through SMAQMD's Heavy Duty Incentive Program, through which select owners of heavy-duty equipment in Sacramento County can repower or retrofit their old engines with cleaner engines or technologies.)				
<p>Mitigation Measure 3A.2-1c: Analyze and Disclose Projected PM10 Emission Concentrations at Nearby Sensitive Receptors Resulting from Construction of On-Site Elements</p> <p>Prior to construction of each discretionary development entitlement of on-site land uses, the project applicant shall perform a project-level CEQA analysis (e.g., supporting documentation for an exemption, negative declaration, or project-specific EIR) that includes detailed dispersion modeling of construction-generated PM10 to disclose what PM10 concentrations would be at nearby sensitive receptors. The dispersion modeling shall be performed in accordance with applicable SMAQMD guidance that is in place at the time the analysis is performed. At the time of writing this EIR/EIS, SMAQMD's most current and most detailed guidance for addressing construction-generated PM10 emissions is found in its Guide to Air Quality Assessment in Sacramento County (SMAQMD 2009a). The project-level analysis shall incorporate detailed parameters of the construction equipment and activities, including the year during which construction would be performed, as well as the proximity of potentially affected receptors, including receptors proposed by the project that exist at the time the construction activity would occur.</p>	Before the approval of all grading plans by the City.	The project applicant(s) responsible for construction of each off-site element in Sacramento County.	<ol style="list-style-type: none"> For all off-site improvements within Sacramento County: Sacramento County Planning and Community Development Department. For the U.S. 50 interchange improvements: Caltrans. 	
<p>Mitigation Measure 3A.2-1d: Implement SMAQMD's Basic Construction Emission Control Practices during Construction of all Off-site Elements located in Sacramento County.</p> <p>The applicants responsible for the construction of each off-site element in Sacramento County shall require their contractors to implement SMAQMD's Basic Construction Emission Control Practices during construction. A list of SMAQMD's Basic Construction Emission Control Practices is provided under Mitigation Measure 3A.2-1a.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be developed by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., Sacramento County or Caltrans) to implement SMAQMD's Basic Construction Emission Control Practices or comparable feasible measures.</p>	Before the approval of all grading plans from SMAQMD.	The project applicant(s) responsible for constructing the roadway connections in El Dorado County.	El Dorado County Development Services Department.	

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<p>Mitigation Measure 3A.2-1e: Implement EDCAQMD-Recommended Measures for Controlling Fugitive PM10 dust During Construction of the Two Roadway Connections in El Dorado County.</p> <p>Prior to construction of each roadway extension in El Dorado County, the applicants or its contractors shall develop a fugitive dust control plan that is approved by EDCAQMD and the applicants shall require their contractors to implement the dust control measures identified in the EDCAQMD-approved fugitive dust control plan. The fugitive dust control plan shall contain measures that are recommended by EDCAQMD at the time the plan is developed, which may include, but is not limited to, the current list of EDCAQMD-recommended dust control measures provided in Table 3A.2-5 below.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be developed by the project applicant(s) of each applicable project phase in consultation with the affected oversight agency(ies) (i.e., El Dorado County).</p>	<p>Before the approval of grading plans by EDCAQMD.</p>	<p>The project applicant(s) responsible for constructing the roadway connections in El Dorado County.</p>	<p>El Dorado County Development Services Department.</p>	
<p>Table 3A.2-5</p>				
<p>EDCAQMD-Recommend Fugitive Dust Control Measures</p>				
<p>Source</p>	<p>Mitigation Measure</p>			
<p>Soil Piles</p>	<p>Enclose, cover, or water twice daily all soil piles Automatic sprinkler system installed on soil piles</p>			
<p>Exposed Surface/Grading</p>	<p>Water all exposed soil twice daily Water exposed soil with adequate frequency to keep soil moist at all times</p>			
<p>Truck Hauling Road</p>	<p>Water all haul roads twice daily Pave all haul roads</p>			
<p>Truck Hauling Load</p>	<p>Maintain at least two feet of freeboard Cover load of all haul/dump trucks securely</p>			
<p>Source: Table 4.12 of EDCAQMD's Guide to Air Quality Assessment (EDCAQMD 2002).</p>				
<p>Mitigation Measure 3A.2-1f: Implement SMAQMD's Enhanced Exhaust Control Practices during Construction of all Off-site Elements.</p> <p>Implement SMAQMD's Enhanced Exhaust Control Practices, which are listed in Mitigation Measure 3A.2-1a, in order to control NOX emissions generated by</p>	<p>Before the approval of all grading plans from the respective air district (i.e., SMAQMD or EDCAQMD).</p>	<p>The project applicant(s) responsible for construction of each off-site</p>	<p>1. For the two roadway connections in El Dorado Hills: El Dorado County Development Services Department.</p>	

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<p>construction of all off-site elements (in Sacramento and El Dorado Counties, or Caltrans right-of-way).</p> <p>Mitigation Measure 3A.2-1g: Pay Off-site Mitigation Fee to SMAQMD to Off-Set NOX Emissions Generated by Construction of Off-site Elements.</p> <p>The off-site elements could result in construction-generated NOX emissions that exceed the SMAQMD threshold of significance, even after implementation of the SMAQMD Enhanced Exhaust Control Practices (listed in Mitigation Measure 3A.2-1a). Therefore, the responsible project applicant(s) for each off-site element in Sacramento County shall pay SMAQMD an off-site mitigation fee for implementation of each off-site element in Sacramento County for the purpose of reducing NOX emissions to a less-than-significant level (i.e., less than 85 lb/day). The specific fee amounts shall be calculated when the daily construction emissions can be more accurately determined. This calculation shall occur if the City/USACE certify the EIR/EIS and select and approves the Proposed Project or one of the other four other action alternatives, the City, Sacramento County, and the applicants establish the phasing by which construction of the off-site elements will occur, and the applicants develop a detailed construction schedule. Calculation of fees associated with each off-site element shall be conducted by the project applicant(s) in consultation with SMAQMD staff before the approval of the respective grading plans by Sacramento County. The project applicant(s) responsible for each off-site element in Sacramento County shall pay into SMAQMD's off-site construction mitigation fund to further mitigate construction-generated emissions of NOX that exceed SMAQMD's daily emission threshold of 85 lb/day. The calculation of daily NOX emissions shall be based on the cost rate established by SMAQMD at the time the calculation and payment are made. At the time of writing this EIR/EIS the cost rate is \$16,000 to reduce 1 ton of NOX plus a 5% administrative fee (SMAQMD 2008c). The determination of the final mitigation fee shall be conducted in coordination with SMAQMD before any</p>	<p>Before the approval of each grading plan for the off-site elements in Sacramento County.</p>	<p>The project applicant(s) of all off-site elements in Sacramento County.</p>	<p>2. For the detention basin west of Prairie City Road; Sacramento County Planning and Community Development Department. 3. For the U.S. 50 interchange improvements: Caltrans.</p>	<p>1. For all off-site improvements within Sacramento County: Sacramento County Planning and Community Development Department shall not grant any grading permits to the respective project applicant(s) until the respective project applicant(s) have paid the appropriate off-site mitigation fee to SMAQMD. 2. For the U.S. 50 interchange improvements: Caltrans shall not grant any grading permits to the respective project applicant(s) until the respective project applicant(s) have paid</p>

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<p>ground disturbance occurs for any project phase. Because the fee is based on the mass quantity of emissions that exceed SMAQMD's daily threshold of significance of 85 lb/day, total fees for construction of the off-site elements would vary according to the timing and potential overlap of construction schedules for off-site elements. This measure applies only to those off-site elements located in SMAQMD's jurisdiction (i.e., in Sacramento County) because EDCAQMD does not offer a similar off-set fee program for construction-generated NOX emissions in its jurisdiction. (This fee is used by SMAQMD to purchase off-site emissions reductions. Such purchases are made through SMAQMD's Heavy Duty Incentive Program, through which select owners of heavy-duty equipment in Sacramento County can repower or retrofit their old engines with cleaner engines or technologies.)</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be developed by the project applicant(s) of each applicable project phase in consultation with the affected oversight agency(ies) (i.e., Sacramento County or Caltrans).</p>	<p>1. For all off-site improvements within unincorporated Sacramento County: Before the approval of the respective grading plans from the Sacramento County Planning and Community Development Department</p> <p>2. For the U.S. 50 interchange improvements: Before the approval of construction plans from Caltrans</p>	<p>All detailed, project-level analysis shall be performed by the responsible lead agency or its selected consultant and funded by the project applicant(s). Implementation of the project-level modeling analysis and any necessary additional mitigation shall be fully funded by the project applicant(s)</p>	<p>the appropriate off-site mitigation fee to SMAQMD.</p>	
<p>Mitigation Measure 3A.2-1h: Analyze and Disclose Projected PM10 Emission Concentrations at Nearby Sensitive Receptors Resulting from Construction of Off-site Elements.</p> <p>Prior to construction of each off-site element located in Sacramento County that would involve site grading or earth disturbance activity that would exceed 15 acres in one day, the responsible agency or its selected consultant shall conduct detailed dispersion modeling of construction-generated PM10 emissions pursuant to SMAQMD guidance that is in place at the time the analysis is performed. At the time of writing this EIR/EIS, SMAQMD's most current and most detailed guidance for addressing construction-generated PM10 emissions is found in its Guide to Air Quality Assessment in Sacramento County SMAQMD 2009a). SMAQMD emphasizes that PM10 emission concentrations at nearby sensitive receptors be disclosed in project-level CEQA analysis. Each project-level analysis shall incorporate detailed parameters of the construction equipment and activities, including the year during which construction would be performed, as well as the proximity of potentially affected receptors, including receptors proposed by the project that exist at the time the construction activity would occur. If the modeling analysis determines that construction activity would result in an exceedance or substantial contribution to the CAAQS and NAAQS at a nearby receptor, then the project applicant(s) shall require their respective contractors to implement additional measures for controlling construction-generated PM10 exhaust</p>	<p>1. For all off-site improvements within Sacramento County: Before the approval of the respective grading plans from the Sacramento County Planning and Community Development Department</p> <p>2. For the U.S. 50 interchange improvements: Before the approval of construction plans from Caltrans</p>	<p>All detailed, project-level analysis shall be performed by the responsible lead agency or its selected consultant and funded by the project applicant(s). Implementation of the project-level modeling analysis and any necessary additional mitigation shall be fully funded by the project applicant(s)</p>	<p>1. For all off-site improvements within Sacramento County: Sacramento County Planning and Community Development Department.</p> <p>2. For the U.S. 50 interchange improvements: Caltrans.</p>	

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<p>emission and fugitive PM10 dust emissions in accordance with SMAQMD guidance, requirements, and/or rules that apply at the time the project-level analysis is performed. It is likely that these measures would be the same or similar to those listed as Enhanced Fugitive PM Dust Control Practices for Soil Disturbance Areas and Unpaved Roads and Enhanced Exhaust Control Practices included in Mitigation Measure 3A.2-1a. Dispersion modeling is not required for the two El Dorado County roadway connections because the total amount of disturbed acreage is expected to be less than the EDCAQMD screening level of 12 acres. Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be developed by the project applicant(s) of each applicable project phase in consultation with the affected oversight agency(ies) (i.e., Sacramento County or Caltrans).</p>		responsible for each off-site improvement.		
<p>Mitigation Measure 3A.2-2: Implement All Measures Prescribed by the Air Quality Mitigation Plan to Reduce Operational Air Pollutant Emissions. To reduce operational emissions, the project applicant(s) for any particular discretionary development application shall implement all measures prescribed in the SMAQMD-approved Folsom Plan Area Specific Plan Air Quality Mitigation Plan (AQMP) (Torrence Planning 2008), a copy of which is included in Appendix C2. The AQMP is intended to improve mobility, reduce vehicle miles traveled, and improve air quality as required by AB 32 and SB 375. The AQMP includes, among others, measures designed to provide bicycle parking at commercial land uses, an integrated pedestrian/bicycle path network, transit stops with shelters, a prohibition against the use of wood-burning fireplaces, energy star roofing materials, electric lawnmowers provided to homeowners at no charge, and on-site transportation alternatives to passenger vehicles (including light rail) that provide connectivity with other local and regional alternative transportation networks.</p>	Before issuance of subdivision maps or improvement plans.	The project applicant(s) any particular discretionary development application.	City of Folsom Community Development Department.	
<p>Mitigation Measure 3A.2-4a: Develop and Implement a Plan to Reduce Exposure of Sensitive Receptors to Construction-Generated Toxic Air Contaminant Emissions. The project applicant(s) for any particular discretionary development application shall develop a plan to reduce the exposure of sensitive receptors to TACs generated by project construction activity associated with buildout of the selected alternative. Each plan shall be developed by the project applicant(s) in consultation with SMAQMD. The plan shall be submitted to the City for review and approval before the approval of any grading plans. The plan may include such measures as scheduling activities when the residences are the least likely to be occupied, requiring equipment to be shut off when not in</p>	Before the approval of all grading plans by the City and throughout project construction, where applicable, for all project phases.	The project applicant(s) any particular discretionary development application.	City of Folsom Community Development Department.	

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<p>use, and prohibiting heavy trucks from idling. Applicable measures shall be included in all project plans and specifications for all project phases. The implementation and enforcement of all measures identified in each plan shall be funded by the project applicant(s) for the respective phase of development.</p> <p>Mitigation Measure 3A.2-4b: Implement Measures to Reduce Exposure of Sensitive Receptors to Operational Emissions of Toxic Air Contaminants. The following measures shall be implemented to reduce exposure of sensitive receptors to Toxic Air Contaminants.</p> <ul style="list-style-type: none"> • Proposed commercial and industrial land uses that have the potential to emit TACs or host TAC-generating activity (e.g., loading docks) shall be located away from existing and proposed on-site sensitive receptors such that they do not expose sensitive receptors to TAC emissions that exceed an incremental increase of 10 in 1 million for the cancer risk and/or a noncarcinogenic Hazard Index of 1.0. • The multi-family residences planned across from the off-site corporation yard near the southwest corner of the SPA shall be set back as far as possible from the boundary of the corporation yard and/or relocated to another area. • Where necessary to reduce exposure of sensitive receptors to an incremental increase of 10 in 1 million for the cancer risk and/or a noncarcinogenic Hazard Index of 1.0, proposed commercial and industrial land uses that would host diesel trucks shall incorporate idle reduction strategies that reduce the main propulsion engine idling time through alternative technologies such as, IdleAire, electrification of truck parking, and alternative energy sources for TRUs, to allow diesel engines to be completely turned off. • Signs shall be posted in at all loading docks and truck loading areas which indicate that diesel-powered delivery trucks must be shut off when not in use for longer than 5 minutes on the premises in order to reduce idling emissions. This measure is consistent with the ATCM to Limit Diesel-Fueled Commercial Motor Vehicle Idling, which was approved by the California Office of Administrative Law in January 2005. • Implement the following additional guidelines, which are recommended in ARB's Land Use Handbook: A Community Health Perspective (ARB 2005) and are considered to be advisory and not regulatory: <ul style="list-style-type: none"> o Sensitive receptors, such as residential units and daycare centers, shall not be located in the same building as dry-cleaning operations that use perchloroethylene. Dry-cleaning operations that use perchloroethylene shall 	<p>Before the approval of all grading plans by the SMAQMD and throughout project construction, where applicable, for all project phases.</p>	<p>The project applicant(s) of all project phases.</p>	<p>City of Folsom Community Development Department.</p>	

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<p>not be located within 300 feet of any sensitive receptor. A setback of 500 feet shall be provided for operations with two or more machines.</p> <ul style="list-style-type: none"> Large gasoline stations (defined as facilities with a throughput of 3.6 million gallons per year or greater) and sensitive land uses shall not be sited within 300 feet of each other. Small gasoline-dispensing facilities (less than 3.6 million gallons of throughput per year) and sensitive land uses shall not be sited within 50 feet of each other. 				
<p>Mitigation Measure 3A.2-5: Implement A Site Investigation to Determine the Presence of NOA and, if necessary, Prepare and Implement an Asbestos Dust Control Plan.</p> <p>A site investigation shall be performed to determine whether and where NOA is present in the soil and rock on the SPA. The site investigation shall include the collection of soil and rock samples by a qualified geologist. If the site investigation determines that NOA is present on the SPA then the project applicant shall prepare an Asbestos Dust Control Plan for approval by SMAQMD as required in Title 17, Section 93105 of the California Code of Regulations, "Asbestos Airborne Toxic Control Measure for Construction, Grading, Quarrying, and Surface Mining Operations." The Asbestos Dust Control Plan shall specify measures, such as periodic watering to reduce airborne dust and ceasing construction during high winds. Measures in the Asbestos Dust Control Plan may include but shall not be limited to dust control measures required by Mitigation Measure 3A.2-1a. The project applicant shall submit the plan to the Folsom Community Development Department for review and approval before construction of the first project phase. SMAQMD approval of the plan must be received before any asbestos-containing rock (serpentine) can be disturbed. Upon approval of the Asbestos Dust Control Plan by SMAQMD, the applicant shall ensure that construction contractors implement the terms of the plan throughout the construction period.</p>	<p>Before the approval of all grading plans by the City and throughout project construction, where applicable, for all project phases.</p>	<p>The project applicant(s) of all project phases.</p>	<p>City of Folsom Community Development Department.</p>	
<p>Mitigation Measure 3A.2-6: Implement Measures to Control Exposure of Sensitive Receptors to Operational Odorous Emissions.</p> <p>The project applicant(s) for any particular discretionary development application shall implement the following measures:</p> <ul style="list-style-type: none"> The odor-producing potential of land uses shall be considered when the exact type of facility that would occupy areas zoned for commercial, industrial, or mixed-use land uses is determined. Facilities that have the potential to emit objectionable odors shall be located as far away as feasible from existing and proposed sensitive receptors. 	<p>Before the approval of building permits by the City and throughout project construction, where applicable, for all project phases.</p>	<p>The project applicant(s) of all project phases.</p>	<p>City of Folsom Community Development Department.</p>	

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<ul style="list-style-type: none"> • The multi-family residences planned across from the off-site corporation yard near the southwest corner of the SPA shall be set back as far as possible from the boundary of the corporation yard and/or relocated to another area. (This measure is also required by Mitigation Measure 3A.2-4b to limit exposure to TAC emissions.) • Before the approval of building permits, odor control devices shall be identified to mitigate the exposure of receptors to objectionable odors if a potential odor-producing source is to occupy an area zoned for commercial, industrial, or mixed-use land uses. The identified odor control devices shall be installed before the issuance of certificates of occupancy for the potentially odor-producing use. The odor-producing potential of a source and control devices shall be determined in coordination with SMAQMD and based on the number of complaints associated with existing sources of the same nature. • The deeds to all properties located within the plan area that are within one mile of an on- or off-site area zoned or used for agricultural use (including livestock grazing) shall be accompanied by a written disclosure from the transferor, in a form approved by the City of Folsom, advising any transferee of the potential adverse odor impacts from surrounding agricultural operations, which disclosure shall direct the transferee to contact the County of Sacramento concerning any such property within the County zoned for agricultural uses within one mile of the subject property being transferred. • Truck loading docks and delivery areas shall be located as far away as feasible from existing and proposed sensitive receptors. • Signs shall be posted at all loading docks and truck loading areas which indicate that diesel-powered delivery trucks must be shut off when not in use for longer than 5 minutes on the premises in order to reduce idling emissions. This measure is consistent with the ATCM to Limit Diesel-Fueled Commercial Motor Vehicle Idling, which was approved by California's Office of Administrative Law in January 2005. (This measure is also required by Mitigation Measure 3A.2-4b to limit TAC emissions.) • Proposed commercial and industrial land uses that have the potential to host diesel trucks shall incorporate idle reduction strategies that reduce the main propulsion engine idling time through alternative technologies such as, IdleAire, electrification of truck parking, and alternative energy sources for TRUs, to allow diesel engines to be completely turned off. (This measure is also required by Mitigation Measure 3A.2-4b to limit TAC emissions.) 				

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<p>Mitigation Measure 3A.3-1a: Mitigation for erosion impacts.*</p> <p>To minimize indirect effects on water quality and wetland hydrology, the project applicant shall include a storm water drainage plan and an erosion and sediment control plan in the improvement plans and shall submit these plans to the City Public Works Department for review and approval. Before approval of these improvement plans, the project applicant shall obtain a National Pollutant Discharge Elimination System MS4 Municipal Stormwater Permit and Grading Permit, comply with the City's Grading Ordinance and County drainage and storm water quality standards, and commit to implementing all measures in their drainage plans and erosion and sediment control plans to avoid and minimize erosion and runoff into Alder Creek and all wetlands and other waters that would remain within the FPASP area.</p> <p>The project applicant shall implement storm water quality treatment controls consistent with the Storm Water Quality Design Manual for Sacramento and South Placer Regions (Sacramento Stormwater Quality-Control Partnership 2007). Appropriate runoff controls such as berms, storm gates, off-stream detention basins, overflow collection areas, filtration systems, and sediment traps shall be implemented to control siltation and the potential discharge of pollutants. Development plans shall incorporate low impact development (LID) features, such as pervious strips, permeable pavements, bioretention ponds, vegetated swales, disconnected rain gutter downspouts, and rain gardens, where appropriate. Use of LID features is recommended by the Environmental Protection Agency (EPA) to minimize impacts on water quality, hydrology, and stream geomorphology. Crossings of wetlands shall be done in accordance with the Section 404 permits which allow for free-spanning bridge systems, the use of bottomless culverts that do not alter the natural stream bed, and/or oversized box culverts that are backfilled with a natural substrate. Consistent with the USACE permits, where installation of box culverts is planned, restoration of a natural streambed/substrate shall be required. Details of all crossings shall be submitted to the USACE for approval prior to each phase of development.</p> <p>In addition to complying with City ordinances, the project applicant shall obtain a General Construction Storm Water Permit from the Central Valley Regional Water Quality Control Board (RWQCB), prepare a storm water pollution prevention plan (SWPPP), and implement best management practices (BMPs) to reduce water quality effects during construction.</p>	<p>Before approval of improvement and drainage plans, and on an ongoing basis throughout and after project construction, as required for all project phases.</p>	<p>Project applicant(s) of all project phases and on-site and off-site elements.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Public Works Department. 2. For the two roadway connections in El Dorado Hills: El Dorado County Development Services Department. 3. For the detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department. 4. For the U.S. 50 interchange improvements: Caltrans. 5. U.S. Army Corps of Engineers, Sacramento District. 6. Central Valley Regional Water Quality Control Board.</p>	

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<p>Each project phase shall result in no net change to peak flows into Alder Creek and associated tributaries, or to tributaries to Buffalo Creek, and Coyote Creek. The project applicant shall establish a baseline of conditions for drainage on-site. The baseline-flow conditions shall be established for 2-, 5-, 10-, and 20-year storm events. These baseline conditions shall be used to develop monitoring standards for the storm water system within the project area. The baseline conditions, monitoring standards, and a monitoring program shall be submitted to the U.S. Army Corps of Engineers (USACE) and the City for their approval. Water quality and detention basins shall be designed and constructed to ensure that the performance standards are met and shall be designed as off-stream detention basins. Discharge sites into Alder Creek and associated tributaries, as well as tributaries to Coyote Creek, and Buffalo Creek, shall be monitored to ensure that pre-project conditions are being met. Corrective measures shall be implemented as necessary. The mitigation measures will be satisfied when the monitoring standards are met for five consecutive years without undertaking corrective measures to meet the performance standard.</p> <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p> <p>Mitigation Measure 3A.3-1b: Implement Clean Water Act Section 404 Permits and Section 401 Water Quality Certifications.* Before the approval of grading and improvement plans and before any groundbreaking activity associated with each distinct project phase, the owner/applicant shall secure all USACE necessary permits obtained under Sections 401 and 404 of the Clean Water Act or the State's Porter-Cologne Act and implement all permit conditions for the proposed Central Valley project. All permits, regulatory approvals, and permit conditions for effects on wetland habitats shall be secured and conditions implemented before implementation of any grading activities within 250 feet (or lesser distance as approved by the applicable agencies) of waters of the U.S., or wetland habitats, including waters of the State, that potentially support federally listed species, or within 100 feet (or lesser distance as approved by the applicable agencies) of other waters of the U.S. or wetland habitats, including waters of the State. The owner/applicant shall adhere to all conditions outlined in the permits. The owner/applicant shall commit to replace, restore, or enhance on a "Uno net loss" basis (in accordance with USACE and the Central Valley Regional Water Quality Control Board) the acreage of all wetlands and other Waters of the U.S. that would be removed, lost, and/or</p>	<p>Before the approval of grading or improvement plans or any ground-disturbing activities for any project development phase containing wetland features or other waters of the U.S.. The MMP must be approved before any impact on wetlands can occur. Mitigation shall be implemented on an ongoing basis throughout and after</p>	<p>Project applicant(s) for each discretionary development entitlement requiring fill of wetlands or other waters of the U.S. or waters of the state.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For the two roadway connections in El Dorado Hills: El Dorado County Development Services Department. 3. For the detention basin west of Prairie City Road: Sacramento County Planning and Community</p>	

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<p>degraded with implementation of the project. Wetland habitat shall be restored, enhanced, and/or replaced at an acreage and location and by methods agreeable to USACE, the Central Valley RWQCB, and the City, as appropriate, depending on agency jurisdiction, and as determined during the Section 401 and Section 404 permitting processes. The boundaries of the 404 permit, including required buffer, shall be shown on the grading plans.</p> <p>All mitigation requirements to satisfy the requirements of the City and the Central Valley RWQCB, for impacts on the non-jurisdictional wetlands beyond the jurisdiction of USACE, shall be determined and implemented before grading plans are approved.</p> <p>All wetland mitigation compliance reports submitted to USACE shall also be copied concurrently to the City.</p> <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p>	<p>construction, as required.</p>		<p>Development Department. 4. For the U.S. 50 interchange improvements: Caltrans. 5. U.S. Army Corps of Engineers, Sacramento District; Central Valley Regional Water Quality Control Board as appropriate depending on agency jurisdiction, and as determined during the Section 401 and Section 404 permitting processes and in compliance with the City's Grading Ordinance (Folsom Municipal Code 14.29), or appropriate county grading ordinance for off-site detention basin and roadway connections from Folsom Heights to El Dorado Hills.</p>	
<p>Mitigation Measure 3A.3-2a: Avoid Direct Loss of Swainson's Hawk and Other Raptor Nests.*</p> <p>*The actions required by this Mitigation Measure were broken into two separate Mitigation Measures (4.4-4 and 4.4-5) in the W/E SPA Addendum to FPASP EIR/EIS (see below).</p>	<p>Before the approval of grading and improvement plans, before any ground-disturbing activities, and during project construction as applicable for all project phases.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. California Department of Fish and Game. 2. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community</p>	

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<p>Mitigation Measure 3A.3-2b. Prepare and Implement a Swainson's Hawk Mitigation Plan.</p> <p>To mitigate for the loss of Swainson's hawk foraging habitat, the project applicant(s) of all project phases shall prepare and implement a Swainson's hawk mitigation plan including, but not limited to the requirements described below. Before the approval of grading and improvement plans or before any ground-disturbing activities, whichever occurs first, the project applicant(s) shall preserve, to the satisfaction of the City or Sacramento County, as appropriate depending on agency jurisdiction, suitable Swainson's hawk foraging habitat to ensure 1:1 mitigation of habitat value for Swainson's hawk foraging habitat lost as a result of the project, as determined by the City, or Sacramento County, after consultation with DFG and a qualified biologist.</p> <p>The 1:1 habitat value shall be based on Swainson's hawk nesting distribution and an assessment of habitat quality, availability, and use within the City's planning area, or Sacramento County jurisdiction. The mitigation ratio shall be consistent with the 1994 DFG Swainson's Hawk Guidelines included in the Staff Report Regarding Mitigation for Impacts to Swainson's Hawks (Buteo swainsoni) in the Central Valley of California, which call for the following mitigation ratios for loss of foraging habitat in these categories: 1:1 if within 1 mile of an active nest site,</p>	<p>Before the approval of grading, improvement, or construction plans and before any ground-disturbing activity in any project development phase that would affect Swainson's hawk foraging habitat.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For the detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department.</p>	<p>Development Department. 3. For the two roadway connections in El Dorado Hills: El Dorado County Development Services Department. 4. For the U.S. 50 interchange improvements: Caltrans. 5. For the detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department.</p>

Table 1: Mitigation Monitoring and Reporting Plan for the Westland/Eagle Area of the Folsom Plan Area Specific Plan
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<p>0.75:1 if over 1 mile but less than 5 miles, and 0.5:1 if over 5 miles but less than 10 miles from an active nest site. Such mitigation shall be accomplished through credit purchase from an established mitigation bank approved to sell Swainson's hawk foraging habitat credits to mitigate losses in the SPA, if available, or through the transfer of fee title or perpetual conservation easement. The mitigation land shall be located within the known foraging area and within Sacramento County. The City, or Sacramento County if outside City jurisdiction, after consultation with DFG, will determine the appropriateness of the mitigation land.</p> <p>Before approval of such proposed mitigation, the City, or Sacramento County for the off-site detention basin, shall consult with DFG regarding the appropriateness of the mitigation. If mitigation is accomplished through conservation easement, then such an easement shall ensure the continued management of the land to maintain Swainson's hawk foraging values, including but not limited to ongoing agricultural uses and the maintenance of all existing water rights associated with the land. The conservation easement shall be recordable and shall prohibit any activity that substantially impairs or diminishes the land's capacity as suitable Swainson's hawk habitat.</p> <p>The project applicant(s) shall transfer said Swainson's hawk mitigation land, through either conservation easement or fee title, to a third-party, nonprofit conservation organization (Conservation Operator), with the City and DFG named as third-party beneficiaries. The Conservation Operator shall be a qualified conservation easement land manager that manages land as its primary function. Additionally, the Conservation Operator shall be a tax-exempt nonprofit conservation organization that meets the criteria of Civil Code Section 815.3(a) and shall be selected or approved by the City or County, after consultation with DFG. The City, or County, after consultation with DFG and the Conservation Operator, shall approve the content and form of the conservation easement. The City, or County, DFG, and the Conservation Operator shall each have the power to enforce the terms of the conservation easement. The Conservation Operator shall monitor the easement in perpetuity to assure compliance with the terms of the easement.</p> <p>The project applicant(s), after consultation with the City, or County of jurisdiction, DFG, and the Conservation Operator, shall establish an endowment or some other financial mechanism that is sufficient to fund in perpetuity the operation, maintenance, management, and enforcement of the conservation easement. If an endowment is used, either the endowment funds shall be submitted to the City for impacts on lands within the City's jurisdiction or Sacramento County for the off-site detention basin to be distributed to an appropriate third-party nonprofit</p>				

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<p>conservation agency, or they shall be submitted directly to the third-party nonprofit conservation agency in exchange for an agreement to manage and maintain the lands in perpetuity. The Conservation Operator shall not sell, lease, or transfer any interest in any conservation easement or mitigation land it acquires without prior written approval of the City and DFG. Mitigation lands established or acquired for impacts incurred at the off-site detention basin shall require approval from Sacramento County prior to sale or transfer of mitigation lands or conservation easement.</p> <p>If the Conservation Operator ceases to exist, the duty to hold, administer, manage, maintain, and enforce the interest shall be transferred to another entity acceptable to the City and DFG, or Sacramento County and DFG depending on jurisdiction of the affected habitat. The City Planning Department shall ensure that mitigation habitat established for impacts on habitat within the City's planning area is properly established and is functioning as habitat by reviewing regular monitoring reports prepared by the Conservation Operator of the mitigation site(s). Monitoring of the mitigation site(s) shall continue for the first 10 years after establishment of the easement and shall be funded through the endowment, or other appropriate funding mechanism, established by the project applicant(s). Sacramento County shall review the monitoring reports for impacts on habitat at the off-site detention basin.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., Sacramento County and Caltrans).</p>				
<p>Mitigation Measure 3A-3-2c: Conduct preconstruction Tricolored Blackbird Nesting surveys.*</p> <p>To avoid and minimize impacts to tricolored blackbird colonies, a qualified biologist shall conduct a preconstruction survey for any project activity that would occur during the tricolored blackbird's nesting season (1 March - 31 August). The preconstruction survey shall be conducted within 500 feet of potential on-site suitable nesting habitat, including freshwater marsh and areas of riparian scrub vegetation, within the ponds and Alder Creek. The survey shall be conducted within 14 days before project activity begins.</p> <p>If no tricolored blackbird nesting activity is documented on-site, no further mitigation is required. If nesting activity is found, the qualified biologist shall consult CDFW to establish a buffer around the nesting colony. No project activity shall commence within the buffer area until a qualified biologist confirms that the</p>	<p>Before the approval of any ground-disturbing activity within 500 feet of suitable nesting habitat as applicable for all project phases.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For the U.S. 50 interchange improvements: Caltrans.</p>	

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<p>colony is no longer active. The size of the buffer shall be determined in consultation with CDFW. Buffer size is anticipated to range from 100 to 500 feet, depending on the nature of the project activity, the extent of existing disturbance in the area, and other relevant circumstances.</p> <p>If required by CDFW, the project applicant shall initiate incidental take permit process according to Section 2081 (b) and (c) of the California Fish and Game Code and shall prepare a mitigation plan as an attachment to the 2081 permit.</p> <p>Avoidance and minimization measures may include protective fencing around sensitive habitat within construction sites, preconstruction notification to CDFW, scientific reporting procedures when an animal is killed, injured, or trapped, compliance inspections and reports, directions for the acquisition and transfer of habitat management lands, and/or associated funding.</p> <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p>				
<p>Mitigation Measure 3A.3-2d: Preconstruction bat roosting survey.*</p> <p>Before construction in any given phase, a pre-construction bat roost survey shall be conducted. A qualified biologist shall conduct a dusk emergence survey (start one hour before sunset and last three hours), followed by a pre-dawn re-entry survey (start one hour before sunrise and last for two hours), as well as a daytime visual inspection of all potential bat roosting habitat within the limits of construction. If no active bat roosts or sign are observed, construction may proceed. If no active special-species bat roosts are found, no further measures pertaining to special-species bats are necessary. If roosting special-species bats are found on-site during the surveys, construction activities shall avoid direct and indirect impacts to roosting sites through the establishment of a no-disturbance buffer of 100 feet around roost sites in consultation with CDFW. Clearing and grubbing adjacent to the roost site and lighting use near the roost site where it would shine on the roost or interfere with bats entering or leaving the roost shall be prohibited. Operation of internal combustion equipment, such as generators, pumps, and vehicles within 100 feet of the roost site shall be prohibited.</p> <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p>	<p>Before the approval of removal or fill of the mine shaft on the SPA.</p>	<p>Project applicant(s) of all project phases containing potential bat roosting habitat.</p>	<p>City of Folsom Community Development Department.</p>	
<p>Mitigation Measure 3A.3-2e: Obtain an Incidental Take Permit under Section 10(a) of ESA; Develop and Implement a Habitat Conservation Plan to Compensate for the Loss of Vernal Pool Habitat.</p>	<p>Before the approval of any grading or improvement plans,</p>	<p>Project applicant(s) of all project</p>	<p>1. U.S. Fish and Wildlife Service.</p>	

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Mitigation Measure	Timing	Implementation	Enforcement	Compliance/Notes
<p>The project applicant(s) for all project phases shall obtain an incidental take permit under Section 10(a) of ESA. No project construction shall proceed in areas supporting potential habitat for Federally listed vernal pool invertebrates, or within adequate buffer areas (250 feet or lesser distance deemed sufficiently protective by a qualified biologist with approval from USFWS), until a BO has been issued by USFWS and the project applicant(s) have abided by conditions in the BO (including all conservation and minimization measures). Conservation and minimization measures are likely to include preparation of supporting documentation describing methods to protect existing vernal pools during and after project construction.</p> <p>Under the No Federal Action Alternative, interagency consultation under Section 7 of ESA would not occur; therefore, the project applicant(s) would be required to develop a habitat conservation plan to mitigate impacts on Federally listed vernal pool invertebrates. The project applicant(s) shall complete and implement, or participate in, a habitat conservation plan that shall compensate for the loss of acreage, function, and value of affected vernal pool habitat. The habitat conservation plan shall be consistent with the goals of the Recovery Plan for Vernal Pool Ecosystems of California and Southern Oregon (USFWS 2005) and must be approved by USFWS.</p> <p>The project applicant(s) for all project phases shall ensure that there is sufficient upland habitat within the target areas for creation and restoration of vernal pools and vernal pool complexes to provide ecosystem health. The land used to satisfy this mitigation measure shall be protected through a fee title or conservation easement acceptable to the City and USFWS.</p> <p>The project applicant(s) for all project phases shall identify the extent of indirectly affected vernal pool and seasonal wetland habitat, either by identifying all such habitat within 250 feet of project construction activities or by providing an alternative technical evaluation in support of a lesser indirect impact distance. If a lesser distance is pursued, this distance shall be approved by USFWS. The project applicant(s) shall preserve 2 wetted acres of vernal pool habitat for each wetted acre of any indirectly affected vernal pool habitat. This mitigation shall occur before the approval of any grading or improvement plans for any project phase that would allow work within 250 feet of such habitat, and before any ground-disturbing activity within 250 feet of the habitat. The project applicant(s) will not be required to complete this mitigation measure for direct or indirect impacts that have already been mitigated to the satisfaction of USFWS through another BO or mitigation plan.</p>	<p>before any ground-disturbing activities within 250 feet of said habitat, and on an ongoing basis throughout construction as applicable for all project phases as required by the habitat conservation plan and/or BO.</p>	<p>phases and on-site and off-site elements.</p>	<p>2. For all project-related improvements that would be located within the City of Folsom Folsom: City of Folsom Community Development Department. 3. For the two roadway connections in El Dorado Hills: El Dorado County Development Services Department. 4. For the detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department. 5. For the U.S. 50 interchange improvements: Caltrans.</p>	

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<p>A standard set of BMPs shall be applied to construction occurring in areas within 250 feet of off-site vernal pool habitat, or within any lesser distance deemed adequate by a qualified biologist (with approval from USFWS) to constitute a sufficient buffer from such habitat. Refer to Section 3A.9, "Hydrology and Water Quality - Land" for the details of BMPs to be implemented.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties or Caltrans).</p>				
<p>Mitigation Measure 3A.3-2f: Obtain an Incidental Take Permit under Section 10(a) of ESA; Develop and Implement a Habitat Conservation Plan to Compensate for the Loss of VETLB Habitat.</p> <p>As long as valley elderberry longhorn beetle remains a species protected under ESA, the project applicant(s) of all project phases containing elderberry shrubs shall obtain an incidental take permit under Section 10(a) of ESA for valley elderberry longhorn beetle. No project construction shall proceed in areas potentially containing valley elderberry longhorn beetle until a BO has been issued by USFWS, and the project applicant(s) for all project phases have abided by all pertinent conditions in the take permit relating to the proposed construction, including all conservation and minimization measures. Conservation and minimization measures are likely to include preparation of supporting documentation that describes methods for relocation of existing shrubs and maintaining existing shrubs and other vegetation in a conservation area. Under the No Federal Action Alternative, interagency consultation under Section 7 of ESA would not occur; therefore, the project applicant(s) would be required to develop a habitat conservation plan to mitigate impacts on valley elderberry longhorn beetle. The project applicant(s) shall complete and implement a habitat conservation plan that will compensate for the loss of valley elderberry longhorn beetle. Relocation of existing elderberry shrubs and planting of new elderberry seedlings shall be implemented on a no-net-loss basis. Detailed information on monitoring success of relocated and planted shrubs and measures to compensate (should success criteria not be met) would also likely be required in the BO. Ratios for mitigation of valley elderberry longhorn beetle habitat will ultimately be determined through the ESA Section 10(a) consultation process with USFWS, but shall be a minimum of "no net loss."</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries (i.e., U.S. 50 interchange improvements) must be coordinated by the</p>	<p>Before the approval of any grading or improvement plans or any ground-disturbing activity within 100 feet of valley elderberry longhorn beetle habitat as applicable for all project phases, and on an ongoing basis as required by the habitat conservation plan and/or BO.</p>	<p>Project applicant(s) of all project phases potentially containing elderberry shrubs.</p>	<p>1. U.S. Fish and Wildlife Service 2. City of Folsom Community Development Department. 3. For the U.S. 50 interchange improvements: Caltrans.</p>	

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<p>project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., Caltrans).</p> <p>Mitigation Measure 3A.3-2g: Secure Take Authorization for Federally Listed Vernal Pool Invertebrates and Implement All Permit Conditions. No project construction shall proceed in areas supporting potential habitat for Federally listed vernal pool invertebrates, or within adequate buffer areas (250 feet or lesser distance deemed sufficiently protective by a qualified biologist with approval from USFWS), until a biological opinion (BO) or Not Likely to Adversely Affect (NLAA) letter has been issued by USFWS and the project applicant(s) for any particular discretionary development entitlements affecting such areas have abided by conditions in the BO (including conservation and minimization measures) intended to be completed before on-site construction. Conservation and minimization measures shall include preparation of supporting documentation describing methods to protect existing vernal pools during and after project construction, a detailed monitoring plan, and reporting requirements. As described under Mitigation Measure 3A.3-1a, an MMP shall be developed that describes details how loss of vernal pool and other wetland habitats shall be offset, including details on creation of habitat, account for the temporal loss of habitat, contain performance standards to ensure success, and outline remedial actions if performance standards are not met. The project applicant(s) for any particular discretionary development application potentially affecting vernal pool habitat shall complete and implement a habitat MMP that will result in no net loss of acreage, function, and value of affected vernal pool habitat. The final habitat MMP shall be consistent with guidance provided in Programmatic Formal Endangered Species Act Consultation on Issuance of 404 Permits for Projects with Relatively Small Effects on Listed Vernal Pool Crustaceans within the Jurisdiction of the Sacramento Field Office, California (USFWS 1996) or shall provide an alternative approach that is acceptable to the City, USACE, and USFWS and accomplishes no net loss of habitat acreage, function, and value. The project applicant(s) for any particular discretionary development application "potentially affecting vernal pool habitat" shall ensure that there is sufficient upland habitat within the target areas for creation and restoration of vernal pools and vernal pool complexes to provide ecosystem health. This standard shall be accomplished by requiring the project applicant(s) for any discretionary development application affecting vernal pool or seasonal wetland habitat to</p>	<p>Before the approval of any grading or improvement plans, before any ground-disturbing activities within 250 feet of said habitat or lesser distance deemed sufficiently protective by a qualified biologist with approval from USFWS, and on an ongoing basis throughout construction as applicable for all project phases as required by the mitigation plan, BO, and/or BMPs.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. U.S. Army Corps of Engineers, Sacramento District; U.S. Fish and Wildlife Service. 2. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 3. For the two roadway connections in El Dorado Hills: El Dorado County Development Services Department. 4. For the U.S. 50 interchange improvements: Caltrans. 5. For the detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department.</p>	

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<p>identify the extent of indirectly affected vernal pool and seasonal wetland habitat, either by identifying all such habitat within 250 feet of project construction activities or by providing an alternative technical evaluation. If a lesser distance is pursued, this distance shall be approved by USFWS. The project applicant(s) shall preserve acreage of vernal pool habitat for each wetted acre of any indirectly affected vernal pool habitat at a ratio approved by USFWS at the conclusion of the Section 7 consultation. This mitigation shall occur before the approval of any grading or improvement plans for any project phase that would allow work within 250 feet of such habitat or lesser distance deemed sufficiently protective by a qualified biologist with approval from USFWS, and before any ground-disturbing activity within 250 feet of the habitat or lesser distance deemed sufficiently protective by a qualified biologist with approval from USFWS. The project applicant(s) will not be required to complete this mitigation measure for direct or indirect impacts that have already been mitigated to the satisfaction of USFWS through another BO or mitigation plan (i.e., if impacts on specific habitat acreage are mitigated by one project phase or element, the project applicant(s) will not be required to mitigate for it again in another phase of the project).</p> <p>A standard set of BMPs shall be applied to construction occurring in areas within 250 feet of off-site vernal pool habitat, or within any lesser distance deemed adequate by a qualified biologist (with approval from USFWS) to constitute a sufficient buffer from such habitat. Refer to Section 3A.9, "Hydrology and Water Quality - Land" for the details of BMPs to be implemented.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be developed by the project applicant(s) of each applicable project phase in consultation with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties, or Caltrans).</p> <p>Mitigation Measure 3A.3-2h: Valley Elderberry Longhorn Beetle avoidance and minimization measures.*</p> <p>The applicant shall comply with all applicable conservation measures as detailed in the Conservation Guidelines for the Valley Elderberry Longhorn Beetle for transplantation of the one elderberry shrub (USFWS 1999).</p> <p>The applicant shall comply with all requirements of the Biological Opinion issued by USFWS for the FPASP (USFWS 2014) before construction can commence within 100 feet of elderberry shrubs.</p> <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p>	<p>Before the approval of any grading or improvement plans or any ground-disturbing activity within 100 feet of valley elderberry longhorn beetle habitat as applicable for all project phases, and</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. U.S. Army Corps of Engineers, Sacramento District; U.S. Fish and Wildlife Service. 2. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community</p>	

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<p>Mitigation Measure 3A.3-3: Conduct Special-Status Plant Surveys; Implement Avoidance and Mitigation Measures or Compensatory Mitigation.</p> <p>To mitigate for the potential loss or degradation of special-status plant species and habitat, the project applicant(s) for any particular discretionary development application shall adhere to the requirements described below.</p> <ul style="list-style-type: none"> The project applicant(s) for any particular discretionary development application, including the proposed off-site elements, shall retain a qualified botanist to conduct protocol level preconstruction special-status plant surveys for all potentially occurring species. Preconstruction special-status plant surveys shall not be required for those portions of the SPA that have already been surveyed according to DFG and USFWS guidelines. If no special-status plants are found during focused surveys, the botanist shall document the findings in a letter report to USFWS, DFG, the City of Folsom, Caltrans (for interchange improvements to U.S. 50), El Dorado County (for roadway connections in El Dorado County), and Sacramento County (for the off-site detention basin) and no further mitigation shall be required. If special-status plant populations are found, the project applicant(s) of affected developments shall consult with DFG and USFWS, as appropriate depending on species status, to determine the appropriate mitigation measures for direct and indirect impacts on any special-status plant population that could occur as a result of project implementation. Mitigation measures may include preserving and enhancing existing populations, creation of off-site populations on project mitigation sites through seed collection or transplantation, and/or restoring or creating suitable habitat in sufficient quantities to achieve no net loss of occupied habitat or individuals. If potential impacts on special-status plant species are likely, a mitigation and monitoring plan shall be developed before the approval of grading plans or any ground-breaking activity within 250 feet of a special-status plant population. The mitigation plan shall be submitted to Caltrans (for interchange improvements to U.S. 50), El Dorado County (for impacts in roadway connections in El Dorado County), Sacramento County (for impacts in the off- 	<p>on an ongoing basis as required by BO.</p> <p>Before approval of grading or improvement plans or any ground disturbing activities, including grubbing or clearing, for any project phase, including off-site elements.</p>	<p>Project applicant(s) of all project phases and on-site elements.</p>	<p>Development Department. 3. For the U.S. 50 interchange improvements: Caltrans.</p> <p>1. U.S. Fish and Wildlife Service, California Department of Fish and Game. 2. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 3. For the two roadway connections in El Dorado Hills: El Dorado County Development Services Department. 4. For the detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department. 5. For the U.S. 50 interchange improvements: Caltrans.</p>	

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<p>site detention basin footprint), or the City of Folsom (for on-site impacts and all other off-site elements), for review and approval. It shall be submitted concurrently to DFG or USFWS, as appropriate depending on species status, for review and comment. The plan shall require maintaining viable plant populations on-site and shall identify avoidance measures for any existing populations to be retained and compensatory measures for any populations directly affected. Possible avoidance measures include fencing populations before construction and exclusion of project activities from the fenced-off areas, and construction monitoring by a qualified botanist to keep construction crews away from the population. The mitigation plan shall also include monitoring and reporting requirements for populations to be preserved on site or protected or enhanced off site.</p> <ul style="list-style-type: none"> • If relocation efforts are part of the mitigation plan, the plan shall include details on the methods to be used, including collection, storage, propagation, receptor site preparation, installation, long-term protection and management, monitoring and reporting requirements, and remedial action responsibilities should the initial effort fail to meet long-term monitoring requirements. • If off-site mitigation includes dedication of conservation easements, purchase of mitigation credits or other off-site conservation measures, the details of these measures shall be included in the mitigation plan, including information on responsible parties for long-term management, conservation easement holders, long-term management requirements, and other details, as appropriate to target the preservation on long term viable populations. <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., Caltrans, El Dorado and/or Sacramento Counties).</p>	<p>Before the approval of grading or improvement plans or any construction activities (including clearing and grubbing) that affect the bed and bank or riparian and freshwater marsh habitat associated</p>	<p>Project applicant(s) of all project phases and the off-site Prairie City Road and Oak Avenue interchange improvements.</p>	<p>1. California Department of Fish and Game, 2. City of Folsom Community Development Department. 3. Caltrans for interchange</p>	
<p>Mitigation Measure 3A.3-4a: Implement Section 1602 Streambed Alteration Agreement. *</p> <p>The owner/applicant shall amend, if necessary, and implement the original Section 1602 Master Streambed Alteration Agreement received from CDFW for all construction activities that would occur in the bed and bank of CDFW jurisdictional features within the project and Wildlife site. As outlined in the Master Streambed Alteration Agreement, the owner/applicant shall submit a Sub-notification Form (SNF) to CDFW 60 days prior to grading and/or the commencement of construction to notify California Department of Fish and Wildlife of the project.</p>				

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<p>Any conditions of issuance of the Master Streambed Alteration Agreement shall be implemented as part of those project construction activities that would adversely affect the bed and bank within on-site drainage channels subject to CDFW jurisdiction. The agreement shall be executed by the owner/applicant and CDFW before the approval of any grading or improvement plans or any construction activities in any project phase that could potentially affect the bed and bank of on-site drainage channels under CDFW jurisdiction.</p> <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p>	<p>with Alder Creek and other on-site or off-site drainage channels and ponds.</p>		<p>Improvements to U.S. 50.</p>	
<p>Mitigation Measure 3A.3-4b: Valley Needlegrass avoidance and minimization measures.*</p> <p>Prior to ground-breaking activities including grading or construction, high visibility construction fencing should be placed around all Valley needlegrass grassland to be preserved. The construction fencing should not be removed until completion of construction activities.</p> <ul style="list-style-type: none"> • All Valley needlegrass grassland areas slated for removal should be replaced at a 1:1 acreage on-site within the preserve areas. • Needlegrass plants in areas slated for removal should be salvaged, to the extent feasible, and replanted within the preserve areas. If this is infeasible, then seedlings/saplings from a local nursery should be obtained. • A mitigation plan outlining methods to be used, success criteria to be met, and adaptive management strategies will be completed prior to project construction. <p>At a minimum, unless agreed upon otherwise with regulatory agencies, the Valley needlegrass grassland creation areas shall be monitored twice annually for the first year and once annually for the four subsequent years for a total of five years; success criteria shall be established to ensure an 80 percent success rate is met by the fifth year, and adaptive management techniques shall be implemented to ensure that the 80 percent success rate is met by the fifth year or as otherwise agreed upon in consultation with CDFW. This plan may be combined with the Operations and Management Plan for the open space preserves.</p> <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p>	<p>Before approval of grading or improvement plans or any ground-disturbing activities, including grubbing or clearing, for any project phase.</p>	<p>Project applicant(s) for any particular discretionary development application affecting valley needlegrassland.</p>	<p>1. California Department of Fish and Game, 2. City of Folsom Community Development Department.</p>	
<p>Mitigation Measure 3A.3-5: Oak woodlands mitigation.*</p>	<p>Before approval of grading or</p>	<p>Project applicant(s) of</p>	<p>1. City of Folsom Community</p>	

Westland/Eagle Specific Plan Amendment to the Folsom Plan Area Specific Plan
 Mitigation Monitoring and Reporting Program

May 2011, Revised November 2012, Revised September 2015
 City of Folsom

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<p>To fully mitigate for impacts to oak woodlands, the project applicant shall implement one or more of the mitigation measures listed below. Together, the mitigation measures will permanently protect approximately of 396,52-acres of existing FPASP oak woodlands and create approximately of 245,63-acres of new oak woodland habitat either on-site or with a combination of on-site and off-site location(s). The combined total of existing preserved oak woodlands and newly created oak woodlands will equal approximately 642 acres. The final area (acres) of preserved and newly created oak woodlands may be adjusted on a project-by-project basis at the time of tentative parcel or subdivision map approval to compensate for minor changes in oak woodland and isolated oak tree canopy impacts.</p> <p>Option 1: Preserve Existing Plan Area Oak Woodlands The FPASP shall permanently preserve and protect approximately 396,52-acres of existing oak woodlands. This figure represents 62 percent of the existing woodland habitat and 65 percent of the existing oak canopy in the FPASP area.</p> <p>Option 2: Create Oak Woodlands within the Plan Area Plant a combination of oak acorns, seedlings and oak trees (refer to Oak Woodlands Mitigation Planting Criteria below) within the boundaries of the Plan Area to create approximately 245,63-acres of new oak woodland habitat in the following locations (refer to Open Space Management Plan for allowable planting locations):</p> <ul style="list-style-type: none"> • non-wooded areas that are adjacent to or within the existing oak woodland habitat, • preserve and passive open space zones throughout the Plan Area, • open space areas that are adjacent to existing oak woodlands that will be impacted by project grading (i.e., catch slopes) and/or • other practical locations within the Plan Area adjacent to open space. <p>Option 3: Preserve and Protect Existing Off-site Oak Woodlands Existing, unprotected oak woodland habitat within Sacramento and El Dorado Counties may be secured and placed under conservation easement in lieu of on-site mitigation measures if necessary. The off-site locations shall be managed as oak woodland habitat in perpetuity.</p> <p>Option 4: Create Oak Woodlands Off-site Plant a combination of oak acorns, seedlings and oak trees at off-site location(s), if needed, following the same guidelines as outlined in the Oak Woodland Mitigation Planting Criteria below. Planted areas shall be placed under conservation easements and managed as oak woodlands in perpetuity.</p> <p>Oak Woodlands Mitigation Planting Criteria</p>	<p>improvement plans or any ground disturbing activities, including grubbing or clearing, for any project phase containing protected trees or oak woodland.</p>	<p>all project phases and off-site elements affecting blue oak woodland and protected trees.</p>	<p>Development Department. 2. Caltrans for interchange improvements to U.S. 50.</p>	

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<p>A minimum of 55 planting sites per acre (with a total of 70 units) will be required with additional minimum requirements of #1, #5 and #15 container plantings. Mitigation acreage that is planted solely with larger oak trees (no acorns) shall have minimum of 35 planting sites per acre. Plantings shall have unit values as outlined below:</p> <ul style="list-style-type: none"> • one established acorn equals one unit (acorns will be over planted to maximize potential germination), • one oak seedling in a #1 container equals two units (minimum of 10 percent required), • one #5 container oak tree equals three units (minimum of 10 percent required), • one #15 container oak tree equals four units (minimum of 10 percent required), • one 24-inch boxed oak tree equals six units, and • one transplanted oak tree equals four units per trunk diameter inch (diameter at breast height [DBH]). <p>The planting of non-oak species shall be required as a component of oak woodland mitigation to augment the overall habitat value of these areas. Appropriate non-oak species shall be determined by the city at the time of mitigation planting. Each non-oak planting will represent unit values as described above for oak trees, but no more than 10 percent of planting may be non-oak species to count as mitigation.</p> <p>Ratios of planting types will vary based upon site-specific conditions which would require an evaluation of several factors including irrigation needs, access, soil types, and evidence of natural oak recruitment. Some areas may be determined (in consultation with the city arborist) to be best suited for acorn planting only. These areas shall not be subject to the minimum planting requirement of #1, #5 and #15 container stock.</p> <p>Mitigation acreage shall be monitored for eight years to ensure that a minimum of 80 percent of planted unit values are successfully established. Trees surviving after 8 years, with a minimum of 3 years without maintenance or irrigation shall be considered successfully established.</p> <p>Isolated Oak Tree Mitigation</p> <p>Isolated oak trees in commercial and residential development parcels may be removed according to the following criteria:</p> <ul style="list-style-type: none"> • Trees rated 0 or 1 may be removed with no mitigation. • Trees rated 2 may be removed with 50 percent of required mitigation. • Trees rated 3, 4 or 5 may be removed at full required mitigation. <p>Isolated Oak Tree Mitigation Planting Criteria</p>				

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<p>For every one (1) diameter inch of removed oak tree, the mitigation shall be either:</p> <ul style="list-style-type: none"> • One half of a 24-inch boxed oak tree or, • One oak tree in a #15 container or, • Two oak trees in #5 containers or, • \$150 or a fee set by Folsom City Council resolution. <ul style="list-style-type: none"> • Replacement trees may be located within the boundaries of any development parcel, natural parkway, landscape corridor or passive or preserve open space zone. • Native oak trees transplanted within the Plan Area will be granted double mitigation credit. <p>Exceptions</p> <ul style="list-style-type: none"> • Isolated oak trees 24-inch (DBH) in diameter or larger, or a multi-trunked oak trees with an aggregate diameter of 40-inches or more (DBH) with a rating of 3 to 5 shall be retained unless retaining walls greater than 4-feet in height are required to save the tree. • Isolated oak trees 12-inch (DBH) to 24-inch (DBH) in diameter with a rating of 4 or 5 shall be retained unless retaining walls greater than 4-feet in height are required to save the tree. Trees with a rating of 2 or 3 may be removed if the cost to preserve the tree is greater than the cost to mitigate its loss based on the Isolated Oak Tree Mitigation Planting criteria above. • Isolated oak trees 5-inch (DBH) to 12-inch (DBH) in diameter with a rating of 4 or 5 shall be retained unless the cost to preserve the tree is greater than the cost to mitigate its loss based on the Isolated Oak Tree Mitigation Planting criteria above. • Isolated oak trees 1-inch (DBH) to 5-inch (DBH) in diameter that are preserved may be credited against oak tree mitigation requirements as follows: <table border="1" data-bbox="310 222 509 989"> <thead> <tr> <th>Trunk Diameter of Tree to be Preserved</th> <th>Mitigation Tree Size Equivalent</th> </tr> </thead> <tbody> <tr> <td>1" or greater, but less than 2"</td> <td>1 - #15 container tree or 2- #5 container trees</td> </tr> <tr> <td>2" or greater, but less than 3"</td> <td>2 - #15 container trees</td> </tr> <tr> <td>3" or greater, but less than 4"</td> <td>3 - #15 container trees</td> </tr> <tr> <td>4" or greater, but less than 5"</td> <td>4 - #15 container trees</td> </tr> </tbody> </table>	Trunk Diameter of Tree to be Preserved	Mitigation Tree Size Equivalent	1" or greater, but less than 2"	1 - #15 container tree or 2- #5 container trees	2" or greater, but less than 3"	2 - #15 container trees	3" or greater, but less than 4"	3 - #15 container trees	4" or greater, but less than 5"	4 - #15 container trees				
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<p>Oak Woodlands & Isolated Oak Tree Planting & Maintenance Agreement A planting and maintenance agreement shall include a planting plan, planting and irrigation design details and a monitoring schedule for the 5-year establishment period. Trees surviving after 8 years, with a minimum of 3 years without maintenance or irrigation shall be considered successfully established. An annual monitoring report shall be completed by 1 December of each year, including a summary of needed corrections, a proposed work plan and notice of compliance. All needed corrections shall be completed within 100 calendar days of receipt of the annual monitoring report.</p> <p><i>Performance Security</i> Security or other financing mechanisms acceptable to the city shall be required to fulfill the planting and maintenance agreement.</p> <p><i>No Additional Mitigation</i> No additional oak woodlands and isolated oak tree canopy mitigation is required for subsequent tentative and final parcel maps, subdivision maps and infrastructure improvement projects that are in compliance with the mitigation requirements of this section, the FPASP Open Space Management Plan and the FPASP EIR/EIS.</p> <p><i>Variances</i> Requests for variances to the isolated oak tree mitigation described above shall follow the process outlined in Folsom Municipal Code chapter 17.62. Any variance request shall be given increased consideration by the city when the purpose of the variance is to preserve additional oak trees.</p> <p>* This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p>				
<p>W/E SPA Mitigation Measure 4.4-1: Conduct environmental awareness training for construction employees. Before beginning construction activities, the project applicant shall employ a qualified biologist to develop and conduct environmental awareness training for construction employees. The training shall describe the importance of on-site biological resources, including special-status wildlife habitats; potential nests of special status birds; and roosting habitat for special-status bats. The biologist shall explain the importance of other responsibilities related to the protection of wildlife during construction such as inspecting open trenches and looking under vehicles and machinery before moving them to ensure there are no lizards,</p>	<p>Before any ground-disturbing activities for any project phase.</p>	<p>Project applicant(s) of all project phases.</p>	<p>City of Folsom Community Development Department.</p>	

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<p>snakes, small mammals, or other wildlife that could become trapped, injured, or killed in construction areas or under equipment.</p> <p>The environmental awareness program shall be provided to all construction personnel to brief them on the life history of special-status species in or adjacent to the project area, the need to avoid impacts on sensitive biological resources, any terms and conditions required by state and federal Agencies, and the penalties for not complying with biological mitigation requirements. If new construction personnel are added to the project, the contractor's superintendent shall ensure that the personnel receive the mandatory training before starting work. An environmental awareness handout that describes and illustrates sensitive resources to be avoided during project construction and identifies all relevant permit conditions shall be provided to each person.</p>				
<p>W/E SPA Mitigation Measure 4.4-2: Conduct preconstruction western spadefoot toad survey.</p> <p>Before ground breaking activities, the applicant shall comply with all conditions issued by the CDFW for the project. At minimum, the following shall occur:</p> <ul style="list-style-type: none"> • A preconstruction survey shall be conducted for Western spadefoot within 48 hours of the initiation of construction activity within suitable tadpole habitat (e.g., vernal pools, seasonal wetlands and drainages with standing water). Any Western spadefoot observed in the survey limits shall be reported to the CNDDB. If no Western spadefoot individuals are found during the preconstruction survey, the biologist shall document the findings in a letter report to CDFW and the City, and no further mitigation shall be required. If Western spadefoot individuals are found, the qualified biologist shall consult with CDFW to determine appropriate avoidance measures. <p>A qualified biological monitor(s) shall be present during construction to re locate any Western spadefoot in to suitable habitat up or downstream of the area of disturbance. Before construction, CDFW shall be notified of the intent to conduct Western spadefoot monitoring and potential relocation. Any Western spadefoot observed during biological monitoring activities shall be reported to the CNDDB.</p>	<p>Before any ground-disturbing activities for any project phase.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. City of Folsom Community Development Department. 2. California Department of Fish and Wildlife.</p>	
<p>W/E SPA Mitigation Measure 4.4-3: Conduct preconstruction western pond turtle survey.</p> <p>Before ground breaking activities, the applicant shall comply with all conditions stipulated in the Lake and Streambed Alteration Agreement issued by the CDFW for the project as required under Mitigation Measure 3A.3-4a. The following shall occur:</p>	<p>Before any ground-disturbing activities for any project phase.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. City of Folsom Community Development Department. 2. California Department of Fish and Wildlife.</p>	

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<ul style="list-style-type: none"> A preconstruction survey shall be conducted for nesting pond turtle within 48 hours prior to the initiation of construction activities by a CDFW approved biologist. If nesting areas for pond turtles are identified within the survey limits, a buffer area determined in coordination with CDFW shall be established between the construction area and the nesting site. Any western pond turtles observed in the survey limits shall be reported to the CNDDB. A qualified biological monitor(s) shall be present during construction to relocate any western pond turtles in to suitable habitat up or downstream of the area of disturbance. Before construction, CDFW shall be notified of the intent to conduct western pond turtle monitoring and potential relocation. Any western pond turtles observed during biological monitoring activities shall be reported to the CNDDB. 				
<p>W/E SPA Mitigation Measure 4.4-4: Conduct preconstruction Swainson's hawk and other raptor surveys.</p> <p>To mitigate impacts on Swainson's hawk and other raptors, a qualified biologist shall be retained to conduct preconstruction surveys and to identify active nests on and within 0.5 mile of the project area if construction begins during March through August. The surveys shall be conducted no less than 14 days and no more than 30 days before the beginning of construction activities/staging. Guidelines provided in Recommended Timing and Methodology for Swainson's Hawk Nesting Surveys in the Central Valley (Swainson's Hawk Technical Advisory Committee 2000) shall be followed for surveys for Swainson's hawk. If no active/occupied nests are found, no further mitigation is required.</p> <p>If active nests are found, impacts on nesting Swainson's hawks and other raptors shall be avoided by establishing appropriate buffers around the nests. No project activity shall commence within the buffer area until the young have fledged, the nest is no longer active, or until a qualified biologist has determined in coordination with CDFW that reducing the buffer would not result in nest abandonment. CDFW guidelines recommend implementation of 0.25- or 0.5-mile-wide buffers, but the size of the buffer may be adjusted if a qualified biologist and the City, in consultation with CDFW, determine that such an adjustment would not be likely to adversely affect the nest. Monitoring of the nest by a qualified biologist during and after construction activities shall be required if the activity has potential to adversely affect the nest.</p>	<p>Before any ground-disturbing activities for any project phase.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. City of Folsom Community Development Department. 2. California Department of Fish and Wildlife.</p>	
<p>W/E SPA Mitigation Measure 4.4-5: Prepare and Implement Swainson's hawk mitigation plan.</p>	<p>Before any ground-disturbing activities for any project phase.</p>	<p>Project applicant(s) of</p>	<p>1. City of Folsom Community</p>	

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<p>To mitigate for the loss of Swainson's hawk foraging habitat, the project applicant shall identify permanent impacts to foraging habitat and prepare and implement a Swainson's hawk mitigation plan including, but not limited to, the requirements described below.</p> <p>Before the approval of grading and improvement plans or before any ground-disturbing activities, whichever occurs first for each phase, the project applicant, to the satisfaction of the City, shall secure suitable Swainson's hawk foraging habitat to ensure 1:1 mitigation (or other agreed upon ratio) of habitat value for Swainson's hawk foraging habitat that is permanently lost as a result of the project phase, as determined by the City after consultation with CDFW and a qualified biologist.</p> <p>The 1:1 ratio (or other agreed-upon ratio) shall be based on Swainson's hawk nesting distribution and an assessment of habitat quality, availability, and use within the project area. The mitigation ratio shall be consistent with the 1994 Department of Fish and Game's Swainson's Hawk Guidelines included in the Staff Report Regarding Mitigation for Impacts to Swainson's Hawks (Butte Swainson) in the Central Valley of California (Swainson's Hawk Technical Advisory Committee 2000). These call for the following mitigation ratios for loss of foraging habitat in these categories: 1:1 if within one mile of an active nest site, 0.75:1 if over one mile but less than five miles, and 0.5:1 if over five miles and less than 10 miles from an active nest. Such mitigation shall be accomplished through purchase of credits at an approved mitigation bank, or the transfer of fee title or perpetual conservation easement. If non-bank mitigation is proposed, the mitigation land shall be located within the known foraging area and within Sacramento County. The City, after consultation with CDFW, shall determine the appropriateness of the mitigation land.</p> <p>The project applicant shall transfer said Swainson's hawk mitigation land, through either conservation easement or fee title, to a third-party, nonprofit conservation organization (Conservation Operator), with the City and CDFW named as third-party beneficiaries. The Conservation Operator shall be a qualified conservation easement land manager that manages land as its primary function. Additionally, the Conservation Operator shall be a tax-exempt nonprofit conservation organization that meets the criteria of Civil Code Section 815.3(a) and shall be selected or approved by the City, after consultation with CDFW. After consultation with CDFW and the Conservation Operator, the City shall approve the content and form of the conservation easement. The City, CDFW, and the Conservation Operator shall each have the power to enforce the terms of the conservation</p>		all project phases.	Development Department. 2. California Department of Fish and Wildlife.	

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<p>easement. The Conservation Operator shall monitor the easement in perpetuity to assure compliance with the terms of the easement.</p> <p>After consultation with the City, the project applicant, CDFW, and the Conservation Operator, shall establish an endowment or some other financial mechanism that is sufficient to fund in perpetuity the operation, maintenance, management, and enforcement of the conservation easement. If an endowment is used, either the endowment funds shall be submitted to the City for impacts on lands within the City's jurisdiction to an appropriate third-party nonprofit conservation agency, or they shall be submitted directly to the third-party nonprofit conservation agency in exchange for an agreement to manage and maintain the lands in perpetuity. The Conservation Operator shall not sell, lease, or transfer any interest of any conservation easement or mitigation land it acquires without prior written approval of the City and CDFW.</p> <p>If the Conservation Operator ceases to exist, the duty to hold, administer, manage, maintain, and enforce the interest shall be transferred to another entity acceptable to the City and CDFW. The City shall ensure that mitigation habitat established for impacts on habitat within the City's planning area is properly established and is functioning as habitat by conducting regular monitoring of the mitigation site(s) for the first ten years after establishment of the easement.</p>				
<p>W/E SPA Mitigation Measure 4.4-6: Conduct preconstruction burrowing owl survey.</p> <p>To mitigate impacts on burrowing owl, a qualified biologist shall be retained to conduct preconstruction surveys to identify active burrows within the project area. The surveys shall be conducted no less than 14 days and no more than 30 days before the beginning of construction. The preconstruction survey shall follow the protocols outlined in the Staff Report on Burrowing Owl Mitigation (CDFG 2012). Burrowing owls may be present on-site during any season.</p> <p>If active burrows are found, a mitigation plan shall be submitted to the City for review and approval before any ground-disturbing activities. The City shall consult with CDFW. The mitigation plan may consist of installation of one-way doors (during the non-breeding season) on all burrows to allow owls to exit, but not reenter, and construction of artificial burrows within the project vicinity, as needed; however, burrow owl exclusions during the breeding season (February 1-August 31) may only be used if a qualified biologist verifies that the burrow does not contain eggs or dependent young. If active burrows contain eggs and/or young, no construction shall occur within a minimum of 50 meters (164 feet) of</p>	<p>Before any ground-disturbing activities for any project phase.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. City of Folsom Community Development Department. 2. California Department of Fish and Wildlife.</p>	

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<p>the burrow until young have fledged. During the nonbreeding season, once it is confirmed that there are no owls inside burrows, the burrows may be collapsed.</p> <p>W/E SPA Mitigation Measure 4.4-7: Preconstruction nesting bird survey. The project applicant shall conduct a preconstruction nesting bird survey of all areas associated with construction activities on the project site within 14 days prior to commencement of construction during the nesting season (February 1 through August 31). If active nests are found, a no-disturbance buffer around the nest shall be established. The buffer distance shall be established by a qualified biologist in consultation with CDFW. The buffer shall be maintained until the fledglings are capable of flight and become independent of the nest, to be determined by a qualified biologist. Once the young are independent of the nest, no further measures are necessary. Pre-construction nesting surveys are not required for construction activity outside of the nesting season.</p>	<p>Before any ground-disturbing activities for any project phase.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. City of Folsom Community Development Department. 2. California Department of Fish and Wildlife.</p>	
<p>3A.4 – Climate Change</p> <p>Mitigation Measure 3A.4-1: Implement Additional Measures to Control Construction-Generated GHG Emissions. To further reduce construction-generated GHG emissions, the project applicant(s) any particular discretionary development application shall implement all feasible measures for reducing GHG emissions associated with construction that are recommended by SMAQMD at the time individual portions of the site undergo construction. Such measures may reduce GHG exhaust emissions from the use of on-site equipment, worker commute trips, and truck trips carrying materials and equipment to and from the SPA, as well as GHG emissions embodied in the materials selected for construction (e.g., concrete). Other measures may pertain to the materials used in construction. Prior to releasing each request for bid to contractors for the construction of each discretionary development entitlement, the project applicant(s) shall obtain the most current list of GHG reduction measures that are recommended by SMAQMD and stipulate that these measures be implemented in the respective request for bid as well as the subsequent construction contract with the selected primary contractor. The project applicant(s) for any particular discretionary development application may submit to the City and SMAQMD a report that substantiates why specific measures are considered infeasible for construction of that particular development phase and/or at that point in time. The report, including the substantiation for not</p>	<p>Before approval of small-lot final maps and building permits for all discretionary development project, including all on- and off-site elements and implementation throughout project construction.</p>	<p>Project applicant(s) during all discretionary development project phases and on-site and off-site elements.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For all on- and off-site project-related activities within the City of Folsom and Sacramento County. 3. For the two roadway extensions into El Dorado Hills: El Dorado County Development Services Department.</p>	

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<p>implementing particular GHG reduction measures, shall be approved by the City, in consultation with SMAQMD prior to the release of a request for bid by the project applicant(s) for seeking a primary contractor to manage the construction of each development project. By requiring that the list of feasible measures be established prior to the selection of a primary contractor, this measure requires that the ability of a contractor to effectively implement the selected GHG reduction measures be inherent to the selection process.</p> <p>SMAQMD's recommended measures for reducing construction-related GHG emissions at the time of writing this EIR/EIS are listed below and the project applicant(s) shall, at a minimum, be required to implement the following:</p> <ul style="list-style-type: none"> • Improve fuel efficiency from construction equipment: <ul style="list-style-type: none"> o reduce unnecessary idling (modify work practices, install auxiliary power for driver comfort); o perform equipment maintenance (inspections, detect failures early, corrections); o train equipment operators in proper use of equipment; o use the proper size of equipment for the job; and o use equipment with new technologies (repowered engines, electric drive trains). 				
<p>Mitigation Measure 3A.4-2a: Implement Additional Measures to Reduce Operational GHG Emissions.</p> <p>Each increment of new development within the project site requiring a discretionary approval (e.g., proposed tentative subdivision map, conditional use permit), shall be subject to a project-specific environmental review (which could support an applicable exemption, negative or mitigated negative declaration or project-specific EIR) and will require that GHG emissions from operation of each phase of development, including supporting roadway and infrastructure improvements that are part of the selected action alternative, will be reduced by an amount sufficient to achieve the 2020-based threshold of significance of 4.36 CO₂e/SP/year for development that would become operational on or before the year 2020, and the 2030-based threshold of significance of 2.86 CO₂e/SP/year for development that would become operational on or before the year 2030.</p> <p>The above-stated thresholds of significance may be subject to change if SMAQMD approves its own GHG significance thresholds, in which case, SMAQMD-adopted thresholds will be used. The amount of GHG reduction required to achieve the applicable significance thresholds will furthermore depend on existing and future regulatory measures including those developed under AB 32).</p>	<p>Before approval of final maps and building permits for all project phases, including all on- and off-site elements.</p>	<p>The project applicant(s) for any particular discretionary development.</p>	<p>City of Folsom Community Development Department.</p>	

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<p>For each increment of new discretionary development, the City shall submit to the project applicant(s) a list of potentially feasible GHG reduction measures to be considered in the development design. The City's list of potentially feasible GHG reduction measures shall reflect the current state of the regulatory environment, available incentives, and thresholds of significance that may be developed by SMAQMD, which will evolve under the mandate of AB 32 and Executive Order S-3-05. If the project applicant(s) asserts it cannot meet the 2020-based goal, then the report shall also demonstrate why measures not selected are considered infeasible. The City shall review and ensure inclusion of the design features in the proposed project before applicant(s) can receive the City's discretionary approval for the any increment of development. In determining what measures should appropriately be imposed by the City under the circumstances, the City shall consider the following factors:</p> <ul style="list-style-type: none"> the extent to which rates of GHG emissions generated by motor vehicles traveling to, from, and within the SPA are projected to decrease over time as a result of regulations, policies, and/or plans that have already been adopted or may be adopted in the future by ARB or other public agency pursuant to AB 32, or by EPA; the extent to which mobile-source GHG emissions, which at the time of writing this EIR/EIS comprise a substantial portion of the state's GHG inventory, can also be reduced through design measures that result in trip reductions and reductions in trip length; the extent to which GHG emissions emitted by the mix of power generation operated by SMUD, the electrical utility that will serve the SPA, are projected to decrease pursuant to the Renewables Portfolio Standard required by SB 1078 and SB 107, as well as any future regulations, policies, and/or plans adopted by the federal and state governments that reduce GHG emissions from power generation; the extent to which any stationary sources of GHG emissions that would be operated on a proposed land use (e.g., industrial) are already subject to regulations, policies, and/or plans that reduce GHG emissions, particularly any future regulations that will be developed as part of ARB's implementation of AB 32, or other pertinent regulations on stationary sources that have the indirect effect of reducing GHG emissions; the extent to which other mitigation measures imposed on the project to reduce other air pollutant emissions may also reduce GHG emissions; 				

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<ul style="list-style-type: none"> the extent to which the feasibility of existing GHG reduction technologies may change in the future, and to which innovation in GHG reduction technologies will continue, effecting cost-benefit analyses that determine economic feasibility; and whether the total costs of proposed mitigation for GHG emissions, together with other mitigation measures required for the proposed development, are so great that a reasonably prudent property owner would not proceed with the project in the face of such costs. <p>In considering how much, and what kind of, mitigation is necessary in light of these factors, the City shall consider the following list of options, though the list is not intended to be exhaustive, as GHG emission reduction strategies and their respective feasibility are likely to evolve over time. These measures are derived from multiple sources including the Mitigation Measure Summary in Appendix B of the California Air Pollution Control Officer's Association (CAPCOA) white paper, CEQA & Climate Change (CAPCOA 2009a); CAPCOA's Model Policies for Greenhouse Gases in General Plans (CAPCOA 2009b); and the California Attorney General's Office publication, The California Environmental Quality Act: Addressing Global Warming Impacts at the Local Agency Level (California Attorney General's Office 2008).</p> <p>Energy Efficiency</p> <ul style="list-style-type: none"> Include clean alternative energy features to promote energy self-sufficiency (e.g., photovoltaic cells, solar thermal electricity systems, small wind turbines). Design buildings to meet CEC Tier II requirements (e.g., exceeding the requirements of the Title 24 [as of 2007] by 35%). Site buildings to take advantage of shade and prevailing winds and design landscaping and sun screens to reduce energy use. Install efficient lighting in all buildings (including residential). Also install lighting systems in all buildings. Install light-colored "cool" pavements, and strategically located shade trees along all bicycle and pedestrian routes. <p>Water Conservation and Efficiency</p> <ul style="list-style-type: none"> With the exception of ornamental shade trees, use water-efficient landscapes with native, drought-resistant species in all public area and commercial landscaping. Use water-efficient turf in parks and other turf-dependent spaces. Install the infrastructure to use reclaimed water for landscape irrigation and/or washing cars. 				

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<ul style="list-style-type: none"> • Install water-efficient irrigation systems and devices, such as soil moisture-based irrigation controls. • Design buildings and lots to be water-efficient. Only install water-efficient fixtures and appliances. • Restrict watering methods (e.g., prohibit systems that apply water to nonvegetated surfaces) and control runoff. Prohibit businesses from using pressure washers for cleaning driveways, parking lots, sidewalks, and street surfaces. These restrictions should be included in the Covenants, Conditions, and Restrictions of the community. • Provide education about water conservation and available programs and incentives. • To reduce stormwater runoff, which typically bogs down wastewater treatment systems and increases their energy consumption, construct driveways to single-family detached residences and parking lots and driveways of multifamily residential uses with pervious surfaces. Possible designs include Hollywood drives (two concrete strips with vegetation or aggregate in between) and/or the use of porous concrete, porous asphalt, turf blocks, or pervious pavers. <p>Solid Waste Measures</p> <ul style="list-style-type: none"> • Reuse and recycle construction and demolition waste (including, but not limited to, soil, vegetation, concrete, lumber, metal, and cardboard). • Provide interior and exterior storage areas for recyclables and green waste at all buildings. • Provide adequate recycling containers in public areas, including parks, school grounds, golf courses, and pedestrian zones in areas of mixed-use development. • Provide education and publicity about reducing waste and available recycling services. <p>Transportation and Motor Vehicles</p> <ul style="list-style-type: none"> • Promote ride-sharing programs and employment centers (e.g., by designating a certain percentage of parking spaces for ride-sharing vehicles, designating adequate passenger loading and unloading zones and waiting areas for ride-share vehicles, and providing a Web site or message board for coordinating ride-sharing). • Provide the necessary facilities and infrastructure in all land use types to encourage the use of low- or zero-emission vehicles (e.g., electric vehicle charging facilities and conveniently located alternative fueling stations). 				

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<p>• At industrial and commercial land uses, all forklifts, "yard trucks," or vehicles that are predominately used on-site at non-residential land uses shall be electric-powered or powered by biofuels (such as biodiesel [B100]) that are produced from waste products, or shall use other technologies that do not rely on direct fossil fuel consumption.</p> <p>Mitigation Measure 3A.4-2b: Participate in and Implement an Urban and Community Forestry Program and/or Off-Site Tree Program to Off-Set Loss of On-Site Trees.</p> <p>The trees on the project site contain sequestered carbon and would continue to provide future carbon sequestration during their growing life. For all harvestable trees that are subject to removal, the project applicant(s) for any particular discretionary development application shall participate in and provide necessary funding for urban and community forestry program (such as the UrbanWood program managed by the Urban Forest Ecosystems Institute [Urban Forest Ecosystems Institute 2009]) to ensure that wood with an equivalent carbon sequestration value to that of all harvestable removed trees is harvested for an end-use that would retain its carbon sequestration (e.g., furniture building, cabinet making). For all nonharvestable trees that are subject to removal, the project applicant(s) shall develop and fund an off-site tree program that includes a level of tree planting that, at a minimum, increases carbon sequestration by an amount equivalent to what would have been sequestered by the blue oak woodland during its lifetime. This program shall be funded by the project applicant(s) of each development phase and reviewed for comment by an independent Certified Arborist unaffiliated with the project applicant(s) and shall be coordinated with the requirements of Mitigation Measure 3.3-5, as stated in Section 3A.3, "Biological Resources - Land." Final approval of the program shall be provided by the City. Components of the program may include, but not be limited to, providing urban tree canopy in the City of Folsom, or reforestation in suitable areas outside the City. Reforestation in natural habitat areas outside the City of Folsom would simultaneously mitigate the loss of oak woodland habitat while planting trees within the urban forest canopy would not. The California Urban Forestry Greenhouse Gas Reporting Protocol shall be used to assess this mitigation program (CCAR 2008). All unused vegetation and tree material shall be mulched for use in landscaping on the project site, shipped to the nearest composting facility, or shipped to a landfill that is equipped with a methane collection system, or combusted in a biomass power plant. Tree and vegetative</p>	<p>Before approval of final maps and/or building permits for all project phases requiring discretionary approval, including all on- and off-site elements.</p>	<p>The project applicant(s) for any particular discretionary development application.</p>	<p>The City of Folsom Community Development Department.</p>	

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<p>material should not be burned on- or off-site unless used as fuel in a biomass power plant.</p> <p>3A.5 Cultural Resources</p> <p>Mitigation Measure 3A.5-1a: Comply with the Programmatic Agreement.* The PA for the proposed project is incorporated by reference. The PA provides a management framework for identifying historic properties, determining adverse effects, and resolving those adverse effects as required under Section 106 of the NHPA. This document is incorporated by reference. The PA is available for public inspection and review at the California Office of Historic Preservation 1725 23rd Street Sacramento, CA 95816.</p> <p>The project and all of its earlier components, including backbone and non-backbone portions of the property, have been subjected to cultural resources studies prepared under the PA and subsequent FAPA. Historical resources have been identified, significant impacts have been determined, and mitigation of significant impacts has been proposed through HPTPs (ECORP 2015b) all with concurrence by SHPO. The applicable mitigation measures from the HPTPs are provided below, relative to Mitigation Measure 3A.5-1b, 3A.5-2, and 3A.5-3.</p> <p>At the time of the submission of the summary report (April 2015), only a portion of the applicable HPTPs for the project have been implemented. Therefore, full compliance with this mitigation measure will be satisfied upon written approval from the USACE and/or SHPO of the documentation resulting from implementation of the applicable portions of the HPTPs for the Backbone, Carpenter Ranch, and Mangini Ranch.</p> <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p> <p>Mitigation Measure 3A.5-1b: Perform an Inventory and Evaluation of Cultural Resources for the California Register of Historic Places, Minimize or Avoid Damage or Destruction, and Perform Treatment Where Damage or Destruction Cannot be Avoided.* These steps may be combined with deliverables and management steps performed for Section 106 provided that management documents prepared for the PA also clearly reference the California Register of Historical Resources (CRHR) listing criteria and significance thresholds that apply under CEQA. Before ground</p>	<p>The PA shall be prepared and executed (signed) prior to issuance of any Federal permit or authorization for any aspect or component of the specific plan project.</p>	<p>USACE (or designee) and the project applicant(s) of all project phases (as directed by USACE)</p>	<p>USACE and the project applicant(s) of all project phases (as directed by USACE), with oversight by the SHPO.</p>	
	<p>Before issuance of building permits and ground-disturbing activities.</p>	<p>The applicable oversight agency and the project applicant(s) (at the agency's direction) of all project phases.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department.</p>	

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<p>disturbing work for each individual development phase or off-site element, the applicable oversight agency (City of Folsom, El Dorado County, Sacramento County, or Caltrans), or the project applicant(s) of all project phases, with applicable agency oversight, shall perform the following actions:</p> <ul style="list-style-type: none"> The project applicant shall retain the services of a qualified archaeologist to perform an inventory of cultural resources within each individual development phase or off-site element subject to approval under CEQA. Identified resources shall be evaluated for listing on the CRHR. The inventory report shall also identify locations that are sensitive for undiscovered cultural resources based upon the location of known resources, geomorphology, and topography. The inventory report shall specify the location of monitoring of ground-disturbing work in these areas by a qualified archaeologist, and monitoring in the vicinity of identified resources that may be damaged by construction, if appropriate. The identification of sensitive locations subject to monitoring during construction of each individual development phase shall be performed in concert with monitoring activities performed under the PA to minimize the potential for conflicting requirements. For each resource that is determined eligible for the CRHR, the applicable agency or the project applicant(s) for any particular discretionary development (under the agency's direction) shall obtain the services of a qualified archaeologist who shall determine if implementation of the individual project development would result in damage or destruction of "significant" (under CEQA) cultural resources. These findings shall be reviewed by the applicable agency for consistency with the significance thresholds and treatment measures provided in this EIR/EIS. Where possible, the project shall be configured or redesigned to avoid impacts on eligible or listed resources. Alternatively, these resources may be preserved in place if possible, as suggested under California Public Resources Code Section 21083.2. Avoidance of historic properties is required under certain circumstances under the Public Resource Code and 36 CFR Part 800. Where impacts cannot be avoided, the applicable agency or the project applicant(s) of all project phases (under the applicable agency's direction) shall prepare and implement treatment measures that are determined to be necessary by a qualified archaeologist. These measures may consist of data recovery excavations for resources that are eligible for listing because of the data they contain (which may contribute to research). Alternatively, for historical architectural, engineered, or landscape features, treatment measures may consist of a preparation of interpretive, narrative, or photographic 			<ol style="list-style-type: none"> For the two roadway connections in El Dorado Hills: El Dorado County Development Services Department. For the detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department. For the U.S. 50 interchange improvements: Caltrans. 	

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<p>documentation. These measures shall be reviewed by the applicable oversight agency for consistency with the significance thresholds and standards provided in this EIR/EIS.</p> <ul style="list-style-type: none"> • To support the evaluation and treatment required under this mitigation measure, the archaeologist retained by either the applicable oversight agency or the project applicant(s) of all project phases shall prepare an appropriate prehistoric and historic context that identifies relevant prehistoric, ethnographic, and historic themes and research questions against which to determine the significance of identified resources and appropriate treatment. • These steps and documents may be combined with the phasing of management and documents prepared pursuant to the PA to minimize the potential for inconsistency and duplicative management efforts. • Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries shall be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties, or Caltrans). <p>As stated and cited above, the Mangini Ranch and Carpenter Ranch projects, including backbone and non-backbone portions of the property, have been subjected to cultural resources studies prepared under the PA and subsequent FARA. Historical resources have been identified with SHPO concurrence, preservation in place has been considered and maximized and remaining significant impacts have been determined with SHPO concurrence, mitigation of significant impacts has been proposed through HPTPs, and the HPMP has been revised and updated to incorporate the historic context.</p> <p>The applicable treatment measures specified in the HPTPs are in progress.</p> <p>Treatment 1: Landscape Mapping of Districts. Section 4.4 of the Backbone HPTP, Section 4.3 of the Carpenter Ranch HPTP, and Section 4.3 of the Mangini Ranch HPTP require low-level aerial photography and topographic mapping of the portions of the RDMID that fall within the FPASP. Because this district partly overlaps the SPA, compliance with this mitigation measure is required. Low-level aerial photography and lidar data collection was carried out in February 2012 for the majority of the project, and supplemental data meeting these standards was acquired August 2014 (collected spring 2008). All data were incorporated into the project's cultural GIS database maintained by ECORP Consulting, Inc. and updated boundaries for the districts and their contributing elements (California Department of Parks and Recreation or DPR records) have</p>				

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<p>been prepared and submitted to the USACE and NCIC. This treatment measure has been satisfied in full. Proof of compliance of the submission of the data to the USACE and NCIC (as applicable) shall be provided to the City.</p> <p>Treatment 2: Archival Research and Cultural Context. Section 4.4 of the Backbone HPTP, Section 4.3 of the Carpenter Ranch HPTP, and Section 4.3 of the Mangini Ranch HPTP require focused archival research and incorporation of information generated by implementation of the HPTP into the cultural context statement in the HPMP.</p> <p>Focused archival research was carried out as part of the preparation of the HPMP for the FPASP and applicable technical studies for the projects. This research was synthesized into the PHPS and later the HPMP, which were approved by the USACE with SHPO concurrence in June 2012. Because the HPMP is a living document that undergoes continual revision, proof of compliance with this treatment measure will be submission of draft interim revised text to the City of Folsom.</p> <p>Treatment 3: Public Interpretation. Section 4.4 of the Backbone HPTP, Section 4.3 of the Carpenter Ranch HPTP, and Section 4.3 of the Mangini Ranch HPTP require development of interpretive panels along public trails or at trailheads. The requirements for number and subject matter of interpretive panels under these requirements are as follows:</p> <ul style="list-style-type: none"> • Backbone HPTP: two panels for RDMD, one panel for the Alder Creek Corridor Mining District (not present within the SPA), and one panel for Native American culture; and • Mangini Ranch HPTP: one panel for RDMD and one panel for Native American culture; and • Carpenter Ranch HPTP: two panels for RDMD. <p>The requirement for interpretive panels in the Backbone HPTP is already being satisfied by the overall FPASP ownership. However, the Mangini Ranch and Carpenter Ranch projects shall be required to develop and install the panels specified in the HPTPs, which collectively number three panels for RDMD and one panel for Native American culture.</p> <p>Treatment 4: Photo-Documentation of Contributing Elements. Section 4.3 of the Backbone HPTP pertains to the documentation requirements for cumulative impacts to the historic White Rock Road and Lincoln Highway. The HPTP states:</p> <ul style="list-style-type: none"> • Resolution of adverse effect to the White Rock Road and Lincoln Highway shall occur through a combination of focused archival research and field documentation. An expanded cultural context statement shall be developed and incorporated into the Historic Property Synthesis report. The cultural 				

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<p>context statement will include historic maps, or recreated historic maps, that show the routes of White Rock Road and the Lincoln Highway adjacent to the SPA during its period of significance.</p> <ul style="list-style-type: none"> Field documentation will include photo-documentation of the existing road alignments using black-and-white and color photography, and videography. The photo-documentation will be extensive enough to capture the setting, alignment, and association with adjacent features. <p>It should be noted that this documentation has already been completed under the Backbone HPTP by the overall specific plan ownership. A copy of the completed and USACE-approved documentation shall be submitted to the City as proof of compliance.</p> <p>Treatment 4: Photo-Documentation of Contributing Elements. Section 4.4 of the Backbone HPTP, Section 4.3 of the Carpenter Ranch HPTP, and Section 4.3 of the Mangini Ranch HPTP require photodocumentation and updated site records for contributing elements to districts that will be significantly impacted by the project.</p> <p>Many of the contributing elements specified in Table 4 that require photo documentation have already been documented and submitted to the USACE for review and approval. The Applicants shall be required to provide proof of USACE-approved documentation for each of the contributing elements to the City as proof of compliance.</p> <p>Treatment 5: HAER Documentation. Impacts to the Rhoades' Branch Ditch, Keefe-McDerby Mine Ditch, and Natomas Canal will require mitigation through preparation of Historic American Engineering Record (HAER) documentation, in accordance with Section 4.1 of the Backbone HPTP, Section 4.1 of the Carpenter Ranch HPTP, and Section 4.1 of the Mangini Ranch HPTP.</p> <p>As of the time of the preparation of the HPTP, HAER documentation for the Rhoades' Branch Ditch and KeefeMcDerby Mine Ditch is already underway by other permittees in the SPA. The HAER documentation includes the portions that fall within the SPA. HAER documentation has not been initiated for the Natomas Canal as of the time of the preparation of this summary. The Applicants shall be required to submit proof of acceptance of the HAER documentation for all three ditches from the USACE to the City as proof of compliance.</p> <p>Treatment 6: Data Recovery Excavation. Significant impacts to sites P-34-1771, -4612, -4722, and -1774 will require mitigation in the form of data recovery, as specified in Section 4.2 of the Backbone HPTP, Section 4.2 of the Mangini Ranch HPTP, and Section 4.2 of the Carpenter Ranch HPTP.</p>				

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<p>As of the time of the preparation of this summary, the data recovery for sites P-34-4612 and -177 4 have been completed in conjunction with the implementation of the Backbone HTPP and the results are awaiting SHPO concurrence. The applicant shall be required to implement the data recovery program for sites P-34-1771 and -4722 and submit proof of acceptance from the USACE (via approval on the data recovery report or issuance of a notice to proceed with authorized fill under Stipulation 8 of the FAPA) to the City as proof of compliance.</p> <p>Treatment 7: Preservation. Impacts to the three prehistoric sites, P-34-1488, -1489; and -1490, and parts or all of historic sites -1744, -4612, -1742, and -1820, require preservation in perpetuity by Section 4.6 of the Mangini Ranch and Carpenter Ranch HPTPs through use of a cultural conservation easement and associated management. The applicant shall submit a copy of the fully executed deed of conservation easement(s) to the City as proof of compliance.</p> <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p>				
<p>Mitigation Measure 3A.5-2: Conduct Construction Personnel Education, Conduct On-Site Monitoring if Required, Stop Work if Cultural Resources are Discovered, Assess the Significance of the Find, and Perform Treatment or Avoidance as Required.*</p> <p>To reduce potential impacts to previously undiscovered cultural resources, the project applicant(s) of all project phases shall do the following:</p> <ul style="list-style-type: none"> • Before the start of ground-disturbing activities, the project applicant(s) of all project phases shall retain a qualified archaeologist to conduct training for construction workers as necessary based upon the sensitivity of the project APE, to educate them about the possibility of encountering buried cultural resources, and inform them of the proper procedures should cultural resources be encountered. • As a result of the work conducted for Mitigation Measures 3A.5-1a and 3A.5-1b, if the archaeologist determines that any portion of the SPA or the off-site elements should be monitored for potential discovery of as-yet-unknown cultural resources, the project applicant(s) of all project phases shall implement such monitoring in the locations specified by the archaeologist. USACE should review and approve any recommendations by archaeologists with respect to monitoring. • Should any cultural resources, such as structural features, unusual amounts of bone or shell, artifacts, or architectural remains be encountered during any 	<p>Before and during ground-disturbing activities.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. For actions taken to satisfy the requirements of Section 106: the SHPO and USACE. 2. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 3. For the two roadway connections off-site into El Dorado Hills: El Dorado County Development Services Department. 4. For the detention basin west of Prairie</p>	

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<p>construction activities, work shall be suspended in the vicinity of the find and the appropriate oversight agency(ies) (Identified below) shall be notified immediately. The appropriate oversight agency(ies) shall retain a qualified archaeologist who shall conduct a field investigation of the specific site and shall assess the significance of the find by evaluating the resource for eligibility for listing on the CRHR and the NRHP. If the resource is eligible for listing on the CRHR or NRHP and it would be subject to disturbance or destruction, the actions required in Mitigation Measures 3A.5-1a and 3A.5-1b shall be implemented. The oversight agency shall be responsible for approval of recommended mitigation if it is determined to be feasible in light of the approved land uses, and shall implement the approved mitigation before resuming construction activities at the archaeological site.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties, or Caltrans).</p> <p>The project applicant in coordination with USACE shall ensure that an archaeological sensitivity training program is developed and implemented during a pre-construction meeting for construction supervisors. The sensitivity training program shall provide information about notification procedures when potential archaeological material is discovered, procedures for coordination between construction personnel and monitoring personnel, and information about other treatment or issues that may arise if cultural resources (including human remains) are discovered during project construction. This protocol shall be communicated to all new construction personnel during orientation and on a poster that is placed in a visible location inside the construction job trailer. The phone number of the USACE cultural resources staff member shall also be included.</p> <p>The on-site sensitivity training shall be carried out each time a new contractor will begin work in the APE and at the beginning of each construction season by each contractor.</p> <p>In the event that unanticipated discoveries of additional Historic Properties, defined in 36 CFR 800.16 (l), are made during the construction of the project, the USACE shall ensure that they will be protected by implementing the following measures:</p> <ul style="list-style-type: none"> • The construction manager, or archaeological monitor, if given the authority to halt construction activities, shall ensure that work in that area is immediately halted within a 100-foot radius of the unanticipated discovery until the find is examined by a person meeting the professional qualifications standards 			<p>City Road: Sacramento County Planning and Community Development Department. 5. For the U.S. 50 interchange improvements: Caltrans.</p>	

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<p>specified in Section 2.2 of Attachment G of the HPMP (Westwood et al. 2013). The Construction Manager, or archaeological monitor, if present, shall notify the USACE within 24 hours of the discovery.</p> <ul style="list-style-type: none"> The USACE shall notify the SHPO within one working day of an unanticipated discovery, and may initiate interim treatment measures in accordance with this HPTP. Once the USACE makes a formal determination of eligibility for the resource, the USACE will notify the SHPO within 48 hours of the determination and afford the SHPO an opportunity to comment on appropriate treatment. The SHPO shall respond within 72 hours of the request to consult. Failure of the SHPO to respond within 72 hours shall not prohibit the USACE from implementing the treatment measures. The Backbone, Carpenter Ranch, and Mangini Ranch HPTPs require development and dissemination of a contractor awareness training program. The project shall be required to submit to the City proof of compliance in the form of a completed training roster and copy of training materials. <p>In addition, Section 4.5 of the Backbone HPTP, Section 4.4 of the Carpenter Ranch HPTP, and Section 4.4 of the Mangini Ranch HPTP require limited geoarchaeological monitoring during ground disturbing activities to 1.5 meters below surface, in specific localities found in the respective HPTPs. The applicant shall be required to submit to the City proof of compliance in the form of a memo of findings from the geoarchaeologist upon completion of the monitoring activities.</p> <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p>				
<p>Mitigation Measure 3A.5-3: Suspend Ground-Disturbing Activities if Human Remains are Encountered and Comply with California Health and Safety Code Procedures.*</p> <p>In accordance with the California Health and Safety Code, if human remains are uncovered during ground-disturbing activities, including those associated with off-site elements, the project applicant(s) of all project phases shall immediately halt all ground-disturbing activities in the area of the find and notify the applicable county coroner and a professional archaeologist skilled in osteological analysis to determine the nature of the remains. The coroner is required to examine all discoveries of human remains within 48 hours of receiving notice of a discovery on private or public lands (California Health and Safety Code Section 7050.5[b]). If the coroner determines that the remains are those of a Native American, he or she</p>	<p>Upon the discovery of suspected human remains.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For the two roadway connections in El Dorado Hills: El Dorado</p>	

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<p>must contact the NAHC by phone within 24 hours of making that determination (California Health and Safety Code Section 7050(c)).</p> <p>After the coroner's findings are complete, the project applicant(s), an archaeologist, and the NAHC-designated MLD shall determine the ultimate treatment and disposition of the remains and take appropriate steps to ensure that additional human interments are not disturbed. The responsibilities for acting on notification of a discovery of Native American human remains are identified in Section 5097.9 of the California Public Resources Code.</p> <p>Upon the discovery of Native American remains, the procedures above regarding involvement of the applicable county coroner, notification of the NAHC, and identification of an MLD shall be followed. The project applicant(s) of all project phases shall ensure that the immediate vicinity (according to generally accepted cultural or archaeological standards and practices) is not damaged or disturbed by further development activity until consultation with the MLD has taken place. The MLD shall have at least 48 hours after being granted access to the site to inspect the site and make recommendations. A range of possible treatments for the remains may be discussed: nondestructive removal and analysis, preservation in place, relinquishment of the remains and associated items to the descendants, or other culturally appropriate treatment. As suggested by Assembly Bill (AB) 2641 (Chapter 863, Statutes of 2006), the concerned parties may extend discussions beyond the initial 48 hours to allow for the discovery of additional remains. AB 2641(e) includes a list of site protection measures and states that the project applicant(s) shall comply with one or more of the following requirements:</p> <ul style="list-style-type: none"> • record the site with the NAHC or the appropriate Information Center, • use an open-space or conservation zoning designation or easement, or • record a document with the county in which the property is located. <p>The project applicant(s) or its authorized representative of all project phases shall rebury the Native American human remains and associated grave goods with appropriate dignity on the property in a location not subject to further subsurface disturbance if the NAHC is unable to identify an MLD or if the MLD fails to make a recommendation within 48 hours after being granted access to the site. The project applicant(s) or its authorized representative may also reinter the remains in a location not subject to further disturbance if it rejects the recommendation of the MLD and mediation by the NAHC fails to provide measures acceptable to the landowner. Ground disturbance in the zone of suspended activity shall not recommence without authorization from the archaeologist.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable</p>			<p>County Development Services Department. 3. For the detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department. 4. For the U.S. 50 interchange improvements: Caltrans.</p>	

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<p>project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties, or Caltrans). The Backbone, Carpenter Ranch, and Mangini Ranch HPTPs require development and dissemination of a contractor awareness training program. The applicant shall be required to submit to the City proof of compliance in the form of a completed training roster and copy of training materials.</p> <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p> <p>3A.7 Geology, Soils, Minerals, and Paleontological Resources</p>				
<p>Mitigation Measure 3A.7-1a: Prepare Site-Specific Geotechnical Report per CBC Requirements and Implement Appropriate Recommendations. Before building permits are issued and construction activities begin any project licensed geotechnical engineer to prepare a final geotechnical subsurface investigation report for the on- and off-site facilities, which shall be submitted for review and approval to the appropriate City or county department (identified below). The final geotechnical engineering report shall address and make recommendations on the following:</p> <ul style="list-style-type: none"> • site preparation; • soil bearing capacity; • appropriate sources and types of fill; • potential need for soil amendments; • road, pavement, and parking areas; • structural foundations, including retaining-wall design; • grading practices; • soil corrosion of concrete and steel; • erosion/winterization; • seismic ground shaking; • liquefaction; and • expansive/unstable soils. <p>In addition to the recommendations for the conditions listed above, the geotechnical investigation shall include subsurface testing of soil and groundwater conditions, and shall determine appropriate foundation designs that are consistent with the version of the CBC that is applicable at the time building and grading permits are applied for. All recommendations contained in the final</p>	<p>Before issuance of building permits and ground-disturbing activities.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For the two off-site roadway connections from Folsom Heights into El Dorado Hills: El Dorado County Public Works Department. 3. For the off-site detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department. 4. For the U.S. 50 interchange improvements: Caltrans.</p>	

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<p>geotechnical engineering report shall be implemented by the project applicant(s) of each project phase. Special recommendations contained in the geotechnical engineering report shall be noted on the grading plans and implemented as appropriate before construction begins. Design and construction of all new project development shall be in accordance with the CBC. The project applicant(s) shall provide for engineering inspection and certification that earthwork has been performed in conformity with recommendations contained in the geotechnical report.</p>				
<p>Mitigation Measure 3A.7-1b: Monitor Earthwork during Earthmoving Activities. All earthwork shall be monitored by a qualified geotechnical or soils engineer retained by the project applicant(s) of each project phase. The geotechnical or soils engineer shall provide oversight during all excavation, placement of fill, and disposal of materials removed from and deposited on both on- and off-site construction areas. Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties, or Caltrans).</p>	<p>Before issuance of building permits and ground-disturbing activities.</p>	<p>Project applicant(s) of all project phases.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For the two off-site roadway connections from Folsom Heights into El Dorado Hills: El Dorado County Public Works Department. 3. For the off-site detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department. 4. For the U.S. 50 interchange improvements: Caltrans.</p>	
<p>Mitigation Measure 3A.7-3: Prepare and Implement the Appropriate Grading and Erosion Control Plan. Before grading permits are issued, the project applicant(s) of each project phase that would be located within the City of</p>	<p>Before the start of construction activities.</p>	<p>Project applicant(s) of</p>	<p>1. For all project-related improvements that would be located</p>	

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<p>Folsom shall retain a California Registered Civil Engineer to prepare a grading and erosion control plan.</p> <p>The grading and erosion control plan shall be submitted to the City Public Works Department before issuance of grading permits for all new development. The plan shall be consistent with the City's Grading Ordinance, the City's Hillside Development Guidelines, and the state's NPDES permit, and shall include the site-specific grading associated with development for all project phases.</p> <p>For the two off-site roadways into El Dorado Hills, the project applicant(s) of that phase shall retain a California Registered Civil Engineer to prepare a grading and erosion control plan. The grading and erosion control plan shall be submitted to the El Dorado County Public Works Department and the El Dorado Hills Community Service District before issuance of grading permits for roadway construction in El Dorado Hills. The plan shall be consistent with El Dorado County's Grading, Erosion, and Sediment Control Ordinance and the state's NPDES permit, and shall include the site-specific grading associated with roadway development.</p> <p>For the off-site detention basin west of Prairie City Road, the project applicant(s) of that phase shall retain a California Registered Civil Engineer to prepare a grading and erosion control plan. The grading and erosion control plan shall be submitted to the Sacramento County Public Works Department before issuance of a grading permit. The plan shall be consistent with Sacramento County's Grading, Erosion, and Sediment Control Ordinance and the state's NPDES permit, and shall include the site-specific grading associated with construction of the detention basin.</p> <p>The plans referenced above shall include the location, implementation schedule, and maintenance schedule of all erosion and sediment control measures, a description of measures designed to control dust and stabilize the construction-site road and entrance, and a description of the location and methods of storage and disposal of construction materials. Erosion and sediment control measures could include the use of detention basins, berms, swales, wattles, and silt fencing, and covering or watering of stockpiled soils to reduce wind erosion. Stabilization on steep slopes could include construction of retaining walls and reseeded with vegetation after construction. Stabilization of construction entrances to minimize trackout (control dust) is commonly achieved by installing filter fabric and crushed rock to a depth of approximately 1 foot. The project applicant(s) shall ensure that the construction contractor is responsible for securing a source of transportation and deposition of excavated materials.</p>		<p>all project phases.</p>	<p>within the City of Folsom: Folsom: City of Folsom Community Development Department. 2. For the two off-site roadway connections from Folsom Heights into El Dorado Hills: El Dorado County Public Works Department. 3. For the off-site detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department.</p>	

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<p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties). Implementation of Mitigation Measure 3A.9-1 (discussed in Section 3A.9, "Hydrology and Water Quality – Land") would also help reduce erosion-related impacts.</p>				
<p>Mitigation Measure 3A.7-4: Prepare a Seismic Refraction Survey and Obtain Appropriate Permits for all On-Site and Off-site Elements East of Old Placerville Road. Before the start of all construction activities east of Old Placerville Road, the project applicant(s) for any discretionary development application shall retain a licensed geotechnical engineer to perform a seismic refraction survey. Project-related excavation activities shall be carried out as recommend by the geotechnical engineer. Excavation may include the use of heavy-duty equipment such as large bulldozers or large excavators, and may include blasting. Appropriate permits for blasting operations shall be obtained from the relevant City or county jurisdiction prior to the start of any blasting activities. Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties).</p>	<p>Before or during earthmoving activities.</p>	<p>Project applicant(s) of all project phases for on-site and off-site elements east of Old Placerville Road.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For the two off-site roadway connections from Folsom Heights into El Dorado Hills: El Dorado County Public Works Department.</p>	
<p>Mitigation Measure 3A.7-5: Divert Seasonal Water Flows Away from Building Foundations. The project applicant(s) of all project phases shall either install subdrains (which typically consist of perforated pipe and gravel, surrounded by nonwoven geotextile fabric), or take such other actions as recommended by the geotechnical or civil engineer for the project that would serve to divert seasonal flows caused by surface infiltration, water seepage, and perched water during the winter months away from building foundations.</p>	<p>Before or during earthmoving activities.</p>	<p>Project applicant(s) of all project phases</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For the two off-site roadway connections from Folsom Heights into El Dorado Hills: El Dorado County Public Works Department.</p>	

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<p>Mitigation Measure 3A.7-9: Conduct Soil Sampling in Areas of the SPA Designated as MRZ-3 for Kaolin Clay and if Found, Delineate its Location and Notify Lead Agency and the California Division of Mines and Geology.</p> <p>The project applicant(s) of all applicable project phases shall retain a licensed geotechnical or soils engineer to analyze soil core samples that shall be extracted from that portion of the SPA zoned MRZ-3 for kaolin clay, as shown on Exhibit 3A.7-3. In the event that kaolin clay is discovered, the City of Folsom, Sacramento County, and CDMG shall be notified. In addition, the approximate horizontal and vertical extent of available kaolin clay shall be delineated by the geotechnical or soils engineer.</p>	<p>Before issuance of building permits for development within the lone Formation.</p>	<p>Project applicant(s) of all project phases in the lone Formation.</p>	<p>City of Folsom Community Development Department, Sacramento County Planning and Community Development Department, California Division of Mines and Geology.</p>	
<p>Mitigation Measure 3A.7-10: Conduct Construction Personnel Education, Stop Work if Paleontological Resources are Discovered, Assess the Significance of the Find, and Prepare and Implement a Recovery Plan as Required.</p> <p>To minimize potential adverse impacts on previously unknown potentially unique, scientifically important paleontological resources, the project applicant(s) of all project phases where construction would occur in the lone and Mehrten Formations shall do the following:</p> <ul style="list-style-type: none"> • Before the start of any earthmoving activities for any project phase in the lone or Mehrten Formations, the project applicant(s) shall retain a qualified paleontologist or archaeologist to train all construction personnel involved with earthmoving activities, including the site superintendent, regarding the possibility of encountering fossils, the appearance and types of fossils likely to be seen during construction, and proper notification procedures should fossils be encountered. • If paleontological resources are discovered during earthmoving activities, the construction crew shall immediately cease work in the vicinity of the find and notify the appropriate lead agency (identified below). The project applicant(s) shall retain a qualified paleontologist to evaluate the resource and prepare a recovery plan in accordance with Society of Vertebrate Paleontology guidelines (1996). The recovery plan may include, but is not limited to, a field survey, construction monitoring, sampling and data recovery procedures, museum storage coordination for any specimen recovered, and a report of findings. Recommendations in the recovery plan that are determined by the lead agency to be necessary and feasible shall be implemented before construction activities can resume at the site where the paleontological resources were discovered. 	<p>During earthmoving activities in the lone and Mehrten Formations.</p>	<p>Project applicant(s) of all project phases within the lone and Mehrten Formations.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom Community Development Department. 2. For the off-site detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department.</p>	

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<p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., Sacramento County).</p> <p>3A.8 Hazards and Hazardous Materials</p> <p>Mitigation Measure 3A.8-2: Complete Investigations Related to the Extent to Which Soil and/or Groundwater May Have Been Contaminated in Areas Not Covered by the Phase I and II Environmental Site Assessments and Implement Required Measures.</p> <p>The project applicant(s) for any discretionary development application shall conduct Phase I Environmental Site Assessments (where a Phase I has not been conducted), and if necessary, Phase II Environmental Site Assessments, and/or other appropriate testing for all areas of the SPA and include, as necessary, analysis of soil and/or groundwater samples for the potential contamination sites that have not yet been covered by previous investigations (as shown in Exhibit 3A.8-1) before construction activities begin in those areas. Recommendations in the Phase I and II Environmental Site Assessments to address any contamination that is found shall be implemented before initiating ground-disturbing activities in these areas.</p> <p>The project applicant(s) shall implement the following measures before ground-disturbing activities to reduce health hazards associated with potential exposure to hazardous substances:</p> <ul style="list-style-type: none"> • Prepare a plan that identifies any necessary remediation activities appropriate for proposed on- and off-site uses, including excavation and removal of on-site contaminated soils, redistribution of clean fill material in the SPA, and closure of any abandoned mine shafts. The plan shall include measures that ensure the safe transport, use, and disposal of contaminated soil and building debris removed from the site. In the event that contaminated groundwater is encountered during site excavation activities, the contractor shall report the contamination to the appropriate regulatory agencies, dewater the excavated area, and treat the contaminated groundwater to remove contaminants before discharge into the sanitary sewer system. The project applicant(s) shall be required to comply with the plan and applicable Federal, state, and local laws. The plan shall outline measures for specific handling and reporting procedures for hazardous materials and disposal of hazardous materials removed from the site at an appropriate off-site disposal facility. • Notify the appropriate Federal, state, and local agencies if evidence of previously undiscovered soil or groundwater contamination (e.g., stained soil, 	<p>Project applicant(s) of all project phases for any discretionary development application.</p>	<p>Before and during earthmoving activities.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department.</p> <p>2. For the off-site detention basin west of Prairie City Road: Sacramento County Environmental Management Department.</p> <p>3. Other regulatory agencies, such as California Department of Toxic Substances Control, or Central Valley Regional Water Quality Control Board, as appropriate.</p>	

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<p>odoriferous groundwater) is encountered during construction activities. Any contaminated areas shall be remediated in accordance with recommendations made by the Sacramento County Environmental Management Department, Central Valley RWQCB, DTSC, and/or other appropriate Federal, state, or local regulatory agencies.</p> <ul style="list-style-type: none"> Obtain an assessment conducted by PG&E and SMUD pertaining to the contents of any existing pole-mounted transformers located in the SPA. The assessment shall determine whether existing on-site electrical transformers contain PCBs and whether there are any records of spills from such equipment. If equipment containing PCB is identified, the maintenance and/or disposal of the transformer shall be subject to the regulations of the Toxic Substances Control Act under the authority of the Sacramento County Environmental Health Department. <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., Sacramento County).</p>	<p>Ongoing to the satisfaction of EPA DTSC and/or the Central Valley RWQCB.</p>	<p>Project applicant(s) for activities that would occur in the Area 40 boundary or on areas used for groundwater monitoring and other remediation activities.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For the off-site detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department.</p>	
<p>Mitigation Measure 3A.8-3a: Require the Project Applicant(s) to Cooperate with Aerojet and Regulatory Agencies to Preserve, Modify, or Close Existing Groundwater Monitoring Wells.</p> <p>The project applicant(s) for any particular discretionary development that would occur in or adjacent to the Area 40 boundary shall consult with Aerojet, EPA, DTSC, and/or the Central Valley RWQCB or any successor in interest to establish the preservation, modification, or closure of existing groundwater monitoring wells. If necessary, Aerojet, or any successor may purchase lots or obtain access agreements from the project applicant(s) to maintain access to monitoring wells and/or remediation systems. If groundwater wells are to be affected by proposed tentative maps, then the project applicant(s) or successors shall provide the City with evidence that the relocation, modification, or closure of the well(s) is approved by the appropriate agencies as part of the City's final map approval process and before development.</p> <p>The project applicant(s) for activities related to the off-site detention basin located outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) with Sacramento County.</p>	<p>Before the approval of grading plans and during construction activities within the Area 40 boundary, off-</p>	<p>Project applicant(s) for activities within the Area 40 boundary or on</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom</p>	
<p>Mitigation Measure 3A.8-3b: Coordinate Development Activities to Avoid Interference with Remediation Activities.</p> <p>The project applicant(s) for any particular discretionary development that would occur in or adjacent to the Area 40 boundary shall provide notice to Aerojet or any successor in interest and DTSC, the Central Valley RWQCB, and the City of Folsom</p>				

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<p>of the location, nature, and duration of construction activities least 30 days before construction activities begin in areas on or near property with current or planned remediation activities (Area 40). Remedial actions, as required by DTSC, RWQCB, and/or the EPA, may include, but are not limited to:</p> <ul style="list-style-type: none"> • deed restrictions on land and groundwater use; • requirements for building ventilation, heating, and air conditioning design; • monitoring; • installation of vertical barriers; • biological, chemical, and/or physical treatment; • extraction or excavation; and/or • pump and treat activities. <p>Before the approval of grading plans which include areas within the Area 40 boundary or the off-site detention basin, the project applicant(s) shall consult with Aerojet, EPA, DTSC, and/or the Central Valley RWQCB or any successor to schedule the timing of construction activities to prevent potential conflicts with investigation and remediation activities.</p> <p>The project applicant(s) for activities related to the off-site detention basin located outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) with Sacramento County.</p>	<p>site detention basin, or on lands used for monitoring or other remediation-related activities.</p>	<p>lands used for monitoring or other remediation-related activities.</p>	<p>Community Development Department: 2. For the off-site detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department.</p>	
<p>Mitigation Measure 3A.8-3c: Provide Written Notification to the City that, as required by EPA, DTSC, and the Central Valley RWQCB, -Required Notification Obligations and/or Easements Have Been Fulfilled to Ensure that Construction Activities Do Not Interfere with Remedial Actions.</p> <p>Pursuant to their oversight over investigations of hazardous substances and determination of remedial action, EPA and/or DTSC establish, as appropriate, deed restrictions (e.g., restrictions on future groundwater uses or future land uses) or easements (e.g., continued access to groundwater wells and pipelines) on property with associated notice requirements. The project applicant(s) for all such affected project activities, located within the Area 40 boundary, the off-site detention basin, or lands subject to monitoring or other remediation activities shall provide notification in writing to the City (or Sacramento County for the off-site detention basin) that said required notification obligations have been fulfilled. Evidence of the method of notification required by EPA and/or DTSC shall be submitted to the City before approval of tentative maps or improvement plans.</p>	<p>Before approval of final maps and/or issuance of permits for sales trailers and model homes within the Area 40 boundary, the off-site detention basin, or lands subject to monitoring or other remediation activities.</p>	<p>Project applicant(s) for activities that would occur in the Area 40 boundary or on areas used for groundwater monitoring and other remediation activities.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department; 2. For the off-site detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department.</p>	

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<p>The project applicant(s) for such affected project activities shall coordinate with the City to include this provision as part of tentative map approval within the Area 40 boundary or lands subject to monitoring or other remediation activities. The project applicant(s) shall coordinate with Sacramento County for such affected project activities pertaining to the off-site detention basin.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., Sacramento County). Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., Sacramento County).</p> <p>Mitigation Measure 3A.8-3d: Land Use Restrictions for Contaminated Soil and Groundwater within Area 40 as depicted on the Remedial Restrictions Area Exhibit 3A.8-9.</p> <p>Prior to approval of any tentative maps, improvement plans, or discretionary project approvals for locations within Area 40, as depicted in the Remedial Restrictions Area (Exhibit 3A.8-9), the project applicant(s) shall designate those areas that are subject to off-gassing hazards in excess of an indoor air standard, as open space or park use, as required by the City and Aerojet in consultation with the EPA. Areas designated for open space or park under this mitigation measure shall be determined by the City and by Aerojet in consultation with the EPA using risk calculations (completed in accordance with EPA's 1989 Risk Assessment Guidance for Superfund [EPA/540/1-89-002] and DTSC's 1992 Supplemental Guidance for Human Health Multimedia Risk Assessments of Hazardous Waste Sites and Permitted Facilities and 1994 Preliminary Endangerment Assessment Guidance Manual, or such guidance as may be in place at the time risk assessment is performed) for exposure to off-gassing from either soil or groundwater based on detected PCE and TCE concentrations. The project applicant(s) for such affected areas located within Area 40 as depicted on the Remedial Restrictions Area Exhibit 3A.8-9 shall implement this measure as part of tentative map applications or other discretionary project approvals when such applications are submitted to the City.</p> <p>If the portions of Area 40 that are designated for park and open space use are not available for use as park and open space as identified in the SPA concurrently with surrounding development that creates demand for park and open space use, the project applicant(s), and the owners of land within the SPA shall identify and the City may rezone equivalent acreage of suitable park and open space land within</p>	<p>Prior to approval of tentative maps within the Community Park West area.</p>	<p>Project applicant(s) in consultation with the City, Aerojet, and U.S. Environmental Protection Agency for activities that would occur in the Community Park West area.</p>	<p>For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department; U.S. Environmental Protection Agency.</p>	

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<p>the SPA for development as interim or permanent park and open space to meet the then current demand.</p> <p>Mitigation Measure 3A.8-5: Prepare and Implement a Blasting Safety Plan in Consultation with a Qualified Blaster. To reduce the potential for accidental injury or death related to blasting, contractors whose work on the SPA will include blasting shall prepare and implement a blasting safety plan. This plan shall be created in coordination with a qualified blaster, as defined by the Construction Safety and Health Outreach Program, Subpart U, Section 1926.901, and distributed to all appropriate members of construction teams. The plan shall apply to project applicant(s) of all project phases in which blasting would be employed. The plan shall include, but is not limited to:</p> <ul style="list-style-type: none"> • storage locations that meet ATF standards contained in 27 CFR Part 55; • safety requirements for workers (e.g., daily safety meetings, personal protective equipment); • an accident management plan that considers misfires (i.e. explosive fails to detonate), unexpected ignition, and flyrock; and • measures to protect surrounding property (e.g., netting, announcement of dates of expected blasting, barricades, and audible and visual warnings). <p>Upon completion of a blasting safety plan, the project applicant(s) contractor shall secure any required permits from the City of Folsom Fire Department and the El Dorado County Sheriff's Department for blasting activities in Sacramento County and El Dorado County, respectively.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado County).</p> <p>Mitigation Measure 3A.8-6: Prudent Avoidance and Notification of EMF Exposure. Potential purchasers of residential properties near the transmission lines shall be made aware of the controversy surrounding EMF exposure. The California Department of Real Estate shall be requested to insert an appropriate notification into the applicant's final Subdivision Public Report application, which shall be provided to purchasers of properties within 100 feet from the 100-115kV power line, or within 150 feet from the 220-230 kV power line. The notification would include a discussion of the scientific studies and conclusions reached to date, acknowledge that the notification distance is not based on specific biological evidence, but rather, the distance where background levels may increase, and</p>	<p>At the submission of tentative map applications.</p>	<p>Project applicant(s) and contractor(s) of all project phases in which blasting would be employed.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Fire Department. 2. For the off-site roadway connections in El Dorado County: El Dorado County Sheriff's Department.</p>	
<p>Mitigation Measure 3A.8-6: Prudent Avoidance and Notification of EMF Exposure. Potential purchasers of residential properties near the transmission lines shall be made aware of the controversy surrounding EMF exposure. The California Department of Real Estate shall be requested to insert an appropriate notification into the applicant's final Subdivision Public Report application, which shall be provided to purchasers of properties within 100 feet from the 100-115kV power line, or within 150 feet from the 220-230 kV power line. The notification would include a discussion of the scientific studies and conclusions reached to date, acknowledge that the notification distance is not based on specific biological evidence, but rather, the distance where background levels may increase, and</p>	<p>At the submission of tentative map applications.</p>	<p>Project applicant(s) of all project phases for any particular discretionary development entitlement in the vicinity of high-tension</p>	<p>1. City of Folsom Community Development Department. 2. Folsom Cordova Unified School District.</p>	

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<p>provide that, given some uncertainty in the data, this notification is merely provided to allow purchasers to make an informed decision.</p> <p>Mitigation Measure 3A.8-7: Prepare and Implement a Vector Control Plan in Consultation with the Sacramento-Yolo Mosquito and Vector Control District. To ensure that operation and design of the stormwater system, including multiple planned detention basins, is consistent with the recommendations of the Sacramento-Yolo Mosquito and Vector Control District regarding mosquito control, the project applicant(s) of all project phases shall prepare and implement a Vector Control Plan. This plan shall be prepared in coordination with the Sacramento-Yolo Mosquito and Vector Control District and shall be submitted to the City for approval before issuance of the grading permit for the detention basins under the City's jurisdiction. For the off-site detention basin, the plan shall be submitted to Sacramento County for approval before issuance of the grading permit for the off-site detention basin. The plan shall incorporate specific measures deemed sufficient by the City to minimize public health risks from mosquitoes, and as contained within the Sacramento-Yolo Mosquito and Vector Control District BMP Manual (Sacramento-Yolo Mosquito and Vector Control District 2008). The plan shall include, but is not limited to, the following components:</p> <ul style="list-style-type: none"> • Description of the project. • Description of detention basins and all water features and facilities that would control on-site water levels. • Goals of the plan. • Description of the water management elements and features that would be implemented, including: <ul style="list-style-type: none"> ○ BMPs that would be implemented on-site; ○ public education and awareness; ○ sanitary methods used (e.g., disposal of garbage); ○ mosquito control methods used (e.g., fluctuating water levels, biological agents, pesticides, larvicides, circulating water); and ○ stormwater management (consistent with Stormwater Management Plan). • Long-term maintenance of the detention basins and all related facilities (e.g., specific ongoing enforceable conditions or maintenance by a homeowner's association). <p>To reduce the potential for mosquitoes to reproduce in the detention basins, the project applicant(s) shall coordinate with the Sacramento-Yolo Mosquito and Vector Control District to identify and implement BMPs based on their potential</p>	<p>Before issuance of grading permits for the project water features.</p>	<p>Project applicant(s) of all project phases containing water features.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For the off-site detention basin west of Prairie City Road: Sacramento-Yolo Mosquito and Vector Control District.</p>

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<p>effectiveness for SPA conditions. Potential BMPs could include, but are not limited to, the following:</p> <ul style="list-style-type: none"> • build shoreline perimeters as steep and uniform as practicable to discourage dense plant growth; • perform routine maintenance to reduce emergent plant densities to facilitate the ability of mosquito predators (i.e., fish) to move throughout vegetated area; • design distribution piping and containment basins with adequate slopes to drain fully and prevent standing water. The design slope should take into consideration buildup of sediment between maintenance periods. Compaction during grading may also be needed to avoid slumping and settling; • coordinate cleaning of catch basins, drop inlets, or storm drains with mosquito treatment operations; • enforce the prompt removal of silt screens installed during construction when no longer needed to protect water quality; • if the sump, vault, or basin is sealed against mosquitoes, with the exception of the inlet and outlet, submerge the inlet and outlet completely to reduce the available surface area of water for mosquito egg-laying (female mosquitoes can fly through pipes); and • design structures with the appropriate pumping, piping, valves, or other necessary equipment to allow for easy dewatering of the unit if necessary (Sacramento Yolo Mosquito and Vector Control District 2008). <p>The project applicant(s) of the project phase containing the off-site detention basin shall coordinate mitigation for the off-site with the affected oversight agency (i.e., Sacramento County).</p>				
<p>3A.9 Hydrology and Water Quality</p> <p>Mitigation Measure 3A.9-1: Acquire Appropriate Regulatory Permits and Prepare and Implement SWPPP and BMPs.</p> <p>Prior to the issuance of grading permits, the project applicant(s) of all projects disturbing one or more acres (including phased construction of smaller areas which are part of a larger project) shall obtain coverage under the SWRCB's NPDES stormwater permit for general construction activity (Order 2009-0009-DWQ), including preparation and submittal of a project-specific SWPPP at the time the NOI is filed. The project applicant(s) shall also prepare and submit any other necessary erosion and sediment control and engineering plans and specifications for pollution prevention and control to Sacramento County, City of Folsom, El Dorado County (for the off-site roadways into El Dorado Hills under the Proposed</p>	<p>Submittal of the State Construction General Permit NOI and SWPPP (where applicable) and development and submittal of any other locally required plans and specifications before the issuance of grading permits for all on-site project phases</p>	<p>Project applicant(s) during all project phases and on-site and off-site elements.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For the two roadway connections in El Dorado Hills: El Dorado</p>	

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<p>Project Alternative). The SWPPP and other appropriate plans shall identify and specify:</p> <ul style="list-style-type: none"> the use of an effective combination of robust erosion and sediment control BMPs and construction techniques accepted by the local jurisdictions for use in the project area at the time of construction, that shall reduce the potential for runoff and the release, mobilization, and exposure of pollutants, including legacy sources of mercury from project-related construction sites. These may include but would not be limited to temporary erosion control and soil stabilization measures, sedimentation ponds, inlet protection, perforated riser pipes, check dams, and silt fences the implementation of approved local plans, non-stormwater management controls, permanent post-construction BMPs, and inspection and maintenance responsibilities; the pollutants that are likely to be used during construction that could be present in stormwater drainage and nonstormwater discharges, including fuels, lubricants, and other types of materials used for equipment operation; spill prevention and contingency measures, including measures to prevent or clean up spills of hazardous waste and of hazardous materials used for equipment operation, and emergency procedures for responding to spills; personnel training requirements and procedures that shall be used to ensure that workers are aware of permit requirements and proper installation methods for BMPs specified in the SWPPP; and the appropriate personnel responsible for supervisory duties related to implementation of the SWPPP. <p>Where applicable, BMPs identified in the SWPPP shall be in place throughout all site work and construction/demolition activities and shall be used in all subsequent site development activities. BMPs may include, but are not limited to, such measures as those listed below.</p> <ul style="list-style-type: none"> implementing temporary erosion and sediment control measures in disturbed areas to minimize discharge of sediment into nearby drainage conveyances, in compliance with state and local standards in effect at the time of construction. These measures may include silt fences, staked straw bales or watties, sediment/silt basins and traps, geofabric, sandbag dikes, and temporary vegetation. Establishing permanent vegetative cover to reduce erosion in areas disturbed by construction by slowing runoff velocities, trapping sediment, and enhancing filtration and transpiration. 	<p>and off-site elements and implementation throughout project construction.</p>		<p>County Department of Transportation. 3. For the detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department. 4. For the U.S. 50 interchange improvements: Caltrans. 5. For all construction activities subject to the state's Construction General Permit and violators of local ordinances referred to the state for enforcement: Central Valley Regional Water Quality Control Board.</p>	

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<ul style="list-style-type: none"> Using drainage swales, ditches, and earth dikes to control erosion and runoff by conveying surface runoff down sloping land, intercepting and diverting runoff to a watercourse or channel, preventing sheet flow over sloped surfaces, preventing runoff accumulation at the base of a grade, and avoiding flood damage along roadways and facility infrastructure. <p>A copy of the approved SWPPP shall be maintained and available at all times on the construction site.</p> <p>For those areas that would be disturbed as part of the U.S. 50 interchange improvements, Caltrans shall coordinate with the development and implementation of the overall project SWPPP, or develop and implement its own SWPPP specific to the interchange improvements, to ensure that water quality degradation would be avoided or minimized to the maximum extent practicable. Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties, or Caltrans).</p> <p>Mitigation Measure 3A.9-2: Prepare and Submit Final Drainage Plans and Implement Requirements Contained in Those Plans.</p> <p>Before the approval of grading plans and building permits, the project applicant(s) of all project phases shall submit final drainage plans to the City, and to El Dorado County for the off-site roadway connections into El Dorado Hills, demonstrating that off-site upstream runoff would be appropriately conveyed through the SPA, and that project-related on-site runoff would be appropriately contained in detention basins or managed with through other improvements (e.g., source controls, biotechnical stream stabilization) to reduce flooding and hydromodification impacts.</p> <p>The plans shall include, but not be limited to, the following items:</p> <ul style="list-style-type: none"> an accurate calculation of pre-project and post-project runoff scenarios, obtained using appropriate engineering methods, that accurately evaluates potential changes to runoff, including increased surface runoff; runoff calculations for the 10-year and 100-year (0.01 AEP) storm events (and other, smaller storm events as required) shall be performed and the trunk drainage pipeline sizes confirmed based on alignments and detention facility locations finalized in the design phase; a description of the proposed maintenance program for the on-site drainage system; project-specific standards for installing drainage systems; 	<p>Before approval of grading plans and building permits of all project phases.</p>	<p>Project applicant(s) during all on-site project phases and off-site elements.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Public Works Department.</p> <p>2. For the two roadway connections in El Dorado Hills: El Dorado County Department of Transportation.</p>	

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<ul style="list-style-type: none"> • City and El Dorado County flood control design requirements and measures designed to comply with them; Implementation of stormwater management BMPs that avoid increases in the erosive force of flows beyond a specific range of conditions needed to limit hydromodification and maintain current stream geomorphology. These BMPs will be designed and constructed in accordance with the forthcoming SSQP Hydromodification Management Plan (to be adopted by the RWQCB) and may include, but are not limited to, the following: <ul style="list-style-type: none"> ○ use of Low Impact Development (LID) techniques to limit increases in stormwater runoff at the point of origination (these may include, but are not limited to: surface swales; replacement of conventional impervious surfaces with pervious surfaces [e.g., porous pavement]; impervious surfaces disconnection; and trees planted to intercept stormwater); ○ enlarged detention basins to minimize flow changes and changes to flow duration characteristics; ○ bioengineered stream stabilization to minimize bank erosion, utilizing vegetative and rock stabilization, and inset floodplain restoration features that provide for enhancement of riparian habitat and maintenance of natural hydrologic and channel to floodplain interactions; ○ minimize slope differences between any stormwater or detention facility outfall channel with the existing receiving channel gradient to reduce flow velocity; and ○ minimize to the extent possible detention basin, bridge embankment, and other encroachments into the channel and floodplain corridor, and utilize open bottom box culverts to allow sediment passage on smaller drainage courses. • The final drainage plan shall demonstrate to the satisfaction of the City of Folsom Community Development and Public Works Departments and El Dorado County Department of Transportation that 100-year (0.01 AEP) flood flows would be appropriately channeled and contained, such that the risk to people or damage to structures within or down gradient of the SPA would not occur, and that hydromodification would not be increased from pre-development levels such that existing stream geomorphology would be changed (the range of conditions should be calculated for each receiving water if feasible, or a conservative estimate should be used, e.g., an Ep of 1 ±10% or other as approved by the Sacramento Stormwater Quality Partnership and/or City of Folsom Public Works Department). 				

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<p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with El Dorado County.</p> <p>Mitigation Measure 3A.9-3: Develop and Implement a BMP and Water Quality Maintenance Plan.</p> <p>Before approval of the grading permits for any development project requiring a subdivision map, a detailed BMP and water quality maintenance plan shall be prepared by a qualified engineer retained by the project applicant(s) the development project. Drafts of the plan shall be submitted to the City of Folsom and El Dorado County for the off-site roadway connections into El Dorado Hills, for review and approval concurrently with development of tentative subdivision maps for all project phases. The plan shall finalize the water quality improvements and further detail the structural and nonstructural BMPs proposed for the project. The plan shall include the elements described below.</p> <ul style="list-style-type: none"> • A quantitative hydrologic and water quality analysis of proposed conditions incorporating the proposed drainage design features. • Predevelopment and postdevelopment calculations demonstrating that the proposed water quality BMPs meet or exceed requirements established by the City of Folsom and including details regarding the size, geometry, and functional timing of storage and release pursuant to the "Stormwater Quality Design Manual for Sacramento and South Placer Regions" ([SSQP 2007b] per NPDES Permit No. CAS082597 WDR Order No. R5-2008-0142, page 46) and El Dorado County's NPDES SWMP (County of El Dorado 2004). • Source control programs to control water quality pollutants on the SPA, which may include but are limited to recycling, street sweeping, storm drain cleaning, household hazardous waste collection, waste minimization, prevention of spills and illegal dumping, and effective management of public trash collection areas. • A pond management component for the proposed basins that shall include management and maintenance requirements for the design features and BMPs, and responsible parties for maintenance and funding. • LID control measures shall be integrated into the BMP and water quality maintenance plan. These may include, but are not limited to: <ul style="list-style-type: none"> ○ surface swales; ○ replacement of conventional impervious surfaces with pervious surfaces (e.g., porous pavement); ○ impervious surfaces disconnection; and ○ trees planted to intercept stormwater. 	<p>Prepare plans before the issuance of grading permits for all project phases and off-site elements and implementation throughout project construction.</p>	<p>Project applicant(s) during all on-site project phases and off-site elements.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department and Public Works Department. 2. For the two roadway connections in El Dorado Hills: El Dorado County Department of Transportation. 3. For the U.S. 50 interchange improvements: Caltrans.</p>	

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<p>• New stormwater facilities shall be placed along the natural drainage courses within the SPA to the extent practicable so as to mimic the natural drainage patterns. The reduction in runoff as a result of the UID configurations shall be quantified based on the runoff reduction credit system methodology described in "Stormwater Quality Design Manual for the Sacramento and South Placer Regions, Chapter 5 and Appendix D4" (SSQP 2007b) and proposed detention basins and other water quality BMPs shall be sized to handle these runoff volumes.</p> <p>For those areas that would be disturbed as part of the U.S. 50 interchange improvements, it is anticipated that Caltrans would coordinate with the development and implementation of the overall project SWPPP, or develop and implement its own SWPPP specific to the interchange improvements, to ensure that water quality degradation would be avoided or minimized to the maximum extent practicable.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with El Dorado County and Caltrans.</p>				
<p>Mitigation Measure 3A.9-4: Inspect and Evaluate Existing Dams Within and Upstream of the Project Site and Make Improvements if Necessary.</p> <p>Prior to submittal to the City of tentative maps or improvement plans the project applicant(s) of all project phases shall perform conduct studies to determine the extent of inundation in the case of dam failure. If the studies determine potential exposure of people or structures to a significant risk of flooding as a result of the failure of a dam, the applicants(s) shall implement of any feasible recommendations provided in that study, potentially through drainage improvements, subject to the approval of the City of Folsom Public Works Department.</p>				
<p>3A.11 Noise</p> <p>Mitigation Measure 3A.11-1: Implement Noise-Reducing Construction Practices, Prepare and Implement a Noise Control Plan, and Monitor and Record Construction Noise near Sensitive Receptors.</p> <p>To reduce impacts associated with noise generated during project-related construction activities, the project applicant(s) and their primary contractors for engineering design and construction of all project phases shall ensure that the following requirements are implemented at each work site in any year of project construction to avoid and minimize construction noise effects on sensitive</p>	<p>Before and during construction activities on the SPA and within El Dorado Hills.</p>	<p>Project applicant(s) and primary contractor(s) of all project phases.</p>	<p>1. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department.</p>	

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<p>receptors. The project applicant(s) and primary construction contractor(s) shall employ noise-reducing construction practices. Measures that shall be used to limit noise shall include the measures listed below:</p> <ul style="list-style-type: none"> • Noise-generating construction operations shall be limited to the hours between 7 a.m. and 7 p.m. Monday through Friday, and between 8 a.m. and 6 p.m. on Saturdays and Sundays. • All construction equipment and equipment staging areas shall be located as far as possible from nearby noise-sensitive land uses. • All construction equipment shall be properly maintained and equipped with noise-reduction intake and exhaust mufflers and engine shrouds, in accordance with manufacturers' recommendations. Equipment engine shrouds shall be closed during equipment operation. • All motorized construction equipment shall be shut down when not in use to prevent idling. • Individual operations and techniques shall be replaced with quieter procedures (e.g., using welding instead of riveting, mixing concrete off-site instead of on-site). • Noise-reducing enclosures shall be used around stationary noise-generating equipment (e.g., compressors and generators) as planned phases are built out and future noise sensitive receptors are located within close proximity to future construction activities. • Written notification of construction activities shall be provided to all noise-sensitive receptors located within 850 feet of construction activities. Notification shall include anticipated dates and hours during which construction activities are anticipated to occur and contact information, including a daytime telephone number, for the project representative to be contacted in the event that noise levels are deemed excessive. Recommendations to assist noise-sensitive land uses in reducing interior noise levels (e.g., closing windows and doors) shall also be included in the notification. • To the extent feasible, acoustic barriers (e.g., lead curtains, sound barriers) shall be constructed to reduce construction-generated noise levels at affected noise-sensitive land uses. The barriers shall be designed to obstruct the line of sight between the noise-sensitive land use and on-site construction equipment. When installed properly, acoustic barriers can reduce construction noise levels by approximately 8–10 dB (EPA 1971). • When future noise sensitive uses are within close proximity to prolonged construction noise, noise-attenuating buffers such as structures, truck trailers, 			<p>2. For the two roadway connections off-site into El Dorado Hills: El Dorado County Development Services Department.</p>	

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<p>or soil piles shall be located between noise sources and future residences to shield sensitive receptors from construction noise.</p> <ul style="list-style-type: none"> The primary contractor shall prepare and implement a construction noise management plan. This plan shall identify specific measures to ensure compliance with the noise control measures specified above. The noise control plan shall be submitted to the City of Folsom before any noise-generating construction activity begins. Construction shall not commence until the construction noise management plan is approved by the City of Folsom. Mitigation for the two off-site roadway connections into El Dorado County must be coordinated by the project applicant(s) of the applicable project phase with El Dorado County, since the roadway extensions are outside of the City of Folsom's jurisdictional boundaries. 				
<p>Mitigation Measure 3A.1.1-3: Implement Measures to Prevent Exposure of Sensitive Receptors to Groundborne Noise or Vibration from Project Generated Construction Activities.</p> <ul style="list-style-type: none"> To the extent feasible, blasting activities shall not be conducted within 275 feet of existing or future sensitive receptors. To the extent feasible, bulldozing activities shall not be conducted within 50 feet of existing or future sensitive receptors. All blasting shall be performed by a blast contractor and blasting personnel licensed to operate in the State of California. A blasting plan, including estimates of vibration levels at the residence closest to the blast, shall be submitted to the enforcement agency for review and approval prior to the commencement of the first blast. Each blast shall be monitored and documented for groundbourne noise and vibration levels at the nearest sensitive land use and associated recorded submitted to the enforcement agency. 	<p>Before and during bulldozing and blasting activities on the SPA and within El Dorado Hills and the County of Sacramento.</p>	<p>Project applicant(s) and primary contractor(s) of all project phases.</p>	<ol style="list-style-type: none"> For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department. For the two roadway connections off-site into El Dorado Hills: El Dorado County Development Services Department. For the off-site detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department. For the U.S. 50 interchange 	

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<p>Mitigation Measure 3A.11-4: Implement Measures to Prevent Exposure of Sensitive Receptors to Increases in Noise from Project-Generated Operational Traffic on Off-site and On-Site Roadways.</p> <p>To meet applicable noise standards as set forth in the appropriate General Plan or Code (e.g., City of Folsom, County of Sacramento, and County of El Dorado) and to reduce increases in traffic-generated noise levels at noise-sensitive uses, the project applicant(s) of all project phases shall implement the following:</p> <ul style="list-style-type: none"> • Obtain the services of a consultant (such as a licensed engineer or licensed architect) to develop noise-attenuation measures for the proposed construction of on-site noise-sensitive land uses (i.e., residential dwellings and school classrooms) that will produce a minimum composite Sound Transmission Class (STC) rating for buildings of 30 or greater, individually computed for the walls and the floor/ceiling construction of buildings, for the proposed construction of on-site noise-sensitive land uses (i.e., residential dwellings and school classrooms). • Prior to submittal of tentative subdivision maps and improvement plans, the project applicant(s) shall conduct a site-specific acoustical analysis to determine predicted roadway noise impacts attributable to the project, taking into account site-specific conditions (e.g., site design, location of structures, building characteristics). The acoustical analysis shall evaluate stationary- and mobile-source noise attributable to the proposed use or uses and impacts on nearby noise-sensitive land uses, in accordance with adopted City noise standards. Feasible measures shall be identified to reduce project-related noise impacts. These measures may include, but are not limited to, the following: <ul style="list-style-type: none"> ○ limiting noise-generating operational activities associated with proposed commercial land uses, including truck deliveries; ○ constructing exterior sound walls; ○ constructing barrier walls and/or berms with vegetation; ○ using "quiet pavement" (e.g., rubberized asphalt) construction methods on local roadways; and ○ using increased noise-attenuation measures in building construction (e.g., dual-pane, sound-rated windows; exterior wall insulation). 	<p>During project construction activities at noise-sensitive receptors on the SPA; at the existing noise-sensitive receptors on Empire Ranch Road from Broadstone Parkway to Iron Point Road; and at the existing noise-sensitive receptors on Latrobe Road from White Rock Road to Golden Foothills Parkway</p>	<p>Project applicant(s) of all project phases.</p>	<p>Improvements: Caltrans.</p> <p>1. For all noise-sensitive receptors that would be located within the City of Folsom: City of Folsom Community Development Department. 2. For all noise-sensitive receptors in El Dorado Hills: El Dorado County Development Services Department. 3. For all noise-sensitive receptors in the vicinity the off-site detention basin west of Prairie City Road: Sacramento County Planning and Community Development Department. 4. For all noise-sensitive receptors adjacent to the U.S. 50 interchange improvements: Caltrans.</p>	
<p>Mitigation Measure 3A.11-5: Implement Measures to Reduce Noise from Project-Generated Stationary Sources.</p>	<p>Before submittal of improvement plans for each project</p>	<p>Project applicant(s) of</p>	<p>City of Folsom Community</p>	

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<p>The project applicant(s) for any particular discretionary development project shall implement the following measures to reduce the effect of noise levels generated by on-site stationary noise sources that would be located within 600 feet of any noise-sensitive receptor:</p> <ul style="list-style-type: none"> • Routine testing and preventive maintenance of emergency electrical generators shall be conducted during the less sensitive daytime hours (i.e., 7:00 a.m. to 6:00 p.m.). All electrical generators shall be equipped with noise control (e.g., muffler) devices in accordance with manufacturers' specifications. • External mechanical equipment associated with buildings shall incorporate features designed to reduce noise emissions below the stationary noise source criteria. These features may include, but are not limited to, locating generators within equipment rooms or enclosures that incorporate noise-reduction features, such as acoustical louvers, and exhaust and intake silencers. Equipment enclosures shall be oriented so that major openings (i.e., intake louvers, exhaust) are directed away from nearby noise-sensitive receptors. • Parking lots shall be located and designed so that noise emissions do not exceed the stationary noise source criteria established in this analysis (i.e., 50 dB for 30 minutes in every hour during the daytime [7 a.m. to 10 p.m.] and less than 45 dB for 30 minutes of every hour during the night time [10 p.m. to 7 a.m.]). <p>Reduction of parking lot noise can be achieved by locating parking lots as far away as feasible from noise sensitive land uses, or using buildings and topographic features to provide acoustic shielding for noise-sensitive land uses.</p> <ul style="list-style-type: none"> • Loading docks shall be located and designed so that noise emissions do not exceed the stationary noise source criteria established in this analysis (i.e., 50 dB for 30 minutes in every hour during the daytime [7 a.m. to 10 p.m.] and less than 45 dB for 30 minutes of every hour during the night time [10 p.m. to 7 a.m.]). Reduction of loading dock noise can be achieved by locating loading docks as far away as possible from noise sensitive land uses, constructing noise barriers between loading docks and noise-sensitive land uses, or using buildings and topographic features to provide acoustic shielding for noise-sensitive land uses. 	<p>phase, and during project operations for testing of emergency generators.</p>	<p>all project phases.</p>	<p>Development Department.</p>	
<p>W/E SPA Mitigation Measure 4.12-1: Implement measures to prevent exposure of sensitive receptors to increases in noise from project-generated operational traffic on offsite and onsite roadways.</p> <p>Residential land uses with outdoor activity areas such as parks, playgrounds, swimming pools, yards, courtyards, or gardens, but not including private balconies, shall be set back a sufficient distance from U.S. 50 and other roadways</p>	<p>Before submittal of tentative maps for each project phase</p>	<p>Project applicant(s) of all project phases.</p>	<p>City of Folsom Community Development Department.</p>	

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<p>and/or incorporate design measures to ensure that the outdoor activity areas are not exposed to daily noise levels that exceed 60 dB CNEL in accordance with Policy 30.4 of the City of Folsom General Plan Noise Element. Design measures may include, but are not limited to, the installation of sound barriers and the orientation of buildings so they minimize exposure of freeway or roadway noise to outdoor activity areas. Proof of compliance with noise standards shall be submitted to the City prior to tentative map approval.</p> <p>Based on the traffic noise modelling results in Table 3A.11-18 of the FPASP EIR/EIS it's not possible to estimate how far of a setback from U.S. 50 would be needed if only a setback was implemented to achieve the performance standard of this mitigation measure. This is because the estimates in Table 3A.11-18 only show the noise level at the approximate road corridor boundary. Additional modeling would need to be performed prior to the tentative map submittal in order to ensure that the appropriate setback is incorporated into the plan.</p>				
<p>3A.14 Public Services</p> <p>Mitigation Measure 3A.14-1: Prepare and Implement a Construction Traffic Control Plan. *</p> <p>Before the beginning of construction, the applicant shall prepare a construction traffic and parking management plan to the satisfaction of the City Traffic Engineer and subject to review by affected agencies.</p> <p>The plan shall ensure that acceptable operating conditions on local roadways and freeway facilities are maintained. At a minimum, the plan shall include:</p> <ul style="list-style-type: none"> description of trucks including: number and size of trucks per day, expected arrival/departure times, truck circulation patterns; description of staging area including: location, maximum number of trucks simultaneously permitted in staging area, use of traffic control personnel, specific signage; description of street closures and/or bicycle and pedestrian facility closures including: duration, advance warning and posted signage, safe and efficient access routes for existing businesses and emergency vehicles, and use of manual traffic control; and description of driveway access plan including: provisions for safe vehicular, pedestrian, and bicycle travel, minimum distance from any open trench, special signage, and private vehicle accesses. <p>*This Mitigation Measure was revised in the W/E SPA Addendum to the FPASP EIR/EIS.</p>	<p>Before the approval of all relevant Plans and/or permits and during construction of all project phases.</p>	<p>Project applicant(s) of all project phases.</p>	<ol style="list-style-type: none"> For those roadways that would be annexed into the City of Folsom: City of Folsom Public Works Department. For those roadways that would remain under the control of Sacramento County: Sacramento County Department of Transportation. For the two off-site roadway connections into El Dorado Hills: El Dorado County Department of Transportation. For U.S. 50 interchange improvements: Caltrans. 	

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<p>Mitigation Measure 3A.14-2: Incorporate California Fire Code; City of Folsom Fire Code Requirements; and EDHFD Requirements, if Necessary, into Project Design and Submit Project Design to the City of Folsom Fire Department for Review and Approval.</p> <p>To reduce impacts related to the provision of new fire services, the project applicant(s) of all project phases shall do the following, as described below.</p> <ol style="list-style-type: none"> Incorporate into project designs fire flow requirements based on the California Fire Code, Folsom Fire Code (City of Folsom Municipal Code Title 8, Chapter 8.36), and other applicable requirements based on the City of Folsom Fire Department fire prevention standards. Improvement plans showing the incorporation automatic sprinkler systems, the availability of adequate fire flow, and the locations of hydrants shall be submitted to the City of Folsom Fire Department for review and approval. In addition, approved plans showing access design shall be provided to the City of Folsom Fire Department as described by Zoning Code Section 17.57.080 ("Vehicular Access Requirements"). These plans shall describe access-road length, dimensions, and finished surfaces for firefighting equipment. The installation of security gates across a fire apparatus access road shall be approved by the City of Folsom Fire Department. The design and operation of gates and barricades shall be in accordance with the Sacramento County Emergency Access Gates and Barriers Standard, as required by the City of Folsom Fire Code. Submit a Fire Systems New Buildings, Additions, and Alterations Document Submittal List to the City of Folsom Community Development Department Building Division for review and approval before the issuance of building permits. <p>In addition to the above measures, the project applicant(s) of all project phases shall incorporate the provisions described below for the portion of the SPA within the EDHFD service area, if it is determined through City/EI Dorado County negotiations that EDHFD would serve the 178-acre portion of the SPA.</p> <ol style="list-style-type: none"> Incorporate into project designs applicable requirements based on the EDHFD fire prevention standards. For commercial development, improvement plans showing roadways, land splits, buildings, fire sprinkler systems, fire alarm systems, and other commercial building improvements shall be submitted to the EDHFD for review and approval. For residential development, improvement plans showing property lines and adjacent streets or roads; total acreage or square footage of the parcel; the footprint of all structures; driveway plan views describing width, length, turnouts, turnarounds, radiuses, and surfaces; and driveway profile views showing the percent grade from the 	<p>Before issuance of building permits and issuance of occupancy permits or final inspections for all project phases.</p>	<p>Project applicant(s) of all project phases.</p>	<p>City of Folsom Fire Department, and City of Folsom Community Development Department, and/or EDHFD for the portion of the SPA within the EDHFD service area.</p>	

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<p>access road to the structure and vertical clearance shall be submitted to the EDHFD for review and approval.</p> <p>4. Submit a Fire Prevention Plan Checklist to the EDHFD for review and approval before the issuance of building permits. In addition, residential development requiring automation fire sprinklers shall submit sprinkler design sheet(s) and hydraulic calculations from a California State Licensed C-16 Contractor.</p> <p>The City shall not authorize the occupancy of any structures until the project applicant(s) have obtained a Certificate of Occupancy from the City of Folsom Community Development Department verifying that all fire prevention items have been addressed on-site to the satisfaction of the City of Folsom Fire Department and/or the EDHFD for the 178-acre area of the SPA within the EDHFD service area.</p>				
<p>Mitigation Measure 3A.14-3: Incorporate Fire Flow Requirements into Project Designs.</p> <p>The project applicant(s) of all project phases shall incorporate into their project designs fire flow requirements based on the California Fire Code, Folsom Fire Code, and/or EDHFD for those areas of the SPA within the EDHFD service area and shall verify to City of Folsom Fire Department that adequate water flow is available, prior to approval of improvement plans and issuance of occupancy permits or final inspections for all project phases.</p>	<p>Before issuance of building permits and issuance of occupancy permits or final inspections for all project phases.</p>	<p>Project applicant(s) of all project phases.</p>	<p>City of Folsom Fire Department, City of Folsom Community Development Department, and/or EDHFD for the 178-acre portion of the SPA within the EDHFD service area.</p>	
<p>3A.15 Traffic and Transportation</p> <p>Mitigation Measure 3A.15-1: Project Participation in Funding Transportation Improvements</p> <p>a. Within and adjacent to the project boundaries, the Applicant shall construct all feasible physical improvements necessary and available to reduce the severity of the project's significant transportation-related impacts, which may be subject to fee credits and/or reimbursement, coordinated by the City, from other fee-paying development projects if available with respect to roads or other facilities that would also serve those non-project fee-paying development projects. Funding of improvements on the perimeter of the project boundaries will be shared with other development/jurisdictions.</p> <p>b. Outside the project boundaries, the Applicant shall be responsible for the project's fair share of feasible physical improvements necessary and available to reduce the severity of the project's significant transportation-related impacts within the City of Folsom, in other jurisdictions and on State facilities, based on "cumulative plus project conditions." For purposes of this measure,</p>	<p>A phasing analysis shall be performed prior to approval of the first subdivision map to determine when the improvement should be implemented and when fair share funding should be paid.</p>	<p>City of Folsom Public Works Department.</p>	<p>City of Folsom Public Works Department.</p>	

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<p>"cumulative plus project conditions" refers to development authorized under the project as well as development consistent with approved general plans, specific plans, and other entitlements in the City and other jurisdictions. In cases where the project's fair share contribution is identified, the share will be based on the project's relative contribution to traffic growth under "cumulative plus project conditions." The project's contribution toward such improvements may take any, or some combination, of the following forms:</p> <ol style="list-style-type: none"> 1. Construction of roads, road improvements, or other transportation facilities outside the boundaries of the project, subject in some instances to fee credit against other improvements necessitated by the project or future reimbursement, coordinated by the City, from other fee-paying development projects if available where the roads or improvements at issue would also serve those non-project fee paying development projects; 2. The payment of impact fees to the City of Folsom in amounts that constitute the project's fair share contributions to the construction of transportation facilities to be built or improved within the City, consistent with the City's Capital Improvement Program ("CIP"); 3. The payment of other adopted regional impact fees that would provide improvements to roadways, intersections and/or interchanges that are affected by multiple jurisdictions, except where the project applicant's payments of other fees or construction of improvements within the City of Folsom creates credit against the payment of regional impact fees; 4. The payment of impact fees to the City of Folsom in amounts that constitute the project's fair share contributions to the construction of transportation facilities and/or improvements within affected jurisdictions outside of Folsom, which payments to the City of Folsom and transmittal of fees to other agencies would occur through one or more enforceable agreements provided that for each required improvement, there is a reasonable mitigation plan that ensures that (i) the fees collected from the project will be used for their intended purposes, and (ii) the improvements will actually be built within a reasonable period of time, and 5. The payment of impact fees to the City of Folsom in amounts that constitute the project's fair share contributions to the construction of transportation facilities and/or improvements on federal or state highways or freeways needed in part because of the project, to be made available to the California Department of Transportation ("Caltrans") if and when Caltrans and the City of Folsom enter into an enforceable agreement consistent with state law provided that, for each required improvement, 				

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<p>Caltrans has a reasonable mitigation plan that ensures that (i) the fees collected from the project will be used for their intended purposes, and (ii) the improvements will actually be built within a reasonable period of time.</p> <p>c. In pursuing a single agreement or multiple agreements with any jurisdictions outside of the City of Folsom that will be affected by traffic from the project in order to effectuate proposed mitigation measures for improvements outside the City of Folsom, the City will seek to negotiate in good faith with these other jurisdictions to enter into fair and reasonable arrangements with the intention of achieving, within a reasonable time period after approval of the project's, commitments for (i) the provision of adequate "fair share" mitigation payments from the project for out-of-jurisdiction traffic impacts and impacts on federal and state freeways and highways, and (ii) reciprocal payments from regional development projects to the City of Folsom to address cumulative "fair share" mitigation payments towards federal and state freeways and highways for transportation-related facilities and/or improvements within the City of Folsom necessitated by the development within the region. It is intended that these agreements shall permit the participating agencies flexibility in providing cross-jurisdictional credits and reimbursements consistent with the general "fair share" mitigation standard, and require an updated model run incorporating the best available information in order to obtain the most accurate, up-to-date impact assessment feasible and to generate the most accurate, up-to-date estimates of regional fair share contributions. Best efforts should be made to secure funding from federal, state and regional sources. These agreements, moreover, should also include provisions that allow for periodic updates to the traffic modeling on which fair share payment calculations depend in order to account for (i) newly approved projects cumulatively contributing to transportation-related impacts and that therefore should contribute to the funding of necessary improvements (ii) additional physical improvements necessitated in whole or in part by newly approved projects, and (iii) changing cost calculations for the construction of needed improvements based on changes in the costs of materials, labor, and other inputs.</p> <p>d. If transportation improvements required to be constructed as mitigation are constructed prior to project implementation, the project will pay its fair share portion for those improvements.</p> <p>e. In considering individual projects within the project area (e.g., small-lot tentative subdivision maps or similar discretionary non-residential approvals), the City of Folsom shall identify required improvements, and shall base its</p>				

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<p>calculations for such projects' fair share payments, based on the most recent traffic modeling (i.e., modeling that accounts for (i) newly approved projects cumulatively contributing to transportation-related impacts and that therefore should contribute to the funding of necessary improvements, (ii) additional physical improvements necessitated in whole or in part by newly approved projects, and (iii) changing cost calculations for the construction of needed improvements based on changes in the costs of materials, labor, and other inputs).</p>				
<p>Mitigation Measure 3A.15-1a: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Folsom Boulevard/Blue Ravine Road Intersection (Intersection 1). To ensure that the Folsom Boulevard/Blue Ravine Road intersection operates at an acceptable LOS, the eastbound approach must be reconfigured to consist of two left-turn lanes, one through lane, and one right-turn lane. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the Folsom Boulevard/Blue Ravine Road intersection (Intersection 1).</p>	<p>A phasing analysis shall be performed prior to approval of the first subdivision map to determine when the improvement should be implemented and when fair share funding should be paid.</p>	<p>City of Folsom Public Works Department.</p>	<p>City of Folsom Public Works Department.</p>	
<p>Mitigation Measure 3A.15-1b: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements at the Sibley Street/ Blue Ravine Road Intersection (Intersection 2). To ensure that the Sibley Street/Blue Ravine Road intersection operates at an acceptable LOS, the northbound approach must be reconfigured to consist of two left-turn lanes, two through lanes, and one right-turn lane. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the Sibley Street/Blue Ravine Road intersection (Intersection 2).</p>	<p>A phasing analysis shall be performed prior to approval of the first subdivision map to determine when the improvement should be implemented and when fair share funding should be paid.</p>	<p>City of Folsom Public Works Department.</p>	<p>City of Folsom Public Works Department.</p>	
<p>Mitigation Measure 3A.15-1c: The Applicant Shall Fund and Construct Improvements to the Scott Road (West)/White Rock Road Intersection (Intersection 28). To ensure that the Scott Road (West)/White Rock Road intersection operates at an acceptable LOS, a traffic signal must be installed.</p>	<p>A phasing analysis shall be performed prior to approval of the first subdivision map to determine when the</p>	<p>City of Folsom Public Works Department.</p>	<p>City of Folsom Public Works Department.</p>	

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<p>Mitigation Measure 3A.15-1e: Fund and Construct Improvements to the Hillside Drive/Easton Valley Parkway Intersection (Intersection 41). To ensure that the Hillside Drive/Easton Valley Parkway intersection operates at an acceptable LOS, the eastbound approach must be reconfigured to consist of one dedicated left turn lane and two through lanes, and the westbound approach must be reconfigured to consist of two through lanes and one dedicated right-turn lane. The applicant shall fund and construct these improvements.</p>	<p>Improvement should be implemented and when fair share funding should be paid.</p>	<p>City of Folsom Public Works Department.</p>	<p>City of Folsom Public Works Department.</p>	
<p>Mitigation Measure 3A.15-1f: Fund and Construct Improvements to the Oak Avenue Parkway/Middle Road Intersection (Intersection 44). To ensure that the Oak Avenue Parkway/Middle Road intersection operates at an acceptable LOS, control all movements with a stop sign. The applicant shall fund and construct these improvements.</p>	<p>A phasing analysis shall be performed prior to approval of the first subdivision map to determine when the improvement should be implemented and when fair share funding should be paid.</p>	<p>City of Folsom Public Works Department.</p>	<p>City of Folsom Public Works Department.</p>	
<p>Mitigation Measure 3A.15-1h: Participate in Fair Share Funding of Improvements to Reduce Impacts to the Hazel Avenue/Folsom Boulevard Intersection (Sacramento County Intersection 2). To ensure that the Hazel Avenue/Folsom Boulevard intersection operates at an acceptable LOS, this intersection must be grade separated including "Jug handle" ramps. No at grade improvement is feasible. Grade separating and extended (south) Hazel Avenue with improvements to the U.S. 50/Hazel Avenue Interchange is a mitigation measure for the approved Easton-Glenborough Specific Plan development project. The applicant shall pay its proportionate share of funding of</p>	<p>A phasing analysis shall be performed prior to approval of the first subdivision map to determine when the improvement should be implemented and when fair share</p>	<p>Sacramento County Public Works Department and Caltrans.</p>	<p>Sacramento County Public Works Department and Caltrans.</p>	

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<p>Improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the Hazel Avenue/Folsom Boulevard intersection (Sacramento County Intersection 2).</p>	<p>funding should be paid.</p>			
<p>Mitigation Measure 3A.15-1: Participate in Fair Share Funding of Improvements to Reduce Impacts on the Grant Line Road/White Rock Road Intersection and to White Rock Road widening between the Rancho Cordova City limit to Prairie City Road (Sacramento County Intersection 3). Improvements must be made to ensure that the Grant Line Road/White Rock Road intersection operates at an acceptable LOS. The currently County proposed White Rock Road widening project will widen and realign White Rock Road from the Rancho Cordova City limit to the El Dorado County line (this analysis assumes that the Proposed Project and build alternatives will widen White Rock Road to five lanes from Prairie City road to the El Dorado County Line). This widening includes improvements to the Grant Line Road intersection and realigning White Rock Road to be the through movement. The improvements include two eastbound through lanes, one eastbound right turn lane, two northbound left turn lanes, two northbound right turn lanes, two westbound left turn lanes and two westbound through lanes. This improvement also includes the signalization of the White Rock Road and Grant Line Road intersection. With implementation of this improvement, the intersection would operate at an acceptable LOS A. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the Grant Line Road/White Rock Road intersection (Sacramento County Intersection 3).</p>	<p>Before project build out. Design of the White Rock Road widening to four lanes, from Grant Line Road to Prairie City Road, with intersection improvements has begun, and because this widening project is environmentally cleared and fully funded, it's construction is expected to be complete before the first phase of the Proposed Project or alternative is built.</p>	<p>Sacramento County Public Works Department and Caltrans.</p>	<p>Sacramento County Public Works Department and Caltrans.</p>	
<p>Mitigation Measure 3A.15-1j: Participate in Fair Share Funding of Improvements to Reduce Impacts on Hazel Avenue between Madison Avenue and Curragh Downs Drive (Roadway Segment 10). To ensure that Hazel Avenue operates at an acceptable LOS between Curragh Downs Drive and Gold Country Boulevard, Hazel Avenue must be widened to six lanes. This improvement is part of the County adopted Hazel Avenue widening project.</p>	<p>Before project build out. Construction of phase two of the Hazel Avenue widening, from Madison Avenue to Curragh Downs Drive, is expected to be completed by year 2013, before the first phase of the Proposed Project or alternative is complete. The</p>	<p>Sacramento County Public Works Department and Caltrans.</p>	<p>Sacramento County Public Works Department and Caltrans.</p>	

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<p>Mitigation Measure 3A.15-11: Participate in Fair Share Funding of Improvements to Reduce Impacts on the White Rock Road/Windfield Way Intersection (El Dorado County Intersection 3).</p> <p>To ensure that the White Rock Road/Windfield Way intersection operates at an acceptable LOS, the intersection must be signalized and separate northbound left and right turn lanes must be striped. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the White Rock Road/Windfield Way intersection (El Dorado County Intersection 3).</p>	<p>applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Hazel Avenue between Madison Avenue and Curragh Downs Drive (Sacramento County Roadway Segment 10).</p>	<p>El Dorado County Department of Transportation.</p>	<p>El Dorado County Department of Transportation.</p>	
<p>Mitigation Measure 3A.15-10: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 as an Alternative to Improvements at the Folsom Boulevard/U.S. 50 Eastbound Ramps Intersection (Caltrans Intersection 4).</p> <p>Congestion on eastbound U.S. 50 is causing vehicles to use Folsom Boulevard as an alternate parallel route until they reach U.S. 50, where they must get back on the freeway due to the lack of a parallel route. It is preferred to alleviate the congestion on U.S. 50 than to upgrade the intersection at the end of this reliever route. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	

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<p>that agency to reduce the impacts to the Folsom Boulevard/U.S. 50 Eastbound Ramps intersection (Caltrans Intersection 4).</p> <p>To ensure that the Folsom Boulevard/U.S. 50 eastbound ramps intersection operates at an acceptable LOS, auxiliary lanes should be added to eastbound U.S. 50 from Hazel Avenue to east of Folsom Boulevard. This was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project.</p>				
<p>Mitigation Measure 3A.15-1p: Participate in Fair Share Funding of Improvements to Reduce Impacts on the Grant Line Road/ State Route 16 Intersection (Caltrans Intersection 12).</p> <p>To ensure that the Grant Line Road/State Route 16 intersection operates at an acceptable LOS, the northbound and southbound approaches must be reconfigured to consist of one left-turn lane and one shared through/right-turn lane. Protected left-turn signal phasing must be provided on the northbound and southbound approaches. Improvements to the Grant Line Road/State Route 16 intersection are contained within the County Development Fee Program, and are scheduled for Measure A funding.</p> <ul style="list-style-type: none"> Improvements to this intersection must be implemented by Caltrans, Sacramento County, and the City of Rancho Cordova. <p>The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the Grant Line Road/State Route 16 intersection (Caltrans Intersection 12).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Sacramento County Department of Transportation and the City of Rancho Cordova Department of Public Works</p>	<p>Sacramento County Department of Transportation and the City of Rancho Cordova Department of Public Works</p>	
<p>Mitigation Measure 3A.15-1q: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1).</p> <p>To ensure that Eastbound U.S. 50 operates at an acceptable LOS between Zinfandel Drive and Sunrise Boulevard, a bus-carpool (HOV) lane must be constructed. This improvement is currently planned as part of the Sacramento 50 Bus-Carpool Lane and Community Enhancements Project. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Eastbound U.S. 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1).</p>	<p>Before project build out. Construction of the Sacramento 50 Bus-Carpool Lane and Community Enhancements Project is expected to be completed by year 2013, before the first phase of the Proposed Project or alternative is complete. Construction of the Sacramento 50 Bus-Carpool Lane and</p>	<p>Caltrans</p>	<p>Caltrans</p>	

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<p>Mitigation Measure 3A.15-1r: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Hazel Avenue and Folsom Boulevard (Freeway Segment 3).</p> <p>To ensure that Eastbound U.S. 50 operates at an acceptable LOS between Hazel Avenue and Folsom Boulevard, an auxiliary lane must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project. This improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Eastbound U.S. 50 between Hazel Avenue and Folsom Boulevard (Freeway Segment 3).</p>	<p>Community Enhancements Project has started since the writing of the Draft EIS/EIR.</p> <p>Before project build out. A phasing analysis should be performed to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	
<p>Mitigation Measure 3A.15-1s: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Folsom Boulevard and Prairie City Road (Freeway Segment 4).</p> <p>To ensure that Eastbound U.S. 50 operates at an acceptable LOS between Folsom Boulevard and Prairie City Road, an auxiliary lane must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project. This improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to Eastbound U.S. 50 between Folsom Boulevard and Prairie City Road (Freeway Segment 4).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	
<p>Mitigation Measure 3A.15-1u: Participate in Fair Share Funding of Improvements to Reduce Impacts on Westbound U.S. 50 between Prairie City Road and Folsom Boulevard (Freeway Segment 15).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS between Prairie City Road and Folsom Boulevard, an auxiliary lane must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project. This improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	

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<p>funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to Westbound U.S. 50 between Prairie City Road and Folsom Boulevard (Freeway Segment 16).</p>	<p>the improvement should be built.</p>			
<p>Mitigation Measure 3A.15-1v: Participate in Fair Share Funding of Improvements to Reduce Impacts on Westbound U.S. 50 between Hazel Avenue and Sunrise Boulevard (Freeway Segment 18). To ensure that Westbound U.S. 50 operates at an acceptable LOS between Hazel Avenue and Sunrise Boulevard, an auxiliary lane must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project, and included in the proposed Rancho Cordova Parkway interchange project. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Westbound U.S. 50 between Hazel Avenue and Sunrise Boulevard (Freeway Segment 18).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	
<p>Mitigation Measure 3A.15-1w: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Folsom Boulevard Ramp Merge (Freeway Merge 4). To ensure that Eastbound U.S. 50 operates at an acceptable LOS at the Folsom Boulevard merge, an auxiliary lane from the Folsom Boulevard merge to the Prairie City Road diverge must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project. This improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the U.S. 50 Eastbound/Folsom Boulevard Ramp Merge (Freeway Merge 4).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	
<p>Mitigation Measure 3A.15-1x: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Diverge (Freeway Diverge 5). To ensure that Eastbound U.S. 50 operates at an acceptable LOS at the Prairie City Road off-ramp diverge, an auxiliary lane from the Folsom Boulevard merge must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	

Table 1: Mitigation Monitoring and Reporting Plan for the Westland/Eagle Area of the Folsom Plan Area Specific Plan
(The Westland/Eagle Specific Plan Amendment to the Folsom Plan Area Specific Plan was approved in September 2015)

Mitigation Measure	Timing	Implementation	Enforcement	Compliance/Notes
applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound/Prairie City Road diverge (Freeway Diverge 5).	the improvement should be built.			
Mitigation Measure 3A.15-1y: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Direct Merge (Freeway Merge 6). To ensure that Eastbound U.S. 50 operates at an acceptable LOS at the Prairie City Road on-ramp direct merge, an auxiliary lane to the East Bidwell Street – Scott Road diverge must be constructed. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound/Prairie City Road direct merge (Freeway Merge 6).	Before project build out: A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Public Works Department	City of Folsom Public Works Department	
Mitigation Measure 3A.15-1z: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Flyover On-Ramp to Oak Avenue Parkway Off-Ramp Weave (Freeway Weave 8). To ensure that Eastbound U.S. 50 operates at an acceptable LOS at the Prairie City Road flyover on-ramp to Oak Avenue Parkway off-ramp weave, an improvement acceptable to Caltrans should be implemented to eliminate the unacceptable weaving conditions. Such an improvement may involve a “braided ramp”. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound / Prairie City Road flyover on-ramp to Oak Avenue Parkway off-ramp weave (Freeway Weave 8).	Before project build out: A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Public Works Department	City of Folsom Public Works Department	
Mitigation Measure 3A.15-1aa: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Oak Avenue Parkway Loop Merge (Freeway Merge 9). To ensure that Eastbound U.S. 50 operates at an acceptable LOS at the Oak Avenue Parkway loop merge, an auxiliary lane to the East Bidwell Street – Scott Road diverge must be constructed. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to	Before project build out: A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase	City of Folsom Public Works Department	City of Folsom Public Works Department	

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<p>reduce the impacts to the U.S. 50 Eastbound/ Oak Avenue Parkway loop merge (Freeway Merge 9).</p> <p>Mitigation Measure 3A.15-1dd: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Empire Ranch Road Loop Ramp Merge (Freeway Merge 23).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS, the northbound Empire Ranch Road loop on ramp should start the westbound auxiliary lane that ends at the East Bidwell Street – Scott Road off ramp. The slip on ramp from southbound Empire Ranch Road would merge into this extended auxiliary lane. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Westbound/Empire Ranch Road loop ramp merge (Freeway Merge 23).</p>	<p>the improvement should be built.</p> <p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department</p>	<p>City of Folsom Public Works Department</p>	
<p>Mitigation Measure 3A.15-1ee: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Oak Avenue Parkway Loop Ramp Merge (Freeway Merge 29).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS, the northbound Oak Avenue Parkway loop on ramp should start the westbound auxiliary lane that ends at the Prairie City Road off ramp. The slip on ramp from southbound Oak Avenue Parkway would merge into this extended auxiliary lane. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Westbound/Oak Avenue Parkway loop ramp merge (Freeway Merge 29).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department</p>	<p>City of Folsom Public Works Department</p>	
<p>Mitigation Measure 3A.15-1ff: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Prairie City Road Loop Ramp Merge (Freeway Merge 32).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS at the Prairie City Road loop ramp merge, an auxiliary lane to the Folsom Boulevard off ramp diverge must be constructed. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	

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<p>reduce the impacts to the U.S. 50 Westbound/Prairie City Road Loop Ramp Merge (Freeway Merge 32).</p> <p>Mitigation Measure 3A.15-16g: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Prairie City Road Direct Ramp Merge (Freeway Merge 33).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS at the Prairie City Road direct ramp merge, an auxiliary lane to the Folsom Boulevard off ramp diverge must be constructed. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Westbound/Prairie City Road direct ramp merge (Freeway Merge 33).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	
<p>Mitigation Measure 3A.15-16h: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Folsom Boulevard Diverge (Freeway Diverge 34).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS at the Folsom Boulevard Diverge, an auxiliary lane from the Prairie City Road loop ramp merge must be constructed. Improvements to this freeway segment must be implemented by Caltrans. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound / Folsom Boulevard diverge (Freeway Diverge 34).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	
<p>Mitigation Measure 3A.15-16i: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Hazel Avenue Direct Ramp Merge (Freeway Merge 38).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS at the Hazel Avenue direct ramp merge, an auxiliary lane to the Sunrise Boulevard off ramp diverge must be constructed. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the U.S. 50 Westbound/Hazel Avenue direct ramp merge (Freeway Merge 38).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Sacramento County Department of Transportation and City of Rancho Cordova Department of Public Works</p>	<p>Sacramento County Department of Transportation and City of Rancho Cordova Department of Public Works</p>	

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<p>Mitigation Measure 3A.15-2a: Develop Commercial Support Services and Mixed-use Development Concurrent with Housing Development, and Develop and Provide Options for Alternative Transportation Modes.</p> <p>The project applicant(s) for any particular discretionary development application including commercial or mixed-use development concurrent with residential uses shall develop commercial and mixed-use development concurrent with housing development, to the extent feasible in light of market realities and other considerations, to internalize vehicle trips. Pedestrian and bicycle facilities shall be implemented to the satisfaction of the City Public Works Department. To further minimize impacts from the increased demand on area roadways and intersections, the project applicant(s) for any particular discretionary development application involving schools or commercial centers shall develop and implement safe and secure bicycle parking to promote alternative transportation uses and reduce the volume of single-occupancy vehicles using area roadways and intersections. The project applicant(s) for any particular discretionary development application shall participate in capital improvements and operating funds for transit service to increase the percent of travel by transit. The project's fair-share participation and the associated timing of the improvements and service shall be identified in the project conditions of approval and/or the project's development agreement. Improvements and service shall be coordinated, as necessary, with Folsom Stage Lines and Sacramento RT.</p>	<p>Before approval of improvement plans for all project phases for any particular discretionary development application that includes residential and commercial or mixed-use development. As a condition of project approval and/or as a condition of the development agreement for all project phases.</p>	<p>City of Folsom and Applicant(s)</p> <p>City of Folsom, Regional Transit, and Applicant(s)</p>	<p>City of Folsom Public Works Department.</p>	
<p>Mitigation Measure 3A.15-2b: Participate in the City's Transportation System Management Fee Program.</p> <p>The project applicant(s) for any particular discretionary development application shall pay an appropriate amount into the City's existing Transportation System Management Fee Program to reduce the number of single-occupant automobile travel on area roadways and intersections.</p>	<p>Concurrent with construction for all project phases.</p>	<p>City of Folsom and Applicant(s)</p>	<p>City of Folsom Public Works Department.</p>	
<p>Mitigation Measure 3A.15-2c: Participate with the 50 Corridor Transportation Management Association.</p> <p>The project applicant(s) for any particular discretionary development application shall join and participate with the 50 Corridor Transportation Management Association to reduce the number of single-occupant automobile travel on area roadways and intersections.</p>	<p>Concurrent with construction for all project phases.</p>	<p>50 Corridor Transportation Management Association and Applicant(s)</p>	<p>City of Folsom Public Works Department.</p>	
<p>Mitigation Measure 3A.15-3: Pay Full Cost of Identified Improvements that Are Not Funded by the City's Fee Program.</p>	<p>As a condition of project approval and/or as a condition</p>	<p>City of Folsom and Applicant(s)</p>	<p>City of Folsom Public Works Department.</p>	

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In accordance with Measure W, the project applicant(s) for any particular discretionary development application shall provide fair-share contributions to the City's transportation impact fee program to fully fund improvements only required because of the Specific Plan.	of the development agreement for all project phases.			
Mitigation Measure 3A.15-4a: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Sibley Street/Blue Ravine Road Intersection (Folsom Intersection 2). To ensure that the Sibley Street/Blue Ravine Road intersection operates at a LOS D with less than the Cumulative No Project delay, the northbound approach must be reconfigured to consist of two left-turn lane, two through lanes, and one dedicated right-turn lane. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the Sibley Street/Blue Ravine Road intersection (Folsom Intersection 2).	Before project build out: A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Public Works Department.	City of Folsom Public Works Department.	
Mitigation Measure 3A.15-4b: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Oak Avenue Parkway/East Bidwell Street Intersection (Folsom Intersection 6). To ensure that the Oak Avenue Parkway/East Bidwell Street intersection operates at an acceptable LOS, the eastbound (East Bidwell Street) approach must be reconfigured to consist of two left-turn lanes, four through lanes and a right-turn lane, and the westbound (East Bidwell Street) approach must be reconfigured to consist of two left-turn lanes, four through lanes, and a right-turn lane. It is against the City of Folsom policy to have eight lane roads because of the impacts to non-motorized traffic and adjacent development; therefore, this improvement is infeasible.				
Mitigation Measure 3A.15-4c: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the East Bidwell Street/College Street Intersection (Folsom Intersection 7). To ensure that the East Bidwell Street/College Street intersection operates at acceptable LOS C or better, the westbound approach must be reconfigured to consist of one left-turn lane, one left-through lane, and two dedicated right-turn lanes. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the East Bidwell Street/Nesmith Court intersection (Folsom Intersection 7).	Before project build out: A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Public Works Department.	City of Folsom Public Works Department	

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<p>Mitigation Measure 3A.15-4d: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the East Bidwell Street/Iron Point Road Intersection (Folsom Intersection 21).</p> <p>To ensure that the East Bidwell Street /Iron Point Road intersection operates at an acceptable LOS, the northbound approach must be reconfigured to consist of two left-turn lanes, four through lanes and a right-turn lane, and the southbound approach must be reconfigured to consist of two left-turn lanes, four through lanes and a right-turn lane. It is against the City of Folsom policy to have eight lane roads because of the impacts to non-motorized traffic and adjacent development; therefore, this improvement is infeasible.</p>		City of Folsom Public Works Department.	City of Folsom Public Works Department.	
<p>Mitigation Measure 3A.15-4e: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Serpa Way/ Iron Point Road Intersection (Folsom Intersection 23).</p> <p>To improve LOS at the Serpa Way/ Iron Point Road intersection, the northbound approaches must be restriped to consist of one left-turn lane, one shared left-through lanes, and one right-turn lane. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the Serpa Way/Iron Point Road Intersection (Folsom Intersection 23).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be build.</p>	City of Folsom Public Works Department.	City of Folsom Public Works Department.	
<p>Mitigation Measure 3A.15-4f: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Empire Ranch Road/Iron Point Road Intersection (Folsom Intersection 24).</p> <p>To ensure that the Empire Ranch Road / Iron Point Road intersection operates at a LOS D or better, all of the following improvements are required:</p> <ul style="list-style-type: none"> • The eastbound approach must be reconfigured to consist of one left-turn lane, two through lanes, and a right-turn lane. • The westbound approach must be reconfigured to consist of two left-turn lanes, one through lane, and a through-right lane. • The northbound approach must be reconfigured to consist of two left-turn lanes, three through lanes, and a right-turn lane. • The southbound approach must be reconfigured to consist of two left-turn lanes, three through lanes, and a right-turn lane. <p>The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the Empire Ranch Road / Iron Point Road Intersection (Folsom Intersection 24).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be build.</p>	City of Folsom Public Works Department.	City of Folsom Public Works Department.	

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<p>Mitigation Measure 3A.15-4g: The Applicant Shall Fund and Construct Improvements to the Oak Avenue Parkway/Easton Valley Parkway Intersection (Folsom Intersection 33).</p> <p>To ensure that the Oak Avenue Parkway/Easton Valley Parkway intersection operates at an acceptable LOS the southbound approach must be reconfigured to consist of two left-turn lanes, two through lanes, and two right-turn lanes. The applicant shall fund and construct these improvements.</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department.</p>	<p>City of Folsom Public Works Department.</p>	
<p>Mitigation Measure 3A.15-4i: Participate in Fair Share Funding of Improvements to Reduce Impacts on the Grant Line Road/White Rock Road Intersection (Sacramento County Intersection 3).</p> <p>To ensure that the Grant Line Road/White Rock Road intersection operates at an acceptable LOS E or better this intersection should be replaced by some type of grade separated intersection or interchange.</p> <p>Improvements to this intersection are identified in the Sacramento County's Proposed General Plan. Implementation of these improvements would assist in reducing traffic impacts on this intersection by providing acceptable operation. Intersection improvements must be implemented by Sacramento County. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the Grant Line Road/White Rock Road Intersection (Sacramento County Intersection 3).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Sacramento County Department of Transportation.</p>	<p>Sacramento County Department of Transportation.</p>	
<p>Mitigation Measure 3A.15-4j: Participate in Fair Share Funding of Improvements to Reduce Impacts on Grant Line Road between White Rock Road and Kiefer Boulevard (Sacramento County Roadway Segments 5-7).</p> <p>To improve operation on Grant Line Road between White Rock Road and Kiefer Boulevard, this roadway segment must be widened to six lanes. This improvement is proposed in the Sacramento County and the City of Rancho Cordova General Plans; however, it is not in the 2035 MTP. Improvements to this roadway segment must be implemented by Sacramento County and the City of Rancho Cordova. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Grant Line Road between White Rock Road and Kiefer Boulevard (Sacramento County Roadway Segments 5-7).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Sacramento County Department of Transportation.</p>	<p>Sacramento County Department of Transportation.</p>	

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<p>The identified improvement would more than offset the impacts specifically related to the Folsom South of U.S. 50 project on this roadway segment.</p> <p>Mitigation Measure 3A.15-4k: Participate in Fair Share Funding of Improvements to Reduce Impacts on Grant Line Road between Kiefer Boulevard and Jackson Highway (Sacramento County Roadway Segment 8).</p> <p>To improve operation on Grant Line Road between Kiefer Boulevard Jackson Highway, this roadway segment could be widened to six lanes. This improvement is proposed in the Sacramento County and the City of Rancho Cordova General Plans; however, it is not in the 2035 MTP. Improvements to this roadway segment must be implemented by Sacramento County and the City of Rancho Cordova. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Grant Line Road between Kiefer Boulevard and Jackson Highway (Sacramento County Roadway Segment 8).</p> <p>The identified improvement would more than offset the impacts specifically related to the Folsom South of U.S. 50 project on this roadway segment.</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Sacramento County Department of Transportation.</p>	<p>Sacramento County Department of Transportation.</p>	
<p>Mitigation Measure 3A.15-4l: Participate in Fair Share Funding of Improvements to Reduce Impacts on Hazel Avenue between Curragh Downs Drive and U.S. 50 Westbound Ramps (Sacramento County Roadway Segment 12-13).</p> <p>To improve operation on Hazel Avenue between Curragh Downs Drive and the U.S. 50 westbound ramps, this roadway segment could be widened to eight lanes. This improvement is inconsistent with Sacramento County's general plan because the county's policy requires a maximum roadway cross section of six lanes. Analysis shown later indicates that improvements at the impacted intersection in this segment can be mitigated (see Mitigation Measure 3A.15-4q). Improvements to impacted intersections on this segment will improve operations on this roadway segment and, therefore, mitigate this segment impact. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Hazel Avenue between Curragh Downs Drive and U.S. 50 Westbound Ramps (Sacramento County Roadway Segments 12-13).</p> <p>Mitigation Measure 3A.15-4m: Participate in Fair Share Funding of Improvements to Reduce Impacts on White Rock Road between Grant Line Road and Prairie City Road (Sacramento County Roadway Segment 22).</p> <p>To improve operation on White Rock Road between Grant Line Road and Prairie City Road, this roadway segment must be widened to six lanes. This improvement</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Sacramento County Department of Transportation.</p>	<p>Sacramento County Department of Transportation.</p>	

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<p>is included in the 2035 MTP but is not included in the Sacramento County General Plan. Improvements to this roadway segment must be implemented by Sacramento County.</p> <p>The identified improvement would more than offset the impacts specifically related to the Folsom South of U.S. 50 project on this roadway segment. However, because of other development in the region that would substantially increase traffic levels, this roadway segment would continue to operate at an unacceptable LOS F even with the capacity improvements identified to mitigate Folsom South of U.S. 50 impacts. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to White Rock Road between Grant Line Road and Prairie City Road (Sacramento County Roadway Segment 22).</p>	<p>subdivision map to determine during which project phase the improvement should be built.</p>			
<p>Mitigation Measure 3A.15-4n: Participate in Fair Share Funding of Improvements to Reduce Impacts on White Rock Road between Empire Ranch Road and Carson Crossing Road (Sacramento County Roadway Segment 28).</p> <p>To improve operation on White Rock Road between Empire Ranch Road and Carson Crossing Road, this roadway segment must be widened to six lanes. Improvements to this roadway segment must be implemented by Sacramento County. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to White Rock Road between Empire Ranch Road and Carson Crossing Road (Sacramento County Roadway Segment 28).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Sacramento County Department of Transportation.</p>	<p>Sacramento County Department of Transportation.</p>	
<p>Mitigation Measure 3A.15-4o: Participate in Fair Share Funding of Improvements to Reduce Impacts on the White Rock Road/Carson Crossing Road Intersection (El Dorado County 1).</p> <p>To ensure that the White Rock Road/Carson Crossing Road intersection operates at an acceptable LOS, the eastbound right turn lane must be converted into a separate free right turn lane, or double right. Improvements to this intersection must be implemented by El Dorado County. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the White Rock Road/Carson Crossing Road Intersection (El Dorado County 1).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>El Dorado County Department of Public Works.</p>	<p>El Dorado County Department of Public Works.</p>	

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Mitigation Measure	Timing	Implementation	Enforcement	Compliance/Notes
<p>Mitigation Measure 3A.15-4p: Participate in Fair Share Funding of Improvements to Reduce Impacts on the Hazel Avenue/U.S. 50 Westbound Ramps Intersection (Caltrans Intersection 1).</p> <p>To ensure that the Hazel Avenue/U.S. 50 westbound ramps intersection operates at an acceptable LOS, the westbound approach must be reconfigured to consist of one dedicated left turn lane, one shared left-through lane and three dedicated right-turn lanes. Improvements to this intersection must be implemented by Caltrans and Sacramento County. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the Hazel Avenue/U.S. 50 Westbound Ramps Intersection (Caltrans Intersection 1).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Sacramento County Department of Transportation.</p>	<p>Sacramento County Department of Transportation.</p>	
<p>Mitigation Measure 3A.15-4q: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1).</p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS between Zinfandel Drive and Sunrise Boulevard, an additional eastbound lane could be constructed. This improvement is not consistent with the Concept Facility in Caltrans State Route 50 Corridor System Management Plan; therefore, it is not likely to be implemented by Caltrans by 2030.</p> <p>Construction of the Capitol South East Connector, including widening White Rock Road and Grant Line Road to six lanes with limited access, could divert some traffic from U.S. 50 and partially mitigate the project's impact. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Eastbound U.S. 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Capitol Southeast Connector Joint Powers Authority.</p>	<p>Capitol Southeast Connector Joint Powers Authority.</p>	
<p>Mitigation Measure 3A.15-4r: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Rancho Cordova Parkway and Hazel Avenue (Freeway Segment 3).</p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS between Rancho Cordova Parkway and Hazel Avenue, an additional eastbound lane could be constructed. This improvement is not consistent with the Concept Facility in Caltrans State Route 50 Corridor System Management Plan; therefore, it is not likely to be implemented by Caltrans by 2030.</p> <p>Construction of the Capitol South East Connector, including widening White Rock Road and Grant Line Road to six lanes with limited access, could divert some traffic off of U.S. 50 and partially mitigate the project's impact. The applicant shall</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Capitol Southeast Connector Joint Powers Authority.</p>	<p>Capitol Southeast Connector Joint Powers Authority.</p>	

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<p>pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Eastbound U.S. 50 between Rancho Cordova Parkway and Hazel Avenue (Freeway Segment 3).</p> <p>Mitigation Measure 3A.15-4s: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Folsom Boulevard and Prairie City Road (Freeway Segment 5).</p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS between Folsom Boulevard and Prairie City Road, the eastbound auxiliary lane should be converted to a mixed flow lane that extends to and drops at the Oak Avenue Parkway off ramp (see mitigation measure 3A.15-4t). Improvements to this freeway segment must be implemented by Caltrans. This improvement is not consistent with the Concept Facility in Caltrans State Route 50 Corridor System Management Plan; therefore, it is not likely to be implemented by Caltrans by 2030. Construction of the Capitol South East Connector, including widening White Rock Road and Grant Line Road to six lanes with limited access, could divert some traffic off of U.S. 50 and partially mitigate the project's impact.</p> <p>The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to Eastbound U.S. 50 between Folsom Boulevard and Prairie City Road (Freeway Segment 5).</p> <p>Mitigation Measure 3A.15-4t: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Prairie City Road and Oak Avenue Parkway (Freeway Segment 6).</p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS between Prairie City Road and Oak Avenue Parkway, the northbound Prairie City Road slip on ramp should merge with the eastbound auxiliary lane that extends to and drops at the Oak Avenue Parkway off ramp (see Mitigation Measures 3A.15-4u, v and w), and the southbound Prairie City Road flyover on ramp should be braided over the Oak Avenue Parkway off ramp and start an extended full auxiliary lane to the East Bidwell Street – Scott Road off-ramp. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to Eastbound U.S. 50 between Prairie City Road and Oak Avenue Parkway (Freeway Segment 6).</p>	<p>Before project build out: A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Capitol Southeast Connector Joint Powers Authority.</p>	<p>Capitol Southeast Connector Joint Powers Authority.</p>	
		<p>City of Folsom Public Works Department</p>	<p>City of Folsom Public Works Department</p>	

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<p>Mitigation Measure 3A.15-4u: Participate in Fair Share Funding of Improvements to Reduce Impacts on the U.S. 50 Eastbound / Prairie City Road Slip Ramp Merge (Freeway Merge 6).</p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS, the northbound Prairie City Road slip on ramp should start the eastbound auxiliary lane that extends to and drops at the Oak Avenue Parkway off ramp (see mitigation measure 3A.15-4u, w and x), and the southbound Prairie City Road flyover on ramp should be braided over the Oak Avenue Parkway off ramp and start an extended full auxiliary lane to the East Bidwell Street – Scott Road off ramp. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound / Prairie City Road slip ramp merge (Freeway Merge 6).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department</p>	<p>City of Folsom Public Works Department</p>	
<p>Mitigation Measure 3A.15-4v: Participate in Fair Share Funding of Improvements to Reduce Impacts on the U.S. 50 Eastbound / Prairie City Road Flyover On Ramp to Oak Avenue Parkway Off Ramp Weave (Freeway Weave 7).</p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS, the northbound Prairie City Road slip on ramp should start the eastbound auxiliary lane that extends to and drops at the Oak Avenue Parkway off ramp (see mitigation measure 3A.15-4u, v and x), and the southbound Prairie City Road flyover on ramp should be braided over the Oak Avenue Parkway off ramp and start an extended full auxiliary lane to the East Bidwell Street – Scott Road off ramp. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound / Prairie City Road Flyover On Ramp to Oak Avenue Parkway Off Ramp Weave (Freeway Weave 7).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department</p>	<p>City of Folsom Public Works Department</p>	
<p>Mitigation Measure 3A.15-4w: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound / Oak Avenue Parkway Loop Ramp Merge (Freeway Merge 8).</p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS, the southbound Oak Avenue Parkway loop on ramp should merge with the eastbound auxiliary lane that starts at the southbound Prairie City Road braided flyover on ramp and ends at the East Bidwell Street – Scott Road off ramp (see mitigation measure 3A.15-4u, v and w). Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase</p>	<p>City of Folsom Public Works Department</p>	<p>City of Folsom Public Works Department</p>	

<p align="center">Table 1: Mitigation Monitoring and Reporting Plan for the Westland/Eagle Area of the Folsom Plan Area Specific Plan (The Westland/Eagle Specific Plan Amendment to the Folsom Plan Area Specific Plan was approved in September 2015)</p>				
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<p>Improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to U.S. 50 Eastbound / Oak Avenue Parkway Loop Ramp Merge (Freeway Merge 8).</p>	<p>the improvement should be built.</p>			
<p>Mitigation Measure 3A.15-4x: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound / Empire Ranch Road Loop Ramp Merge (Freeway Merge 27).</p> <p>To ensure that Westbound US 50 operates at an acceptable LOS, the northbound Empire Ranch Road loop on ramp should start the westbound auxiliary lane that ends at the East Bidwell Street – Scott Road off ramp. The slip on ramp from southbound Empire Ranch Road slip ramp would merge into this extended auxiliary lane. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Westbound / Empire Ranch Road loop ramp merge (Freeway Merge 27).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department</p>	<p>City of Folsom Public Works Department</p>	
<p>Mitigation Measure 3A.15-4y: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound / Prairie City Road Loop Ramp Merge (Freeway Merge 35).</p> <p>To ensure that Westbound US 50 operates at an acceptable LOS, the northbound Prairie City Road loop on ramp should start the westbound auxiliary lane that continues beyond the Folsom Boulevard off ramp. The slip on ramp from southbound Prairie City Road slip ramp would merge into this extended auxiliary lane. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Westbound / Prairie City Road Loop Ramp Merge (Freeway Merge 35).</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	<p>City of Folsom Public Works Department and Sacramento County Department of Transportation</p>	
<p>W/E SPA Mitigation Measure 4.16-1: Participate in Fair Share Funding of modification of the Iron Point Road/East Bidwell Street Intersection.</p> <p>The project applicant shall pay a fair share fee towards modifying the westbound approach to include three left-turn lanes, two through lanes, and one right-turn lane at the Iron Point Road/East Bidwell Street intersection.</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase</p>	<p>City of Folsom Public Works Department</p>	<p>City of Folsom Public Works Department</p>	

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<p>W/E SPA Mitigation Measure 4.16-2: Participate in Fair Share Funding of Improvements to the Scott Road/Easton Valley Parkway Intersection. Project Applicant shall pay a fair share fee towards improvements to the Scott Road/Easton Valley Parkway Intersection: Provide right-of-way and add a channelized westbound right-turn lane.</p>	<p>the improvement should be built. Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>City of Folsom Public Works Department</p>	<p>City of Folsom Public Works Department</p>	
<p>3A.16 Utilities and Service Systems</p> <p>Mitigation Measure 3A.16-1: Submit Proof of Adequate On- and Off-Site Wastewater Conveyance Facilities and Implement On- and Off-Site Infrastructure Service Systems or Ensure That Adequate Financing Is Secured. Before the approval of the final map and issuance of building permits for all project phases, the project applicant(s) of all project phases shall submit proof to the City of Folsom that an adequate wastewater conveyance system either has been constructed or is ensured through payment of the City's facilities augmentation fee as described under the Folsom Municipal Code Title 3, Chapter 3.40, "Facilities Augmentation Fee – Folsom South Area Facilities Plan," or other sureties to the City's satisfaction. Both on-site wastewater conveyance infrastructure and off-site force main sufficient to provide adequate service to the project shall be in place for the amount of development identified in the tentative map before approval of the final map and issuance of building permits for all project phases, or their financing shall be ensured to the satisfaction of the City.</p> <p>Mitigation Measure 3A.16-3: Demonstrate Adequate SRWTP Wastewater Treatment Capacity. The project applicant(s) of all project phases shall demonstrate adequate capacity at the SRWTP for new wastewater flows generated by the project. This shall involve preparing a tentative map-level study and paying connection and capacity fees as identified by SRCSO. Approval of the final map and issuance of building permits for all project phases shall not be granted until the City verifies adequate</p>	<p>Before approval of final maps and issuance of building permits for any project phases.</p>	<p>The project applicant(s) of all project phases.</p>	<p>City of Folsom Community Development Department and City of Folsom Public Works Department.</p>	
<p>Mitigation Measure 3A.16-3: Demonstrate Adequate SRWTP Wastewater Treatment Capacity. The project applicant(s) of all project phases shall demonstrate adequate capacity at the SRWTP for new wastewater flows generated by the project. This shall involve preparing a tentative map-level study and paying connection and capacity fees as identified by SRCSO. Approval of the final map and issuance of building permits for all project phases shall not be granted until the City verifies adequate</p>	<p>Before approval of final maps and issuance of building permits for any project phases.</p>	<p>The project applicant(s) of all project phases.</p>	<p>City of Folsom Community Development Department and City of Folsom Public Works Department.</p>	

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SRWTP capacity is available for the amount of development identified in the tentative map.				
<p>Mitigation Measure 3A.16-4: Submit Proof of Adequate EID Off-Site Wastewater Conveyance Facilities and Implement EID Off-Site Infrastructure Service Systems or Ensure That Adequate Financing Is Secured.</p> <p>Before the approval of the final map and issuance of building permits for all project phases, the project applicant(s) of all project phases shall obtain proof from EID that an adequate wastewater conveyance system either has been constructed or is ensured through the use of bonds or other sureties. The project applicants of all project phases shall submit this proof to the City of Folsom. EID off-site wastewater conveyance infrastructure sufficient to provide adequate service to project shall be in place for the amount of development identified in the tentative map before approval of the final map and issuance of building permits for all project phases, and before issuance of occupancy permits, or their financing shall be ensured to the satisfaction of the City.</p>	Before approval of final maps and issuance of building permits for any project phase	The project applicant(s) of all project phases.	City of Folsom Community Development Department and City of Folsom Public Works Department.	
<p>Mitigation Measure 3A.16-5: Demonstrate Adequate El Dorado Hills Wastewater Treatment Plant Capacity.</p> <p>The project applicant(s) of all project phases shall demonstrate adequate capacity at the El Dorado Hills WWTP for new wastewater flows generated by project development. This shall involve preparing a tentative map-level study and paying connection and capacity fees as identified by EID. Approval of the final map and issuance of building permits for all project phases shall not be granted until the City verifies adequate El Dorado Hills WWTP capacity is available for the amount of development identified in the tentative map.</p>	Before approval of final maps and issuance of building permits for any project phases involving the El Dorado Hills WWTP.	The project applicant(s) of all project phases.	City of Folsom Community Development Department and City of Folsom Public Works Department.	
<p>3A.18 Water Supply</p> <p>Mitigation Measure 3A.18-1: Submit Proof of Surface Water Supply Availability.</p> <p>a. Prior to approval of any small-lot tentative subdivision map subject to Government Code Section 66473.7 (58 221), the City shall comply with that statute. Prior to approval of any small-lot tentative subdivision map for a proposed residential project not subject to that statute, the City need not comply with Section 66473.7, or formally consult with any public water system that would provide water to the affected area; nevertheless, the City shall make a factual showing or impose conditions similar to those required by Section 66473.7 to ensure an adequate water supply for development authorized by the map.</p>	Before approval of final maps and issuance of building permits for any project phases.	The project applicant(s) of all project phases.	City of Folsom Community Development Department and City of Folsom Public Works Department.	

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<p>b. Prior to recordation of each final subdivision map, or prior to City approval of any similar project-specific discretionary approval or entitlement required for nonresidential uses, the project applicant(s) of that project phase or activity shall demonstrate the availability of a reliable and sufficient water supply from a public water system for the amount of development that would be authorized by the final subdivision map or project-specific discretionary nonresidential approval or entitlement. Such a demonstration shall consist of information showing that both existing sources are available or needed supplies and improvements will be in place prior to occupancy.</p>				
<p>Mitigation Measure 3A.18-2a: Submit Proof of Adequate Off-Site Water Conveyance Facilities and Implement Off-Site Infrastructure Service System or Ensure That Adequate Financing Is Secured.</p> <p>Before the approval of the final subdivision map and issuance of building permits for all project phases, the project applicant(s) of any particular discretionary development application shall submit proof to the City of Folsom that an adequate off-site water conveyance system either has been constructed or is ensured or other sureties to the City's satisfaction. The off-site water conveyance infrastructure sufficient to provide adequate service to the project shall be in place for the amount of development identified in the tentative map before approval of the final subdivision map and issuance of building permits for all project phases, or their financing shall be ensured to the satisfaction of the City. A certificate of occupancy shall not be issued for any building within the SPA until the water conveyance infrastructure sufficient to serve such building has been constructed and is in place.</p>	<p>Before approval of final maps and issuance of building permits for any project phases.</p>	<p>The project applicant(s) for any particular discretionary development application.</p>	<p>City of Folsom Community Development Department and City of Folsom Public Works Department.</p>	
<p>Mitigation Measure 3A.18-2b: Demonstrate Adequate Off-Site Water Treatment Capacity (if the Off-Site Water Treatment Plant Option is Selected).</p> <p>If an off-site water treatment plant (WTP) alternative is selected (as opposed to the on-site WTP alternative), the project applicant(s) for any particular discretionary development application shall demonstrate adequate capacity at the off-site WTP. This shall involve preparing a tentative map-level study and paying connection and capacity fees as determined by the City. Approval of the final project map shall not be granted until the City verifies adequate water treatment capacity either is available or is certain to be available when needed for the amount of development identified in the tentative map before approval of the final map and issuance of building permits for all project phases. A certificate of occupancy shall not be issued for any building within the SPA until the water</p>	<p>Before approval of final maps and issuance of building permits for any project phases.</p>	<p>The project applicant(s) for any particular discretionary development application.</p>	<p>City of Folsom Community Development Department and City of Folsom Public Works Department.</p>	

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<p>Cumulative Mitigation Measure AIR-1-Land: Implement East Sacramento Regional Aggregate Mining Truck Management Plan or Other Measures to Reduce Exposure of Sensitive Receptors to Operational Emissions of Toxic Air Contaminants from Quarry Truck Traffic.</p> <p>The City of Folsom is a participant in the development of an East Sacramento Regional Aggregate Mining Truck Management Plan (TMP), a cooperative effort led by the County of Sacramento, with the input of the City of Folsom, the City of Rancho Cordova and other interested parties, including representatives of quarry project applicants. When the County Board of Supervisors approved entitlements for the Teichert quarry project in November 2010, it also adopted conditions of approval and a development agreement that requires Teichert's participation in, and fair share funding of, a TMP to implement roadway capacity and safety improvements required to improve the compatibility of truck traffic from the quarries with the future urban development in the Folsom Specific Plan area and other jurisdictions that will be affected by quarry truck traffic. The development agreement adopted by the County for the Teichert project imposes limits on the amounts of annual aggregate sales from Teichert's facility until a TMP is adopted. The City of Folsom does not have direct jurisdiction over the Teichert, Desilva Gates, or Wallowan quarry project applicants as these projects are located within the unincorporated portion of the County. The County, as the agency with the primary authority over the quarries, has indicated that it intends to prepare an environmental analysis in accordance with CEQA prior to adoption of a TMP. The City's authority to control the activities of the quarry trucks includes restrictions or other actions, such as the approval and implementation of specialized road improvements to accommodate quarry truck traffic, that would be applicable within the City's jurisdictional boundaries. For the foregoing reasons, the City of Folsom considers itself a "responsible agency" (as that term is defined at State CEQA Guidelines, CCR Section 15381), in that it has some discretionary power over some elements of a future TMP, if such TMP calls for improvements or other activities on roadways within the jurisdiction of the City. In a responsible agency role, the City would follow the process specified in the CEQA Guidelines for consideration and approval of the environmental analysis prepared by the County for a TMP after such documentation is prepared and adopted by the County. (State CEQA Guidelines, CCR Section 15096.)</p>	<p>Prior to approval of first tentative map or discretionary approval within SPA that would place sensitive receptors along roadways that quarry trucks would reasonably use to access U.S. Highway 50.</p>	<p>The project applicant(s) of the Folsom South of U.S. 50 Specific Plan project.</p>	<p>City of Folsom Community Development Department.</p>	
<p>treatment capacity sufficient to serve such building has been constructed and is in place.</p>				
<p>Cumulative</p>				

Westland/Eagle Specific Plan Amendment to the Folsom Plan Area Specific Plan
Mitigation Monitoring and Reporting Program

May 2011, Revised November 2012, Revised September 2015
City of Folsom

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<p>Because no final project description for a TMP has been developed as of the completion of this FEIR/FEIS, the City would have to speculate as to those portions of a TMP that might be proposed for implementation within its jurisdiction, or the impacts that could arise from the implementation of as-yet uncertain components. Accordingly, formulation of the precise means of mitigating the potential cumulative air quality impacts pursuant to the TMP is not currently feasible or practical. However, as the preferred, feasible, and intended mitigation strategy to address the cumulative impacts of quarry truck traffic through the SPA, the City shall implement, or cause to be implemented those portions of the TMP (as described above) that are within its authority to control. In implementing the TMP, the City shall ensure that the TMP or traffic measures imposed by the City within the SPA reduce the risk of cancer to sensitive receptors along routes within the SPA from toxic air contaminant emissions to no more than 296 in one million (SMAQMD 2009. March. Recommended Protocol for Evaluating the Location of Sensitive Land Uses Adjacent to Major Roadways, Version 2.2:7), or such different threshold of significance mandated by SMAQMD or ARB at the time, if any. With this mitigation, the cumulative air quality impacts from truck toxic air contaminants would be less than significant.</p> <p>As an alternative (or in addition) to implementing the TMP within the SPA, the following measures could (and should) be voluntarily implemented by the quarry project applicant(s) (Teichert, Desilva Gates, and Granite [Walltown]) to help ensure exposure of sensitive receptors to TACs generated by quarry truck traffic to the 296-in-one-million threshold of significance identified above. The City encourages implementation of the following measures:</p> <ul style="list-style-type: none"> • The quarry project applicant(s) should meet with the City of Folsom to discuss mitigation strategies, implementation, and cost. • A site-specific, project-level screening analysis and/or Health Risk Assessment (HRA) should be conducted by the City of Folsom and funded by the truck applicant(s) for all proposed sensitive receptors (e.g., residences, schools) in the SPA that would be located along the sides of roadway segments that are identified in Table 4-4 as being potentially significant under any of the analyzed scenarios. Each project-level analysis shall be performed according to the standards set forth by SMAQMD for the purpose of disclosure to the public and decision makers. The project-level analysis shall account for the location of the receptors relative to the roadway, their distance from the roadway, the projected future traffic volume for the year 2030 (including the proportion of diesel trucks), and emission rates representative of the vehicle fleet for the year when the sensitive land uses would first become operational and/or occupied. If 				

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<p>the incremental increase in cancer risk determined by in the HRA exceeds 296 in one million (or a different threshold of significance recommended by SMAQMD or ARB at the time, if any), then project design mitigation should be employed, which may include the following:</p> <ul style="list-style-type: none"> ○ Increase the setback distance between the roadway and affected receptor. If this mitigation measure is determined by the City of Folsom to be necessary, based on the results of the HRA, the quarry truck applicant(s) should pay the Folsom South of U.S. 50 Specific Plan project applicant(s) and the City of Folsom a fee that shall serve as compensation for lost development profit and lost City tax revenues, all as determined by the parties. Said mitigation fee shall be determined in consultation with the quarry project applicant(s), the Folsom South of U.S. 50 Specific Plan project applicant(s), and the City of Folsom. No quarry trucks shall be allowed to pass on any roadway segment immediately adjacent to or within the SPA until said mitigation fees are paid. ○ Implement tiered tree planting of fine-needle species, such as redwood, along the near side of the roadway segments and, if feasible, along the roadway 500 feet in both directions of the initial planting (e.g., 500 feet north and south of a roadway that runs east-west) to enhance the dispersion and filtration of mobile-source TACS associated with the adjacent roadway. These trees should be planted at a density such that a solid visual buffer is achieved after the trees reach maturity, which breaks the line of sight between U.S. 50 and the proposed homes. These trees should be planted before occupation of any affected sensitive land uses. This measure encourages the planting of these trees in advance of the construction of potentially affected receptors to allow the trees to become established and progress toward maturity. The life of these trees should be maintained through the duration of the quarry projects. The planting, cost, and ongoing maintenance of these trees should be funded by the quarry project applicant(s). ○ To improve the indoor air quality at affected receptors, implement the following measures before the occupancy of the affected residences and schools: <ul style="list-style-type: none"> ○ equip all affected residences and school buildings developed in the SPA with High Efficiency Particle Arresting (HEPA) filter systems at all mechanical air intake points to the interior rooms; ○ use the heating, ventilation, and air conditioning (HVAC) systems to maintain all residential units under positive pressure at all times; 				

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<p>o locate air intake systems for HVAC as far away from roadway air pollution sources as possible; and</p> <p>o develop and implement an ongoing education and maintenance plan about the filtration systems associated with HVAC for residences and schools.</p> <p>To the extent this indoor air quality mitigation would not already be implemented as part of the Folsom South of U.S. 50 Specific Plan project development, this mitigation should be paid for by the quarry project applicant(s) before any quarry trucks are allowed to pass on any roadway that is within 400 feet of any residence or school within the SPA.</p>				
<p>Cumulative Mitigation Measure NOISE-1-Land: Implement East Sacramento Regional Aggregate Mining Truck Management Plan or Other Measures to Reduce Exposure of Sensitive Receptors to Operational Noise from Quarry Truck Traffic.</p> <p>The City of Folsom is a participant in the development of an East Sacramento Regional Aggregate Mining Truck Management Plan (TMP), a cooperative effort led by the County of Sacramento, with the input of the City of Folsom, the City of Rancho Cordova and other interested parties, including representatives of quarry project applicants. When the County Board of Supervisors approved entitlements for the Teichert quarry project in November 2010, it also adopted conditions of approval and a development agreement that requires Teichert's participation in, and fair share funding of, a TMP to implement roadway capacity and safety improvements required to improve the compatibility of truck traffic from the quarries with the future urban development in the SPA and other jurisdictions that will be affected by quarry truck traffic. The development agreement adopted by the County for the Teichert project imposes limits on the amounts of annual aggregate sales from Teichert's facility until a TMP is adopted. The City of Folsom does not have direct jurisdiction over the Teichert, DeSilva Gates, or Walltown quarry project applicants as these projects are located within the unincorporated portion of the County. The County, as the agency with the primary authority over the quarries, has indicated that it intends to prepare an environmental analysis in accordance with CEQA prior to adoption of a TMP. The City's authority to control the activities of the quarry trucks includes restrictions or other actions, such as the approval and implementation of specialized road improvements to accommodate quarry truck traffic, that would be applicable within the City's jurisdictional boundaries. For the foregoing reasons, the City of Folsom considers itself a "responsible agency" (as that term is defined at State CEQA Guidelines, CCR Section 15381), in that it has some discretionary power over some elements of a</p>	<p>Prior to approval of first tentative map or discretionary approval within SPA that would place sensitive receptors along roadways that quarry trucks would reasonably use to access U.S. 50.</p>	<p>The project applicant(s) of the Folsom South of U.S. 50 Specific Plan project.</p>	<p>City of Folsom Community Development Department.</p>	

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<p>future TMP, if such TMP calls for improvements or other activities on roadways within the jurisdiction of the City. In a responsible agency role, the City would follow the process specified in the CEQA Guidelines for consideration and approval of the environmental analysis prepared by the County for a TMP after such documentation is prepared and adopted by the County. (State CEQA Guidelines, CCR Section 15096.)</p> <p>Because no final project description for a TMP has been developed as of the completion of this FEIR/EIS, the City would have to speculate as to those portions of a TMP that might be proposed for implementation within its jurisdiction, or the impacts that could arise from the of as-yet uncertain components. Accordingly, formulation of the precise means of mitigating the potential cumulative noise impacts pursuant to the TMP is not currently feasible or practical. However, as the preferred, feasible, and intended mitigation strategy to address the cumulative impacts of quarry truck traffic through the SPA, the City shall implement, or cause to be implemented those portions of the TMP (as described above) that are within its authority to control. In implementing the TMP, the City shall ensure that the TMP or traffic measures imposed by the City within the SPA reduce the traffic noise exposure to sensitive receptors along routes within the SPA so as to ensure that sensitive receptors are not exposed to interior noise levels in excess of 45 dBA, or increases in interior noise levels of 3 dBA or more, whichever is more restrictive. With this mitigation, the cumulative noise impacts from truck traffic would be less than significant.</p> <p>As an alternative (or in addition) to implementing the TMP within the SPA, the following measures could (and should) be voluntarily implemented by the quarry project applicant(s) (Teichert, Desilva Gates, and Granite [Waltown]) to help ensure interior noise levels for sensitive receptors to noise generated by quarry truck traffic would not exceed 45 dBA or increase of 3 dBA over existing conditions, as identified above. The City encourages implementation of the following measures:</p> <ul style="list-style-type: none"> • The quarry project applicant(s) should meet with the City of Folsom to discuss mitigation strategies, implementation, and cost. • A site-specific, project-level screening analysis should be conducted by the City of Folsom and funded by the quarry truck applicant(s) for all proposed sensitive receptors (e.g., residences, schools) in the SPA that would be located along the sides of roadway segments that are identified in Table 4-8 as being potentially significant under any of the analyzed scenarios. The analysis should be conducted using an approved three dimensional traffic noise modeling program (i.e., TNM or Soundplan). Each project-level analysis should be performed 				

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<p>according to the standards set forth by the City of Folsom for the purpose of disclosure to the public and decision makers. The project-level analysis should account for the location of the receptors relative to the roadway, their distance from the roadway, and the projected future traffic volume for the year 2030 (including the percentage of heavy trucks). If the incremental increase in traffic noise levels are determined to exceed the threshold of significance recommended by the City of Folsom, then design mitigation should be employed, which may include the following:</p> <ul style="list-style-type: none"> • Model the benefits of soundwalls (berm/wall combination) along the quarry truck hauling roadways and affected receptors not to exceed a total height of eight feet (two-foot berm and six-foot concrete mason wall). If this mitigation measure is determined by the City of Folsom to be inadequate, additional three dimensional traffic noise modeling should be conducted with the inclusion of rubberized asphalt at the expense of the quarry truck applicant(s). No quarry trucks should be allowed to pass on any roadway segment immediately adjacent to or within the SPA until said mitigation has been agreed upon by the City of Folsom and fees for construction of said mitigation are paid by the quarry truck applicant(s). • Implement the installation of rubberized asphalt (quiet pavement) on roadway segments adjacent to sensitive receptors that carry quarry trucks if soundwalls do not provide adequate reduction of traffic noise levels. The inclusion of rubberized asphalt would provide an additional 3 to 5 dB of traffic noise reduction. The cost of construction using rubberized asphalt should be borne by the quarry truck applicant(s). Said mitigation fee should be determined in consultation with the quarry project applicant(s), the Folsom South of U.W. 50 Specific Plan project applicant(s), and the City of Folsom. No quarry trucks should be allowed to pass on any roadway segment immediately adjacent to or within the SPA until said mitigation fees are paid. • To improve the indoor noise levels at affected receptors, implement the following measures before the occupancy of the affected residences and schools: <ul style="list-style-type: none"> ○ Conduct an interior noise analysis once detailed construction plans of residences adjacent to affected roadways are available to determine the required window package at second and third floor receptors to achieve the interior noise level standard of 45 dB Ldn without quarry trucks. ○ Determine the interior quarry truck traffic noise level increases at second and third floor receptors adjacent to affected roadways compared to no quarry truck conditions. Window package upgrades are expected to be 				

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<p>necessary due to the traffic noise level increases caused by quarry trucks along affected roadways. Quarry truck applicant(s) should pay for the cost of window package upgrades (increased sound transmission class rated windows) required to achieve the interior noise level standard of 45 dB Ldn with the inclusion of quarry truck traffic.</p> <p>To the extent this noise mitigation would not already be implemented as part of the Folsom South of U.W. 50 Specific Plan project development, this mitigation should be paid for by the quarry project applicant(s) before any quarry trucks are allowed to pass on any roadway that is within 400 feet of any residence or school within the SPA.</p>				

Attachment 20

Mitigation Monitoring and Reporting Program for Highway 50 Backbone Infrastructure Project

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<p>visual and scenic qualities of the site(s). To the extent practical, the design will minimize the need for supplemental irrigation.</p> <ul style="list-style-type: none"> • New or replacement vegetation shall be compatible with surrounding vegetation and shall be adaptable to the site with regard to rainfall, soil type, exposure, growth rate, erosion control, and energy conservation purposes. • Plant materials chosen shall be species which do not present any safety hazards, which allow native flora to reestablish in the area, and which require minimal maintenance, including watering, pest control, and clean-up of litter from fruit and droppings. <p>Prior to the approval of improvement plans for the pump stations and storage tank facilities, the project applicant(s) shall show on the landscaping plan that the above measures are employed. The above requirements shall be subject to review and approval by the Folsom Environmental and Water Resources Department and the Community Development Department</p>	Department		
<p>III-1. South of US 50 Backbone and US 50 Crossings</p> <p>Prior to initiation of construction, the project contractor shall provide a plan for approval by SMAQMD demonstrating that the heavy-duty (50 horsepower [hp] or more) off-road vehicles to be used during construction of the project, including owned, leased, and subcontractor vehicles, will achieve a project-wide fleet-average 20 percent NO_x reduction compared to the most recent CARB fleet average. Acceptable options for reducing emissions may include use of late model engines, low-emission diesel products, alternative fuels, engine retrofit technology, after-treatment products, and/or other options as they become available. The SMAQMD's Construction Mitigation Calculator (available at: http://www.airquality.org/ceqa/mitigation.shtml) can be used to identify an equipment fleet that achieves this reduction.</p>	SMAQMD Folsom Community Development Department	Prior to initiation of construction	
<p>III-2. South of US 50 Backbone and US 50 Crossings</p> <p>Prior to the approval of grading plans, the applicant shall pay SMAQMD an off-site mitigation fee for implementation of the proposed project for the purpose of reducing NO_x emissions to a less-than-significant level (i.e., less than 85 lb/day). The mitigation fee is</p>	SMAQMD Folsom Community Development Department	Prior to the approval of grading plans	

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<i>used by SMAQMD to purchase off-site emissions reductions. Such purchases are made through SMAQMD's Heavy Duty Incentive Program, through which select owners of heavy-duty equipment in Sacramento County can repower or retrofit their old engines with cleaner engines or technologies. The applicant shall calculate the project's off-site mitigation fee amount in accordance with SMAQMD's recommended guidance and the current mitigation fee rate. The fee amount shall be subject to review and approval by SMAQMD. Verification of payment shall be provided to the Folsom Community Development Department.</i>	Department		
<p>III-3. <i>North of US 50 Water Improvements</i></p> <p><i>Prior to the approval of grading plans for the booster pump station at the WTP and associated water transmission pipeline to be located north of US 50, the project applicant shall perform a detailed site- and project-specific air quality analysis for review by SMAQMD and the Folsom Community Development Department and approval by the City. The analysis shall be conducted consistent with the SMAQMD CEQA Guide and shall show that the resulting emissions are below the SMAQMD thresholds. If necessary, the analysis shall include mitigation measures to reduce the emissions below the SMAQMD thresholds. Mitigation could include, but is not limited to, the payment of off-site mitigation fees for reducing NO_x emissions, the use of late model engines, low-emission diesel products, alternative fuels, engine retrofit technology, after-treatment products, and/or other options as they become available.</i></p>	SMAQMD Folsom Community Development Department	Prior to the approval of grading plans for the booster pump station at the WTP and associated water transmission pipeline to be located north of US 50	
<p>III-4. <i>South of US 50 Backbone, US 50 Crossings, and North of US 50</i></p> <p><i>Prior to the commencement of any site-disturbing activities, the applicant shall obtain the services of a California Certified Geologist to conduct a thorough site investigation of the development area per the protocol outlined in the California Geological Survey Special Report 124¹ to determine whether and where NOA is present in the soil and rock on the project site and/or areas that would be disturbed by the project, except for those areas</i></p>	Folsom Community Development Department SMAQMD	Prior to the commencement of any site-disturbing activities	

¹ California Department of Conservation. *Special Publication 124: Guidelines for Geologic Investigations of Naturally Occurring Asbestos in California*. 2002. Available at: http://www.conservation.ca.gov/cgs/minerals/hazardous_minerals/asbestos/Documents/Asbestos_Guidelines_SP124.pdf. Accessed October 13, 2014.

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<p><i>previously explored and sampled for NOA as part of the Geotechnical Engineering Study for Russell Ranch South prepared by Youngdahl Consulting Group, Inc. in December 2013. The site investigation shall include the collection of three soil and rock samples per acre to be analyzed via the CARB 435 Method. If the investigation determines that NOA is not present on the project site, then the project applicant shall submit a Geologic Exemption to SMAQMD as allowed under Title 17, Section 93105, Asbestos Airborne Toxic Control Measure for Construction, Grading, Quarrying, and Surface Mining (Asbestos ATCM). The project applicant shall submit proof of compliance with the above to the Folsom Community Development Department.</i></p> <p><i>If the site investigation determines that NOA is present on the project site, then, prior to commencement of any ground disturbance activity, the project applicant shall submit to the SMAQMD for review and approval an Asbestos Dust Mitigation Plan, including, but not limited to, control measures required by the Asbestos ATCM, such as vehicle speed limitations, application of water prior to and during ground disturbance, keeping storage piles wet or covered, and track-out prevention and removal.² The project applicant shall submit proof of compliance with the above to the Folsom Community Development Department. Upon approval of the Asbestos Dust Control Plan by the SMAQMD, the applicant shall ensure that construction contractors implement the terms of the plan throughout the construction period.</i></p> <p><i>If NOA is determined to be located on the surface of the project site, all surface soil containing NOA shall be replaced with clean soil or capped with another material (e.g., cinder or rubber), as necessary, subject to review and approval by the City Engineer.</i></p>	City Engineer		
<p><i>IV-1. Conduct Special-Status Plant Surveys; Implement Avoidance and Mitigation Measures or Compensatory Mitigation</i></p> <p><i>Determinate-level presence/absence plant surveys have not been conducted for all of the properties within the Backbone and the off-site areas. These areas may support suitable</i></p>	Folsom Community Development Department	Prior to issuance of a grading permit or any ground-breaking	

² California Air Resources Board, *Asbestos Airborne Toxic Control Measure for Construction, Grading, Quarrying, and Surface Mining Operations*. July 29, 2002. Available at: <http://www.arb.ca.gov/toxics/atcm/asb2atcm.htm>. Accessed October 13, 2014.

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<p><i>habitat for special status plants. The following measures should be implemented to mitigate for impacts to special status plants to a less-than-significant level:</i></p> <ul style="list-style-type: none"> • <i>The project applicant(s) shall retain a qualified botanist to conduct protocol level special-status plant surveys for the remaining unsurveyed areas within the Backbone and off-site areas. If no special-status plants are found during focused surveys, the botanist shall document the findings in a letter report to USFWS, CDFW and, the City of Folsom, and no further mitigation shall be required.</i> • <i>If special-status plant populations are found, the project applicant(s) shall consult with CDFW and USFWS, as appropriate depending on species status, to determine the appropriate mitigation measures for direct and indirect impacts on any special-status plant population that could occur as a result of project implementation. Mitigation measures may include preserving and enhancing existing populations, creation of off-site populations on project mitigation sites through seed collection or transplantation, and/or restoring or creating suitable habitat in sufficient quantities to achieve no net loss of occupied habitat or individuals.</i> • <i>If potential impacts on special-status plant species are likely, a mitigation and monitoring plan shall be developed before the approval of grading plans or any ground-breaking activity within 250 feet of a special-status plant population. The mitigation plan shall be submitted to the City of Folsom for review and approval. It shall be submitted concurrently to CDFW or USFWS, as appropriate depending on species status, for review and comment. For Federally-listed species, the plan shall require maintaining viable plant populations on-site and shall identify avoidance measures for any existing population(s) to be retained and compensatory measures for any populations directly affected. Possible avoidance measures include fencing populations before construction and exclusion of project activities from the fenced-off areas, and construction monitoring by a qualified botanist to keep construction crews away from the population. The mitigation plan shall also include monitoring and reporting requirements for populations to be</i> 	CDFW	activity	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>preserved on site or protected or enhanced off-site.</i></p> <ul style="list-style-type: none"> ▪ <i>If relocation efforts are part of the mitigation plan, the plan shall include details on the methods to be used, including collection, storage, propagation, receptor site preparation, installation, long-term protection and management, monitoring and reporting requirements, and remedial action responsibilities should the initial effort fail to meet long-term monitoring requirements.</i> ▪ <i>If off-site mitigation includes dedication of conservation easements, purchase of mitigation credits or other off-site conservation measures, the details of these measures shall be included in the mitigation plan, including information on responsible parties for long-term management, conservation easement holders, long-term management requirements, and other details, as appropriate to target the preservation on long term viable populations.</i> 			
<p><i>IV-2. Implement Conditions of the Biological Opinion (BO) for Federally Listed Vernal Pool Invertebrates.</i></p> <p><i>A Biological Opinion (BO) [#81420-2010-F-0620-1] was issued by USFWS for the Folsom Area Specific Plan Project, which includes the proposed backbone infrastructure project. Project construction shall not proceed in areas supporting potential habitat for Federally listed vernal pool invertebrates, or within adequate buffer areas (250 feet or lesser distance deemed sufficiently protective by a qualified biologist with approval from USFWS), until the project applicant(s) have abided by the conditions of the BO (including conservation and minimization measures) intended to be completed before on-site construction.</i></p>	<p>Folsom Community Development Department</p> <p>USFWS</p>	<p>Prior to construction activities in areas supporting vernal pool invertebrates or buffers</p>	
<p><i>IV-3. Implement Conditions of the Biological Opinion for Impacts on Valley Elderberry Longhorn Beetle.</i></p> <p><i>A biological opinion [#81420-2010-F-0620-1(BO)] has been issued by USFWS that identifies impacts to 6 elderberry shrubs with 26 stems measuring one inch or larger in diameter at ground level with project implementation. Prior to project implementation, the project applicant(s) shall abide by all pertinent conditions in the BO relating to the proposed construction, including conservation and minimization measures, intended to be completed before on-site construction. As identified in the BO, all impacted shrubs will be removed and transplanted in accordance with the Service's 1999 Conservation Guidelines</i></p>	<p>Folsom Community Development Department</p> <p>USFWS</p>	<p>Prior to construction activities in areas supporting elderberry shrubs</p>	

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<p><i>for the Valley Elderberry Longhorn Beetle to a Service approved conservation bank within the service area of the project or another location approved by the service. In addition, compensatory mitigation for elderberry shrubs that would be removed and transplanted from their current locations shall include purchasing beetle conservation credits at a Service-approved conservation bank within the service area encompassing the project area. Credits will be determined in the amended BO through consultation with USFWS.</i></p> <p><i>The population of valley elderberry longhorn beetles, the general condition of the conservation area, and the condition of the elderberry and associated native plantings in the conservation area must be monitored over a period of either ten consecutive years or for seven years over a 15-year period. A minimum survival rate of at least 60 percent of the elderberry plants and 60 percent of the associated native plants must be maintained throughout the monitoring period. Within one year of discovering that survival has dropped below 60 percent, the project proponent shall replace failed plantings to bring survival above this level. Detailed information on monitoring success of relocated and planted shrubs and measures to compensate (should success criteria not be met) would be required in the amended BO.</i></p>			
<p><i>IV-4. Western Spadefoot Toad</i></p> <p><i>Determinate-level presence/absence western spadefoot toad surveys have not been conducted for all of the properties within the Backbone and the off-site areas. These areas may support suitable habitat for western spadefoot toad. The following measures should be implemented to mitigate the potential impacts to western spadefoot toad to less than significant:</i></p> <ul style="list-style-type: none"> <i>• The project applicant(s), shall retain a qualified biologist to conduct protocol-level western spadefoot toad surveys for the remaining unsurveyed areas within the Backbone and off-site areas. If no western spadefoot toad are found during focused surveys, the biologist shall document the findings in a letter report to CDFW and the City of Folsom, and no further mitigation shall be required.</i> <i>• If western spadefoot toad populations are found, the project applicant(s) shall</i> 	<p>Folsom Community Development Department CDFW</p>	<p>Prior to any ground- breaking activity</p>	

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<i>consult with CDFW, to determine the appropriate mitigation measures for direct impacts to the western spadefoot toad population that could occur as a result of project implementation. Mitigation measures may include relocation of aquatic larvae, construction monitoring, or preserving and enhancing existing populations.</i>			
<p><i>IV-5. Western Pond Turtle</i></p> <p><i>The project applicant(s), shall retain a qualified biologist to conduct preconstruction western pond turtle survey within 48 hours of the initiation of construction activity within the ponds. If no western pond turtles are found during the preconstruction survey, the biologist shall document the findings in a letter report to CDFW and the City of Folsom, and no further mitigation shall be required. If western pond turtles are found, the qualified biologist shall capture and relocate the turtles to a suitable preserved location in the vicinity of the project.</i></p>	<p>Folsom Community Development Department</p> <p>CDFW</p>	<p>Within 48 hours of the initiation of construction activity within large ponds in oak woodlands and the Alder Creek tributaries</p>	
<p><i>IV-6(a). Swainson's Hawk Nesting Habitat</i></p> <p><i>To mitigate impacts on Swainson's hawk a qualified biologist shall be retained to conduct preconstruction surveys and to identify active nests on and within 0.5 mile of the project area. The surveys shall be conducted before the approval of grading and/or improvement plans (as applicable) and no less than 14 days and no more than 30 days before the beginning of construction. To the extent feasible, guidelines provided in Recommended Timing and Methodology for Swainson's Hawk Nesting Surveys in the Central Valley (Swainson's Hawk Technical Advisory Committee 2000) shall be followed for surveys for Swainson's hawk. If no nests are found, no further mitigation is required.</i></p> <p><i>If active nests are found, impacts on nesting Swainson's hawks shall be avoided by establishing appropriate buffers around the nests. No project activity shall commence within the buffer area until the young have fledged, the nest is no longer active, or until a qualified biologist has determined in coordination with CDFW that reducing the buffer would not result in nest abandonment. CDFW guidelines recommend implementation of 0.25- or 0.5-mile-wide buffers, but the size of the buffer may be adjusted if a qualified</i></p>	<p>Folsom Community Development Department</p>	<p>Before the approval of grading and/or improvement plans (as applicable) and no less than 14 days and no more than 30 days before the beginning of construction</p>	

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<p><i>biologist and the City, in consultation with CDFW, determine that such an adjustment would not be likely to adversely affect the nest. Monitoring of the nest by a qualified biologist during and after construction activities will be required if the activity has potential to adversely affect the nest.</i></p>			
<p><i>IV-6(b). Swainson's Hawk Foraging Habitat</i></p> <p><i>To mitigate for the loss of Swainson's hawk foraging habitat, the project applicant(s) shall identify permanent impacts to foraging habitat and prepare and implement a Swainson's hawk mitigation plan including, but not limited to the requirements described below.</i></p> <p><i>Before the approval of grading and improvement plans or before any ground-disturbing activities, whichever occurs first, the project applicant(s) shall preserve, to the satisfaction of the City, suitable Swainson's hawk foraging habitat to ensure 1:1 mitigation of habitat value for Swainson's hawk foraging habitat that is permanently lost as a result of the project, as determined by the City after consultation with CDFW and a qualified biologist.</i></p> <p><i>The 1:1 habitat value shall be based on Swainson's hawk nesting distribution and an assessment of habitat quality, availability, and use within the City's planning area, or Sacramento County jurisdiction. The mitigation ratio shall be consistent with the 1994 DFG Swainson's Hawk Guidelines included in the Staff Report Regarding Mitigation for Impacts to Swainson's Hawks (Buteo swainsoni) in the Central Valley of California. Such mitigation shall be accomplished through either the transfer of fee title or perpetual conservation easement. The mitigation land shall be located within the known foraging area and within Sacramento County. The City after consultation with CDFW, will determine the appropriateness of the mitigation land.</i></p> <p><i>The project applicant(s) shall transfer said Swainson's hawk mitigation land, through either conservation easement or fee title, to a third-party, nonprofit conservation organization (Conservation Operator), with the City and CDFW named as third-party beneficiaries. The Conservation Operator shall be a qualified conservation easement land manager that manages land as its primary function. Additionally, the Conservation Operator shall be a tax-exempt nonprofit conservation organization that meets the criteria</i></p>	<p>Folsom Community Development Department CDFW</p>	<p>Before the approval of grading and improvement plans, or before any ground-disturbing activities, whichever occurs first</p>	

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<p><i>of Civil Code Section 815.3(a) and shall be selected or approved by the City or County, after consultation with CDFW. The City, or County, after consultation with CDFW and the Conservation Operator, shall approve the content and form of the conservation easement. The City, or County, CDFW, and the Conservation Operator shall each have the power to enforce the terms of the conservation easement. The Conservation Operator shall monitor the easement in perpetuity to assure compliance with the terms of the easement.</i></p> <p><i>The project applicant(s), after consultation with the City, CDFW, and the Conservation Operator, shall establish an endowment or some other financial mechanism that is sufficient to fund in perpetuity the operation, maintenance, management, and enforcement of the conservation easement. If an endowment is used, either the endowment funds shall be submitted to the City for impacts on lands within the City's jurisdiction to an appropriate third-party nonprofit conservation agency, or they shall be submitted directly to the third-party nonprofit conservation agency in exchange for an agreement to manage and maintain the lands in perpetuity. The Conservation Operator shall not sell, lease, or transfer any interest of any conservation easement or mitigation land it acquires without prior written approval of the City and CDFW. Mitigation lands established or acquired for impacts incurred at the off-site elements shall require approval from Sacramento County prior to sale or transfer of mitigation lands or conservation easement.</i></p> <p><i>If the Conservation Operator ceases to exist, the duty to hold, administer, manage, maintain, and enforce the interest shall be transferred to another entity acceptable to the City and CDFW. The City Planning Department shall ensure that mitigation habitat established for impacts on habitat within the City's planning area is properly established and is functioning as habitat by conducting regular monitoring of the mitigation site(s) for the first 10 years after establishment of the easement. Sacramento County shall monitor habitat and ensure success for impacts on habitat at the off-site detention basin.</i></p>			
<p><i>IV-7. Tricolored Blackbird</i></p> <p><i>A qualified biologist shall conduct a preconstruction survey for any project activity that would occur during the tricolored blackbird's nesting season (March 1–August 31). The</i></p>	<p>Folsom Community Development Department</p>	<p>Within 14 days before project activity begins that would</p>	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>preconstruction survey shall be conducted before any activity occurring within 500 feet of suitable nesting habitat, including freshwater marsh and areas of riparian scrub vegetation. The survey shall be conducted within 14 days before project activity begins.</i></p> <p><i>If no tricolored blackbird colony is present, no further mitigation is required. If a colony is found, the qualified biologist shall establish a buffer around the nesting colony. No project activity shall commence within the buffer area until a qualified biologist confirms that the colony is no longer active. The size of the buffer shall be determined in consultation with CDFW. Buffer size is anticipated to range from 100 to 500 feet, depending on the nature of the project activity, the extent of existing disturbance in the area, and other relevant circumstances.</i></p>	CDFW	occur during the tricolored blackbird's nesting season (March 1- August 31)	
<p>IV-8. Nesting Raptors</p> <p><i>To mitigate impacts on nesting raptors a qualified biologist shall be retained to conduct preconstruction surveys and to identify active nests and occupied burrows on and within 0.5 mile of the project area. The surveys shall be conducted before the approval of grading and/or improvement plans (as applicable) and no less than 14 days and no more than 30 days before the beginning of construction.</i></p> <p><i>If active nests are found, impacts on nesting raptors shall be avoided by establishing appropriate buffers around the nests. No project activity shall commence within the buffer area until the young have fledged, the nest is no longer active, or until a qualified biologist has determined in coordination with CDFW that reducing the buffer would not result in nest abandonment. The buffer may be adjusted if a qualified biologist and the City, in consultation with CDFW, determine that such an adjustment would not be likely to adversely affect the nest. Monitoring of the nest by a qualified biologist during and after construction activities will be required if the activity has potential to adversely affect the nest.</i></p> <p><i>If active burrowing owl burrows are found, a mitigation plan shall be submitted to the City for review and approval before any ground-disturbing activities. The City shall consult with CDFW. The mitigation plan may consist of installation of one-way doors on</i></p>	Folsom Community Development Department CDFW	Before approval of grading and/or improvement plans (as applicable) and no less than 14 days and no more than 30 days before the beginning of construction	

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<p><i>all burrows to allow owls to exit, but not reenter, and construction of artificial burrows within the project vicinity, as needed; however, burrow owl exclusions may only be used if a qualified biologist verifies that the burrow does not contain eggs or dependent young. If active burrows contain eggs and/or young, no construction shall occur within 200-500 meters (depending on level of disturbance (CDFW 2012)) of the burrow until young have fledged. Once it is confirmed that there are no owls inside burrows, these burrows may be collapsed.</i></p>			
<p>IV-9. Nesting Special Status Birds and Migratory Birds</p> <p><i>A qualified biologist shall conduct a preconstruction survey for any project activity that would occur in suitable nesting habitat during the avian nesting season (approximately March 1–August 31). The preconstruction survey shall be conducted before any activity occurring within 100 feet of suitable nesting habitat. The survey shall be conducted within 14 days before project activity begins.</i></p> <p><i>If no active special-status or other migratory bird nests are present, no further mitigation is required. If an active nest is found, the qualified biologist shall establish a buffer around the nest. No project activity shall commence within the buffer area until a qualified biologist confirms that the nest is no longer active. The size of the buffer shall be determined in consultation with CDFW. Buffer size is anticipated to range from 50 to 100 feet, depending on the nature of the project activity, the extent of existing disturbance in the area, and other relevant circumstances.</i></p>	<p>Folsom Community Development Department CDFW</p>	<p>Within 14 days before project activity begins if in nesting season (March 1 – August 31)</p>	
<p>IV-10. Special-Status Bats</p> <p><i>The project applicant(s) shall retain a qualified biologist to conduct surveys for roosting bats. Surveys shall be conducted in the fall to determine if the mine shafts are used as a hibernaculum and in spring and/or summer to determine if it is used as a maternity or day roost. Surveys shall consist of evening emergence surveys to note the presence or absence of bats and could consist of visual surveys at the time of emergence. If evidence of bat use is observed, the number and species of bats using the roost shall be determined. Bat detectors may be used to supplement survey efforts. If no bat roosts are found, then no further study shall be required.</i></p>	<p>Folsom Community Development Department CDFW</p>	<p>During the fall season and spring/summer season</p>	

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<p><i>If roosts of pallid bat or Townsend's big-eared bats are determined to be present and must be removed, the bats shall be excluded from the roosting site before the mine shaft is removed. A mitigation program addressing compensation, exclusion methods, and roost removal procedures shall be developed in consultation with CDFW before implementation. Exclusion methods may include use of one-way doors at roost entrances (bats may leave but not reenter), or sealing roost entrances when the site can be confirmed to contain no bats. Exclusion efforts may be restricted during periods of sensitive activity (e.g., during hibernation or while females in maternity colonies are nursing young). The loss of each roost (if any) will be replaced in consultation with CDFW and may include construction and installation of bat boxes suitable to the bat species and colony size excluded from the original roosting site. Roost replacement will be implemented before bats are excluded from the original roost sites. Once the replacement roosts are constructed and it is confirmed that bats are not present in the original roost site, the mine shaft may be removed.</i></p>			
<p>IV-11. American Badger</p> <p><i>The project applicant(s) shall retain a qualified biologist to conduct preconstruction American badger burrow surveys within 48 hours of the initiation of construction activity. If no American badger burrows are found during the preconstruction survey, the biologist shall document the findings in a letter report to CDFW and the City of Folsom, and no further mitigation shall be required. If potential American badger burrows are found, the qualified biologist shall consult with CDFW to determine appropriate measures.</i></p>	<p>Folsom Community Development Department CDFW</p>	<p>48 prior to the initiation of construction activities</p>	
<p>IV-12. Implement Section 1602 Master Streambed Alteration Agreement</p> <p><i>The project applicant(s) shall amend, if necessary, and implement the original Section 1602 Master Streambed Alteration Agreement received from CDFW for all construction activities that would occur in the bed and bank of Alder Creek and other drainage channels and ponds within SPA. As outlined in the Master agreement, the project applicant(s) shall submit a Subnotification Form (SNF) to CDFW 60 days prior to the commencement of construction to notify CDFW of the project. Any conditions of issuance of the Master Streambed Alteration Agreement shall be</i></p>	<p>Folsom Community Development Department CDFW</p>	<p>60 days prior to the initiation of construction activities</p>	

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<p><i>implemented as part of project construction activities that adversely affect the bed and bank and riparian habitat associated with Alder Creek and other drainage channels and ponds that are within the project area that is subject to CDFW jurisdiction. The agreement shall be executed by the project applicant(s) and CDFW before the approval of any grading or improvement plans or any construction activities in any project phase that could potentially affect the bed and bank of Alder Creek and other on-site or off-site drainage channels under CDFW jurisdiction and their associated freshwater marsh and riparian habitat.</i></p>			
<p>IV-13. Conduct Surveys to Identify and Map Valley Needlegrass Grassland; Implement Avoidance and Minimization Measures or Compensatory Mitigation, if necessary</p> <p><i>The project applicant(s) shall retain a qualified botanist to conduct preconstruction surveys to determine if valley needlegrass grassland is present within the project area. This could be done concurrently with any special-status plant surveys conducted on-site as special-status plant surveys are floristic in nature, i.e. require that all species encountered be identified. If valley needlegrass grassland is not found within the project area, the botanist shall document the findings in a letter report to the City of Folsom, and no further mitigation shall be required.</i></p> <p><i>If valley needlegrass grassland is found within the project area, the location and extent of the community shall be mapped and the acreage of this community type, if any, that would be removed by project implementation shall be calculated. The project applicant(s) shall consult with CDFW and the City of Folsom to determine appropriate mitigation for removal of valley needlegrass grassland resulting from project implementation. Mitigation measures may include establishment of valley needlegrass grassland within project's open space areas currently characterized by annual grassland, establishment of valley needlegrass grassland off-site, or preservation and enhancement of existing valley needlegrass grassland either on or off the project area.</i></p>	<p>Folsom Community Development Department CDFW</p>	<p>Prior to ground-breaking activity</p>	
<p>IV-14. Secure Amended Clean Water Act Section 404 Permit and Section 401 Permit and Implement All Permit Conditions; Ensure No Net Loss of Functions of Wetlands, Other Waters of the U.S., and Waters of the State:</p>	<p>Folsom Community Development Department</p>	<p>Before the approval of grading and improvement</p>	

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<p><i>Before the approval of grading and improvement plans and before any groundbreaking activity associated with each distinct project phase, the project applicant(s) shall amend all necessary permits obtained under Sections 401 and 404 of the CWA or the state's Porter-Cologne Act for the original backbone infrastructure, if necessary. All amended and/or revised permits, regulatory approvals, and permit conditions for effects on wetland habitats shall be secured before implementation of any grading activities within 250 feet of waters of the U.S. or wetland habitats, including waters of the state, that potentially support Federally listed species, or within 100 feet of any other waters of the U.S. or wetland habitats, including waters of the state. The project applicant shall adhere to all conditions outlined in the amended and/or revised permits. The project applicant shall commit to replace, restore, or enhance on a "no net loss" basis (in accordance with USACE and the Central Valley RWQCB) the acreage of all wetlands and other waters of the U.S. that would be removed, lost, and/or degraded with implementation of project plan. Wetland habitat shall be restored, enhanced, and/or replaced at an acreage and location and by methods agreeable to USACE, the Central Valley RWQCB, and the City, as appropriate, depending on agency jurisdiction, and as determined during the Section 401 and Section 404 permitting processes.</i></p> <p><i>As identified in the original Section 404 permit, compensation for the loss of waters of the U.S. shall occur through the purchase of mitigation credits from the Cosumnes Floodplain Mitigation Bank and/or the Toad Hill Mitigation bank at the following compensation to impact ratios for aquatic resources, unless otherwise revised in the amended section 404 permit:</i></p> <ul style="list-style-type: none"> <i>• For loss of jurisdictional ditches, ponds, and marshes, purchase floodplain mosaic re-establishment credits from the Cosumnes Floodplain Mitigation Bank at a ratio of 1:1;</i> <i>• For loss of creeks/channels and intermittent drainages located in the Lower American River watershed (018020111), purchase floodplain riparian re-establishment credits from the Cosumnes Floodplain Mitigation Bank at a ratio of 2:1;</i> <i>• For loss of creeks/channels and intermittent drainages located in the Upper Cosumnes River (18040013), purchase floodplain riparian re-establishment</i> 	<p>USACE Central Valley RWQCB</p>	<p>plans, or before any ground-disturbing activities, whichever occurs first</p>	

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<p><i>credits from the Cosumnes Floodplain Mitigation Bank at a ratio of 1:1;</i></p> <ul style="list-style-type: none"> • <i>For loss of seasonal wetlands and seasonal wetland swales located in the Lower American River watershed (018020111), purchase floodplain mosaic re-establishment credits from the Cosumnes Floodplain Mitigation Bank at a ratio of 1.3:1;</i> • <i>For loss of seasonal wetlands and seasonal wetland swales located in the Upper Cosumnes River (18040013), purchase floodplain mosaic re-establishment credits from the Cosumnes Floodplain Mitigation Bank at a ratio of 1:1;</i> • <i>For loss of seasonal wetlands and seasonal wetland swales located in the Lower American River watershed (018020111), purchase floodplain mosaic re-establishment credits from the Cosumnes Floodplain Mitigation Bank at a ratio of 4:1;</i> • <i>For loss of seeps located in the Upper Cosumnes River (18040013), purchase floodplain mosaic re-establishment credits from the Cosumnes Floodplain Mitigation Bank at a ratio of 3:1;</i> • <i>For the loss of vernal pools, purchase creation credits from the Toad Hill Mitigation Bank at a ratio of 1:1.</i> <p><i>All mitigation requirements to satisfy the requirements of the City and the Central Valley RWQCB, for impacts on the non-jurisdictional wetlands beyond the jurisdiction of USACE, shall be determined and implemented before grading plans are approved. An amended Water quality certification pursuant to Section 401 of the CWA will be required before issuance of the record of decision and before issuance of the amended Section 404 permit. Before construction in any areas containing wetland features, the project applicant(s) shall obtain water quality certification for the project. Any measures required as part of the issuance of water quality certification shall be implemented.</i></p>			
<p><i>IV-15. South of US 50 Backbone, US 50 Crossings, and North of US 50 Water Improvements</i></p> <p><i>Conduct Tree Survey, Prepare and Implement an Oak Woodland Mitigation Plan, Replace Native Oak Trees Removed, and Implement Measures to Avoid and Minimize Indirect Impacts on Oak Trees and Oak Woodland Habitat Retained On-Site.</i></p>	<p>Folsom Community Development Department</p>	<p>Prior to any ground- disturbing activities within the oak</p>	

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<p><i>Prior to any ground-disturbing activities, the project applicant shall prepare and submit a site map, arborist report, canopy survey, individual free-standing oak tree survey, and tree preservation program to the City's Community Development Department, subject to review and approval. The individual free-standing oak tree survey shall show trees to be preserved and to be removed consistent with the requirements of Folsom Municipal Code, Chapter 12.16.</i></p> <p><i>Mitigation for impacts to oak woodland habitat shall include:</i></p> <ul style="list-style-type: none"> • <i>Following ground verification by certified arborist of the oak woodland habitat on-site, preservation of existing oak woodlands</i> • <i>Creation of new oak woodland on-site in pre-approved locations as shown in the Open Space Management Plan. Follow the oak woodland mitigation planting criteria outlined in the Oak Woodland Mitigation Planting Criteria contained in the FPASP.</i> • <i>Preserve and protect existing off-site oak woodland habitat. Existing, unprotected oak woodland habitat within Sacramento and El Dorado Counties may be secured and placed under conservation easement in lieu of onsite mitigation measures if necessary. The off-site locations would be managed as oak woodland habitat in perpetuity.</i> • <i>Create oak woodlands off site. Plant a combination of blue oak acorns, seedlings, and trees at off-site location(s). Off-site creation shall follow the same guidelines as outlined in the Mitigation Planting Criteria for on-site creation. Planted areas shall be placed under conservation easement and managed as oak woodland habitat in perpetuity.</i> • <i>The oak woodland mitigation plan prepared by the City's Public Works Department shall include a maintenance and monitoring program for any replacement trees. The program shall include monitoring and reporting requirements, schedule, and success criteria. Replacement oak trees shall be maintained and monitored for a time period consistent with the provisions of the FPASP.</i> 	<p>Folsom Public Works Department</p> <p>Sacramento County Planning Department</p>	<p>woodland habitat areas</p>	

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<p><i>The City's Public Works Department, for areas containing individual trees that will be impacted, shall develop a map depicting the tree canopy of all oak trees in the survey area and identifying the acreage of tree canopy that would be preserved and the acreage that would be removed. A tree permit for removal of isolated oak trees (those not located within the delineated boundary of oak woodland habitat) shall be obtained from the City Planning Director. As a condition of the tree removal permit, project applicant(s) shall be required to develop a Planting and Maintenance Agreement. The City's Tree Preservation Code requires compensatory mitigation and the City has developed a plan, as set forth Section 10 of the FPASP specifically to avoid and minimize adverse effects on individual oak trees from project development and to provide compensatory mitigation for removal of protected trees within the project area. In addition to the language contained in the Folsom Plan Area Specific Plan, the following elements shall be included in a protected tree mitigation plan to be developed by the City's Public Works Department and agreed upon by the Folsom Community Development Department:</i></p> <ul style="list-style-type: none"> • <i>The City's Public Works Department shall retain a certified arborist or registered professional forester to perform a determinate survey of tree species, size (dbh), condition, and location for all areas of the project site proposed for tree removal and encroachment of development. The condition of individual trees shall be assessed according to the American Society of Consulting Arborists rating system with the following added explanations:</i> <ul style="list-style-type: none"> ▪ <i>5 = Excellent; No problems – tree has no structural problems, branches are properly spaced and tree characteristics are nearly perfect for the species.</i> ▪ <i>4 = Good; No apparent problems – tree is in good condition and no apparent problems from visual inspection. If potential structural or health problems are tended at this stage, future hazard can be reduced and more serious health problems can be averted.</i> ▪ <i>3 = Fair; Minor problems – There are some minor structural or health problems that pose no immediate danger. When the recommended actions in an arborist report are completed correctly the defect(s) can be</i> 			

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<p><i>minimized or eliminated.</i></p> <ul style="list-style-type: none"> ▪ 2 = Poor; Major problems – the tree is in poor condition, but the condition could be improved with correct arboricultural work including, but not limited to: pruning, cabling, bracing, bolting, guying, spraying, mistletoe removal, vertical mulching, and fertilization. If the recommended actions are completed correctly, hazard can be reduced and the rating can be elevated to a 3. If no action is taken the tree is considered a liability and should be removed. ▪ 1 = Hazardous or non correctable condition – the tree is in extremely poor condition and in nonreversible decline. This rating is assigned to a tree that has structural and/or health problems that no amount of tree care work or effort can change. The issues may or may not be considered a dangerous situation. The tree may also be infested with a disease or pest(s) that is non-controllable at this time and is causing an unacceptable risk of spreading the disease or pests(s) to other trees. ▪ 0 = Dead – the tree has no significant signs of life (dead or very close to being dead). <ul style="list-style-type: none"> • The determination for whether an individual tree shall be preserved, removed without compensation, or removed with compensatory mitigation shall be based on the condition and size of the tree as follows: <ul style="list-style-type: none"> ▪ Trees rated 0 or 1 may be removed with no mitigation. ▪ Trees rated 2 may be removed at 50% of the normal Folsom Municipal Code mitigation. ▪ Trees rated 3, 4, and/or 5 may be removed at the normal Folsom Municipal Code mitigation. ▪ Native oaks measuring 24 inches or greater dbh for a single trunk or 40 inches or more for a multi-trunked tree and rated a 4 or 5 shall be retained. Trees of this size but having a rating of 2 or 3 shall not be removed or mitigated, unless retaining wall(s) higher than 4 feet tall (from bottom of footing to the top of the wall) would be required to protect the tree(s) from mass grading within the project area. ▪ Native oaks measuring between 12 and 24 inches dbh and rated a 4 or 5 			

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<p><i>shall not be removed or mitigated unless wall(s) would need to be built that are higher than 4 feet tall (from bottom of footing to the top of the wall) would be required to protect the tree(s) from mass grading within the project area. Trees in this size class but rated 2 or 3 shall not be removed unless unreasonable costs to save the tree(s) (greater than the normal Folsom Municipal Code mitigation) would result.</i></p> <ul style="list-style-type: none"> ▪ <i>Native oaks measuring 5 inches or greater dbh but less than 12 inches dbh shall not be removed unless unreasonable costs to save the tree(s) (greater than the normal Folsom Municipal Code mitigation) would result.</i> ▪ <i>Native oak trees measuring 1 inch or greater dbh but less than 5 inches dbh may be preserved to receive a Small Tree Preservation Credit (STPC). Any tree that is to be considered for preservation credit shall be evaluated, included in the arborist report, and shall have been found to be rated a 3, 4, or a 5. Credits shall only be accepted if the tree protection zone (TPZ) (i.e., the outer edge of the tree canopy drip line) is protected with fencing in the exact manner that 5 inches dbh and greater trees are protected on a construction site, and the spacing is equal to the proper tree spacing dictated by the Folsom Master Tree List. STPC shall not count if they the tree is in a poor growing space due to its position within the TPZ of another protected tree to be preserved. The City shall accept the preservation of native oak trees in this size class as credit towards the total removed inches based on the following STPC criteria:</i> <table border="1" data-bbox="293 1230 1032 1388"> <thead> <tr> <th>Caliper of Tree Preserved</th> <th>Mitigation Tree Credit Equivalent</th> </tr> </thead> <tbody> <tr> <td>1 inch or greater, but less than 2 inches</td> <td>One #15 tree or two #5 container trees</td> </tr> <tr> <td>2 inches or greater, but less than 3 inches</td> <td>Two #15 container trees</td> </tr> <tr> <td>3 inches or greater, but less than 4 inches</td> <td>Tree #15 container trees</td> </tr> <tr> <td>4 inches or greater, but less than 5 inches</td> <td>Four #15 container trees</td> </tr> </tbody> </table>	Caliper of Tree Preserved	Mitigation Tree Credit Equivalent	1 inch or greater, but less than 2 inches	One #15 tree or two #5 container trees	2 inches or greater, but less than 3 inches	Two #15 container trees	3 inches or greater, but less than 4 inches	Tree #15 container trees	4 inches or greater, but less than 5 inches	Four #15 container trees			
Caliper of Tree Preserved	Mitigation Tree Credit Equivalent												
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<ul style="list-style-type: none"> • <i>Folsom Municipal Code requires one of the following be planted as compensation for each diameter inch of protected tree removed:</i> <ul style="list-style-type: none"> ▪ <i>half of a 24-inch box tree;</i> ▪ <i>one #15 container tree;</i> ▪ <i>two #5 container trees; or</i> ▪ <i>\$150 in-lieu payment or other fee set by City Council Resolution.</i> • <i>The Planting and Maintenance Agreement shall include a planting plan, planting and irrigation design details, and a weaning schedule for the establishment period. The plan shall include a 5-year establishment period for trees and 8 years for planted acorns with an annual monitoring report that includes corrections needed with proposed work plan, and notice of compliance within 90-days of annual monitoring report. Security in a form acceptable to the City and sufficient to cover maintenance and monitoring costs for eight years shall be provided to the Folsom Community Development Department. The security will be forfeited if the project applicant or designated responsible party fails to fulfill the Planting and Maintenance Agreement.</i> • <i>To avoid and minimize indirect impacts on protected trees to remain within the project area, the City's Public Works Department shall install high visibility fencing outside the outer edge of the drip lines of all trees to be retained within the project area during project construction. The fencing may be installed around groups or stands of trees or whole wooded areas, but must be installed so that the drip lines of all trees are protected. Grading, trenching, equipment or materials storage, parking, paving, irrigation, and landscaping shall be prohibited within the fenced areas (i.e. drip lines of protected trees). If the activities listed cannot be avoided within the drip line of a particular tree, that tree shall be counted as an affected tree and compensatory mitigation shall be provided, or the tree in question shall be monitored for a period of five years and replaced only if the tree appears to be dead or dying within five years of project implementation.</i> <p><i>Through a combination of the mitigation options presented above along with the proposed on-site preservation of blue oak woodland habitat in the open space areas, the City can satisfy the mitigation requirements for removal of trees protected under the Folsom</i></p>			

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<i>Municipal Code while also mitigating the impacts on oak woodland habitat, as determined through consultation with the Sacramento County Planning Department and the City of Folsom.</i>			
<p><i>V-1. South of US 50 Backbone and US 50 Crossings</i></p> <p><i>The FAPA provides a management framework for identifying historic properties and historical resources, determining adverse effects, and resolving those adverse effects with appropriate mitigation.</i></p> <p><i>Proof of compliance with the applicable procedures in the FAPA and implementation of applicable historic property treatment plans (HPTPs) with regard to mitigation for the individually eligible sites and contributing elements to the districts shall be provided to the Folsom Community Development Department prior to authorization of any ground disturbing activities in any given segment of the project area. Proof of compliance is defined as written approval from the USACE of all applicable mitigation documentation generated from implementation of an approved HPTP and includes the following mitigation actions:</i></p> <ul style="list-style-type: none"> ▪ <i>Historic American Engineering Record documentation of the Keefe-McDerby Mine Ditch (P-34-1475) and Rhoades' Branch Ditch (P-34-1742):</i> <ul style="list-style-type: none"> ▪ <i>In order to determine the appropriate level of documentation necessary, the USACE shall first consult with the National Park Service (NPS), which administers the HAER program. Consultation with the NPS will be initiated through the submission of the DPR site record and copies of applicable technical reports with a request for review and issuance of a stipulation letter. Unless an objection to the requirements of the stipulation letter is expressed and resolved through the process outlined in the FAPA, the level of documentation stipulated by the NPS shall be implemented and all documentation will be approved by the USACE and NPS prior to ground-disturbing activities affecting the resource, or as governed by the permit conditions. Focused archival research conducted as part of the HAER documentation shall be incorporated into the revised</i> 	<p>Folsom Community Development Department</p> <p>USACE</p>	<p>Prior to authorization of any ground-disturbing activities</p>	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>cultural context statement for the FPASP area via the Historic Property Synthesis report. A non-archival set of the final documentation shall be submitted to the City.</i></p> <ul style="list-style-type: none"> • <i>Data recovery excavations of sites P-34-4612, -1746, -1788, -1910, 1911, -4714, and -2166:</i> <ul style="list-style-type: none"> ▪ <i>Data recovery shall follow the standards and guidelines in the HPTP and shall include at least 4 1m-x-1m excavation units. The results of the data recovery, including results of excavation, laboratory analysis, artifact analysis, and archival research, shall be documented in a confidential data recovery technical report, which shall be submitted to the City.</i> • <i>Documentation of White Rock Road, P-34-1555:</i> <ul style="list-style-type: none"> ▪ <i>Documentation shall consist of focused archival research, field photography, videography, and mapping, extensive enough to capture the setting, alignment, and association with adjacent features. Copies of the documentation shall be submitted to the City.</i> • <i>Landscape Mapping of Districts:</i> <ul style="list-style-type: none"> ▪ <i>Low level aerial photography and topographic mapping of the districts within the entire APE will be completed, which includes both contributing and non-contributing elements. Color multiband digital photography will be collected at or better than 0.5-foot pixel resolution, equating to 1"=100' scale in traditional imagery. Topographic data was already acquired for most of the districts by aircraft-mounted LIDAR equipment with an approximate ground point spacing of better than one meter, allowing for the creation of one-foot contours at 1"=100' scale, which fulfills the National Mapping Accuracy Standards. The digital aerial photographs and topographic data will be incorporated into a Geographic Information System database with the ACCMD boundaries delineated, as established below. Preservation and archiving of digital imagery and topographic data shall be carried out in accordance with the requirements of Attachment G of the PHPS (Westwood et al. 2011).</i> <p><i>The district boundaries will be permanently established and mapped to</i></p>			

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>establish permanent boundaries for the district within the APE with sub-meter accuracy. The verification will define the boundaries of the district within the APE using an updated DPR 523A (Primary Record) form, color photography, and a district-level plan map. The feature records for each element of the district will be updated to reflect the new mapping data.</i></p> <ul style="list-style-type: none"> • <i>Geoarchaeological monitoring: <ul style="list-style-type: none"> ▪ <i>Due to a potential for deeply buried archaeological resources down to a depth of 1.5 meters (approximately 5 feet) below soil formations known as the T-2 terrace, where colluvial deposits grade onto the T-2 terrace, and along the distal edge of tributary alluvial fans, all ground disturbing activity in those areas shall be monitored by a qualified professional archaeologist with a specialization in geoarchaeology. Once subsurface disturbance extends beyond 1.5 meters below surface, monitoring is no longer needed.</i> </i> <p><i>A confidential map showing the locations of required monitoring has been submitted to the City. The City shall apply a map condition that requires geoarchaeological monitoring in those locations only, and proof of compliance is a copy of the monitoring report submitted to the City.</i></p>			
<p><i>V-2. South of US 50 Backbone, US 50 Crossings, and North of US 50 Water Improvements</i> <i>Before the start of ground-disturbing activities, the project applicant(s) shall retain a qualified archaeologist (for cultural resources) and a qualified professional (for paleontological resources) to conduct training for all construction personnel involved with earthmoving activities, including the site superintendent, to inform them about the possibility of encountering buried cultural and paleontological resources (i.e., fossils), and inform them of the proper procedures should cultural or paleontological resources be encountered. Proof of the contractor awareness training shall be submitted to the Folsom Community Development Department in the form of a copy of training materials and the completed training attendance roster.</i></p> <p><i>Should any cultural resources, such as structural features, bone or shell, artifacts, or</i></p>	<p>Folsom Community Development Department USACE</p>	<p>Prior to ground-disturbing activities</p>	

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<p><i>architectural remains be encountered during any construction activities, work shall be suspended within 200 feet of the find and the City of Folsom and USACE shall be notified immediately. The City shall retain a qualified archaeologist who shall conduct a field investigation of the specific site and shall evaluate the significance of the find by evaluating the resource for eligibility for listing on the CRHR and the NRHP. If the resource is eligible for listing on the CRHR or NRHP and it would be subject to disturbance or destruction, the City shall require that the resource be treated with appropriate mitigation measures prior to work resuming, such as data recovery excavation or field documentation. The City of Folsom and USACE shall be responsible for approval of recommended mitigation if it is determined to be feasible in light of the approved land uses, and shall implement the approved mitigation and seek written approval on mitigation documentation before resuming construction activities at the archaeological site.</i></p> <p><i>If paleontological resources are discovered during earthmoving activities, the construction crew shall immediately cease work in the vicinity of the find and notify the City of Folsom. The project applicant(s) shall retain a qualified paleontologist to evaluate the resource and prepare a recovery plan in accordance with Society of Vertebrate Paleontology guidelines (1996). The recovery plan may include, but is not limited to, a field survey, construction monitoring, sampling and data recovery procedures, museum storage coordination for any specimen recovered, and a report of findings. Recommendations in the recovery plan that are determined by the lead agency to be necessary and feasible shall be implemented before construction activities can resume at the site where the paleontological resources were discovered.</i></p>			
<p><i>V-3. South of US 50 Backbone, US 50 Crossings, and North of US 50 Water Improvements</i></p> <p><i>In the event that human remains are discovered, construction activities within 150 feet of the discovery shall be halted or diverted and the requirements for managing unanticipated discoveries in Mitigation Measure V-3 shall be implemented. In addition, the provisions of Section 7050.5 of the California Health and Safety Code, Section 5097.98 of the California Public Resources Code, and Assembly Bill 2641 shall be implemented. When human remains are discovered, state law requires that the discovery be reported to the</i></p>	<p>Folsom Community Development Department Sacramento County Coroner</p>	<p>During construction</p>	

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<p><i>County Coroner (Section 7050.5 of the Health and Safety Code) and that reasonable protection measures be taken during construction to protect the discovery from disturbance (AB 2641).</i></p> <p><i>If the Coroner determines the remains are Native American, the Coroner shall notify the Native American Heritage Commission, which then designates a Native American Most Likely Descendant (MLD) for the project (Section 5097.98 of the Public Resources Code). The designated MLD then has 48 hours from the time access to the property is granted to make recommendations concerning treatment of the remains (AB 2641).</i></p> <p><i>If the landowner does not agree with the recommendations of the MLD, the NAHC can mediate (Section 5097.94 of the Public Resources Code). If no agreement is reached, the landowner must rebury the remains where they will not be further disturbed (Section 5097.98 of the Public Resources Code). In addition, the site shall be recorded with the NAHC or the appropriate Information Center; using an open space or conservation zoning designation or easement; or recording a document with the county in which the property is located (AB 2641).</i></p>	Native American Heritage Commission		
<p>V-4. <i>North of US 50 Water Improvements</i></p> <p><i>Once the water pipeline alignment and booster pump station locations are finalized, the Folsom Community Development Department and applicant shall comply with the procedures for identification, evaluation, and treatment of historical resources under CEQA.</i></p> <p><i>Management of cultural resources eligible for or listed on the CRHR under CEQA mirrors management steps required under Section 106. These steps may be combined with deliverables and management steps performed for Section 106 provided that management documents prepared for the FAPA also clearly reference the CRHR listing criteria and significance thresholds that apply under CEQA. The following steps and documents may be combined with the phasing of management and documents prepared pursuant to the FAPA (if a USACE permit is required) to minimize the potential for inconsistency and duplicative management efforts. The Folsom Community Development Department shall</i></p>	Folsom Community Development Department	Prior to ground-disturbing activities	

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<p><i>consult with the USACE when applicable.</i></p> <p><i>Prior to ground-disturbing work for each element, the project applicant(s), with Folsom Community Development Department oversight, shall retain the services of a qualified professional who meets the applicable Professional Qualifications Standards of the Secretary of the Interior to perform the following actions:</i></p> <ul style="list-style-type: none"> • <i>Perform an inventory of cultural resources within each individual element footprint, subject to approval by the Folsom Community Development Department. The inventory shall include a current records search and literature review from the North Central Information Center and an intensive pedestrian survey to identify any cultural resources inside the project footprint, including archaeological sites, historic buildings and structures, cultural landscapes, or districts. The inventory report shall follow the California Office of Historic Preservation's Archaeological Resource Management Reports: Recommended contents and format include confidential Department of Parks and Recreation (DPR) site records and maps for all resources identified within the footprint. The report shall identify locations that are sensitive for undiscovered cultural resources based upon the location of known resources, geomorphology, and topography.</i> • <i>Evaluate all cultural resources relative to Criteria 1, 2, 3, and 4 of the CRHR through an appropriate combination of archival or archaeological research, and based on an appropriate prehistoric and historic context that identifies relevant prehistoric, ethnographic, and historic themes and research questions. The prehistoric and historic context statements shall be used to determine the significance of identified resources and appropriate treatment of resources determined eligible for the CRHR (or NRHP), which are considered historical resources for the purpose of CEQA.</i> • <i>For each historical resource within the project footprint, determine if implementation of the individual project development phase would result in damage or destruction of historical resources. The findings shall be reviewed by the Folsom Community Development Department for consistency with the</i> 			

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>significance thresholds and treatment measures provided in the FPASP Final EIR/EIS and FAPA (if applicable).</i></p> <ul style="list-style-type: none"> • <i>Where possible, configure or redesign the project to avoid impacts on historical resources. Alternatively, these resources may be preserved in place if possible, as suggested under California Public Resources Code Section 21083.2.</i> • <i>For historical resources that cannot feasibly be avoided and will be significantly impacted by the project, prepare a treatment and mitigation plan that may include, but is not limited to, the measures listed below. The measures shall be reviewed by the Folsom Community Development Department for consistency with the significance thresholds and standards provided in the FPASP Final EIR/EIS, and in consultation with the USACE for compliance with the FAPA (if applicable).</i> <ul style="list-style-type: none"> ▪ <i>Monitoring of ground-disturbing work in areas determined by the qualified professional to be highly sensitive for known or unknown cultural resources, if appropriate. The identification of sensitive locations subject to monitoring during construction of each individual development phase shall be performed in concert with monitoring activities performed under the FAPA to minimize the potential for conflicting requirements.</i> ▪ <i>Data recovery excavations for resources that are eligible for listing under CRHR Criterion 4 because of the data they contain (which may contribute to research).</i> ▪ <i>For historical architectural, engineered, or landscape features, treatment measures may consist of a preparation of interpretive, narrative, or photographic documentation.</i> • <i>All technical and mitigation documentation is subject to approval by the Folsom Community Development Department, in consultation with appropriate agencies, when applicable.</i> 			
<p><i>VI-1. South of US 50 Backbone and US 50 Crossings</i></p> <p><i>Prior to the start of construction activities, the project applicant(s) for the south of US 50 backbone and US 50 crossings improvements shall hire a licensed geotechnical engineer to prepare final, site-specific geotechnical subsurface investigation report(s), which shall</i></p>	<p>Folsom Environmental and Water Resources Department</p>	<p>Prior to the start of construction activities</p>	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>be submitted for review and approval to the Folsom Environmental and Water Resources Department and the Community Development Department. The final geotechnical engineering report(s) shall address and make recommendations on the following:</i></p> <ul style="list-style-type: none"> • Site preparation; • Soil bearing capacity; • Appropriate sources and types of fill; • Potential need for soil amendments; • Road, pavement, and parking areas; • Structural foundations, including retaining-wall design; • Grading practices; • Soil corrosion of concrete and steel; • Erosion/winterization; • Seismic ground shaking; • Liquefaction; and • Expansive/unstable soils. <p><i>The portion of Alder Creek that the sewer pipeline will cross shall be evaluated for slope stability. In addition to the recommendations for the conditions listed above, the geotechnical investigation shall include subsurface testing of soil and groundwater conditions, and shall determine appropriate foundation design for water storage tanks and booster pump stations that are consistent with the California Building Code (CBC). Special recommendations contained in the geotechnical engineering report shall be noted on the grading plans and implemented as appropriate before construction begins. Design and construction of the structural foundations for the booster pump station and the water storage tanks shall be in accordance with the CBC. The project applicant(s) shall provide for engineering inspection and certification that earthwork has been performed in conformity with recommendations contained in the geotechnical report.</i></p>	Folsom Community Development Department		
<p><i>VI-2. North of US 50 Water Improvements</i></p> <p><i>Prior to the start of construction activities, the project applicant(s) for the north of US 50</i></p>	Folsom Community Development	Prior to the start of construction	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>water improvements shall hire a licensed geotechnical engineer to prepare a final, site-specific geotechnical subsurface investigation report for the proposed improvements, which shall be submitted for review and approval to the Folsom Community Development Department. The final geotechnical engineering report shall address and make recommendations on the following:</i></p> <ul style="list-style-type: none"> • <i>Site preparation;</i> • <i>Soil bearing capacity;</i> • <i>Appropriate sources and types of fill;</i> • <i>Structural foundations, including retaining-wall design;</i> • <i>Potential need for soil amendments;</i> • <i>Grading practices;</i> • <i>Soil corrosion of concrete and steel;</i> • <i>Erosion/winterization;</i> • <i>Seismic ground shaking;</i> • <i>Liquefaction; and</i> • <i>Expansive/unstable soils.</i> <p><i>In addition to the recommendations for the conditions listed above, the geotechnical investigation shall include subsurface testing of soil and groundwater conditions, and shall determine appropriate foundation design for the booster pump stations that are consistent with the CBC. Special recommendations contained in the geotechnical engineering report shall be noted on the grading plans and implemented as appropriate before construction begins. Design and construction of the structural foundations for the booster pump station shall be in accordance with the CBC. The project applicant(s) shall provide for engineering inspection and certification that earthwork has been performed in conformity with recommendations contained in the geotechnical report.</i></p>	Department	activities	
<p>VI-3. South of US 50 Backbone, US 50 Crossings, and North of US 50 Water Improvements</p> <p><i>Prior to initiation of ground disturbance, a geotechnical engineer shall develop a program to monitor the sites during construction to ensure compliance with the</i></p>	Folsom Community Development Department	Prior to the initiation of a ground disturbance	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<i>recommendations presented in the geotechnical report(s) and conditions for performing such monitoring. The geotechnical monitoring program shall include a description of the improvements areas where geotechnical monitoring shall be required. The monitoring program shall be subject to review and approval by the Folsom Community Development Department.</i>			
<p><i>VI-4. South of US 50 Backbone</i></p> <p><i>Prior to the start of all construction activities east of Old Placerville Road, the project applicant(s) for all improvements south of US 50 shall retain a licensed geotechnical engineer to perform a seismic refraction survey. Project-related excavation activities shall be carried out as recommend by the geotechnical engineer, and approved by the Folsom Community Development Department.</i></p>	Folsom Community Development Department	Prior to the start of all construction activities east of Old Placerville Road	
<p><i>VI-5(a). South of US 50 Backbone, US 50 Crossings, and North of US 50 Water Improvements</i></p> <p><i>Prior to issuance of grading permits, the project applicant(s) constructing portions of the backbone infrastructure shall retain a California Registered Civil Engineer to prepare a grading and erosion control plan. The grading and erosion control plan shall be submitted to the City Public Works Department. The plan shall be consistent with the City's Grading Ordinance, the City's Hillside Development Guidelines, and the state's NPDES permit, and shall include the site-specific grading associated with backbone construction.</i></p> <p><i>Erosion and sediment control measures could include the use of detention basins, berms, swales, wattles, and silt fencing, and covering or watering of stockpiled soils to reduce wind erosion. Stabilization on steep slopes could include construction of retaining walls and reseeding with vegetation after construction. Stabilization of construction entrances to minimize trackout (control dust) is commonly achieved by installing filter fabric and</i></p>	Folsom Public Works Department	Prior to the issuance of grading permits	
<p><i>VI-5(b). South of US 50 Backbone</i></p> <p><i>Prior to issuance of grading permits for the detention basin west of Prairie City Road, the project applicant shall retain a California Registered Civil Engineer to prepare a grading and erosion control plan. The grading and erosion control plan shall be submitted to the City Public Works Department. The plan shall be consistent with the City's Grading</i></p>	Folsom Public Works Department	Prior to the issuance of grading permits for the detention basin west of Prairie	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>Ordinance, the City's Hillside Development Guidelines, and the state's NPDES permit, and shall include the site-specific grading associated with construction of the detention basin construction.</i></p> <p><i>Erosion and sediment control measures could include the use of detention basins, berms, swales, wattles, and silt fencing, and covering or watering of stockpiled soils to reduce wind erosion. Stabilization on steep slopes could include construction of retaining walls and reseeding with vegetation after construction. Stabilization of construction entrances to minimize trackout (control dust) is commonly achieved by installing filter fabric and crushed rock to a depth of approximately one foot.</i></p>		City Road	
<p>VII-1. Greenhouse Gas Emissions</p> <p><i>Prior to releasing a request for bid to contractors for construction of the proposed project, the project applicant shall stipulate that, at a minimum, the following SMAQMD GHG reduction measures be implemented in the respective request for bid, as well as the subsequent construction contract with the selected primary contractor:</i></p> <ul style="list-style-type: none"> • <i>Improve fuel efficiency from construction equipment:</i> <ul style="list-style-type: none"> ▪ <i>Reduce unnecessary idling (modify work practices, install auxiliary power for driver comfort);</i> ▪ <i>Perform equipment maintenance (inspections, detect failures early, corrections);</i> ▪ <i>Train equipment operators in proper use of equipment;</i> ▪ <i>Use the proper size of equipment for the job; and</i> ▪ <i>Use equipment with new technologies (repowered engines, electric drive trains).</i> • <i>Use alternative fuels for electricity generators and welders at construction sites such as propane or solar, or use electrical power.</i> • <i>Use a CARB-approved low-carbon fuel, such as biodiesel or renewable diesel for construction equipment. (Emissions of NO_x from the use of low carbon fuel must be reviewed and increases mitigated.) Additional information about low-carbon fuels is available from CARB's Low Carbon Fuel Standard Program.</i> 	SMAQMD	Prior to releasing a request for bid to contractors	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<ul style="list-style-type: none"> • Encourage and provide carpools, shuttle vans, transit passes and/or secure bicycle parking for construction worker commutes. • Reduce electricity use in the construction office, if applicable, by using compact fluorescent bulbs, powering off computers every day, and replacing heating and cooling units with more efficient ones. • Recycle non-hazardous construction and demolition debris (goal of at least 75 percent by weight). • Use locally sourced or recycled materials for construction materials (goal of at least 20 percent based on costs for building materials, and based on volume for roadway, parking lot, sidewalk and curb materials). • Minimize the amount of concrete used for paved surfaces or use a low carbon concrete option. • Produce concrete on-site if determined to be less emissive than transporting ready mix. • Use EPA-certified SmartWay trucks for deliveries and equipment transport. Additional information about the SmartWay Transport Partnership Program is available from ARB's Heavy-Duty Vehicle Greenhouse Gas Measure (ARB 2009c) and EPA (EPA 2009). • Develop a SMAQMD-approved plan to efficiently use water for adequate dust control. This may consist of the use of non-potable water from a local source. <p><i>If the above-listed SMAQMD GHG reduction measures are determined to be infeasible by the project applicant, the applicant shall submit to SMAQMD a report that substantiates why specific measures are considered infeasible for construction of the proposed project. The report, including the substantiation for not implementing particular GHG reduction measures, shall be subject to review and approval by SMAQMD prior to initiating construction.</i></p>			
<p><i>VIII-1. South of US 50 Backbone</i></p> <p><i>The project applicant(s) for all improvements south of US 50 that would occur in or adjacent to the Area 40 boundary or on Aerojet property west of Prairie City Road shall submit copies of the improvement plans for review and approval relative to contamination</i></p>	<p>Folsom Community Development Department</p>	<p>Prior to Improvement Plan approval of areas in or adjacent to the</p>	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>conditions and avoidance of interference with remediation if the plans involve groundbreaking or other activities that either may lead to exposure to contamination conditions or impact future or existing remedial activities or involve modifications or closure of existing groundwater monitoring wells to (i) Aerojet, DTSC, the Central Valley RWQCB, and (ii) Folsom Community Development Department for property other than property west of Prairie City Road, or any successor in interest (the entities described in (i) and (ii) are referred to as "approving entities"). The approving entities shall work with the project applicant(s) to establish the preservation, modification, or closure of existing groundwater monitoring wells. Improvement Plans and related construction activities may be subject to approving entity conditions on location, design, construction, or maintenance conditions to mitigate environmental conditions or avoid interference with remediation, including approving entity requirements for vapor barriers in the utility corridors. Relative to sediment basins within or adjacent to Area 40 or (to the extent ultimately sited on Aerojet property west of Prairie City Road), the basins will be lined and subject to leak detection to avoid contribution to groundwater.</i></p>	<p>DTSC Central Valley RWQCB</p>	<p>Area 40 boundary or on Aerojet property west of Prairie City Road</p>	
<p><i>VIII-2. South of US 50 Backbone</i></p> <p><i>The project applicant(s) shall provide notice the approving entities of the location, nature, and duration of construction activities within the Area 40 boundary at least 30 days before construction activities begin. Remedial actions that may be required as to contamination conditions within the Area 40 boundary or on lands used for monitoring or other remediation activities, as required by DTSC, RWQCB, and/or the EPA, may include, but are not limited to:</i></p> <ul style="list-style-type: none"> ▪ <i>Monitoring;</i> ▪ <i>Installation of vertical barriers;</i> ▪ <i>Biological, chemical, and/or physical treatment;</i> ▪ <i>Extraction or excavation; and/or</i> ▪ <i>Pump and treat activities.</i> 	<p>Folsom Community Development Department DTSC Central Valley RWQCB</p>	<p>At least 30 days before construction activities begin within the Area 40 boundary</p>	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>VIII-3. South of US 50 Backbone</i></p> <p><i>The project applicant(s) for all improvements south of US 50 that would occur in or adjacent to the Area 40 boundary, the detention basin west of Prairie City Road, or lands subject to monitoring or other remediation activities shall provide notification in writing to the City that the required notice and approving entity (including, but not limited to, DTSC, CVRWQCB, and Folsom Community Development Department) conditions have been fulfilled. Evidence shall be submitted to the Folsom Community Development Department before approval of improvement plans.</i></p>	<p>Folsom Community Development Department</p>	<p>Prior to Improvement Plan approval of areas in or adjacent to the Area 40 boundary or on Aerojet property west of Prairie City Road</p>	
<p><i>IX-1. South of US 50 Backbone, US 50 Crossings, and North of US 50 Water Improvements</i></p> <p><i>Prior to issuance of grading permits, the applicant(s) shall obtain an NPDES Construction General Permit from the Central Valley Regional Water Quality Control Board. The permit is required to control both construction and operation activities that may adversely affect water quality. To obtain coverage under this General Permit, the appropriate Legally Responsible Person (LRP) must electronically file Permit Registration Documents (PRDs), which include a Notice of Intent (NOI), a SWPPP, and other documents required by the General Permit, and mail the appropriate permit fee to the SWRCB. In addition, a Risk Level Assessment shall be completed in accordance with SWRCB Order No. 2009-0009-DWQ. The SWPPP shall describe the erosion and sediment controls using BMPs and Best Available Technologies (BATs). The SWPPP shall also include means of waste disposal, implementation of approved local plans, control of post-construction sediment and erosion control. Typical BMPs that could be used during construction of the proposed projects include, but are not limited to temporary facilities such as straw wattles and sandbags. Temporary facilities will capture a majority of the siltation resulting from construction activities prior to discharging into existing natural channels. The construction contractor shall be required to comply with the permit and implement, monitor, and maintain all BMPs during construction to ensure they function properly for review and approval of the City Engineer.</i></p>	<p>Central Valley RWQCB City Engineer</p>	<p>Prior to the issuance of grading permits</p>	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>XII-1 North of US 50 Water Improvements</i></p> <p><i>Prior to approval of construction drawings for a new 6.8 gpm booster pump station at the Folsom Water Treatment Plant, an acoustical analysis shall be prepared to evaluate whether the proposed booster pump station could generate stationary noise levels that would exceed the City of Folsom's non-transportation-related exterior noise level standards (see Table 26-3) at nearby residential backyards. If it is determined that nearby residential backyards would be subject to exterior noise levels in excess of the General Plan noise standards, as a result of the proposed booster pump station, then the acoustical analysis shall include recommendations to reduce exterior noise levels at nearby backyards below the General Plan noise standards. This may include relocating the proposed booster pump station, or redesigning the pump station. The acoustical analysis shall be reviewed and approved by the Folsom Public Works Department prior to approval of construction drawings for the booster pump station improvements.</i></p>	<p>Folsom Public Works Department</p>	<p>Prior to approval of construction drawings for a new booster pump station at the Folsom Water Treatment Plant</p>	
<p><i>XVI-1 South of US-50 Backbone, US 50 Crossings, and North of US 50 Water Improvements</i></p> <p><i>The project applicant(s) shall prepare and implement traffic control plans for construction activities that may affect road rights-of-way. The traffic control plans must follow any applicable standards of the agency responsible for the affected roadway and must be approved and signed by a professional engineer. Measures typically used in traffic control plans include advertising of planned lane closures, warning signage, a flag person to direct traffic flows when needed, and methods to ensure continued access by emergency vehicles. During project construction, access to existing land uses shall be maintained at all times, with detours used as necessary during road closures. Traffic control plans shall be submitted to the City of Folsom Public Works Department or Caltrans for review and approval prior to the approval of all project plans or permits, for all improvements where implementation may cause impacts on traffic along roadways within their respective areas of jurisdiction. The traffic control plan shall, at minimum, include the following measures:</i></p> <ul style="list-style-type: none"> <i>• Maintaining the maximum amount of travel lane capacity during non-construction periods, possible, and advanced notice to drivers through the provision of construction signage.</i> <i>• Maintaining alternate one-way traffic flow past the lay down area and site</i> 	<p>Folsom Public Works Department</p>	<p>Prior to construction activities</p>	

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Mitigation Measure	Monitoring Agency	Timing	Sign-Off
<p><i>access when feasible.</i></p> <ul style="list-style-type: none"> • <i>Heavy trucks and other construction transport vehicles shall avoid the busiest commute hours (7:00 AM to 8:00 AM and 5:00 PM to 6:00 PM on weekdays).</i> • <i>The City shall provide a minimum 72-hour advance notice of access restrictions for residents, businesses, and local emergency response agencies. The notice shall include the identification of alternative routes and detours to enable for the avoidance of the immediate construction zone.</i> • <i>The City, in cooperation with the contractor(s), shall provide a phone number and community contact for inquiries about the schedule of the construction of the south of US 50 backbone infrastructure throughout the construction period. The information will be posted in a local newspaper, on the City's web site, or at City Hall and will be updated on a monthly basis.</i> • <i>To the extent practical, the City shall maximize opportunities for coordinated construction and installation of the conveyance pipeline with other planned roadway improvement projects.</i> 			

Attachment 21

Mitigation Monitoring and Reporting Program for Off-Site Water Facility Alternative

**Table A
Mitigation Monitoring and Reporting Plan for the Folsom South of U.S. Highway 50 Specific Plan Project
Revised Proposed Off-site Water Facility Alternative¹**

Mitigation Measure	Timing	Implementation ²	Enforcement ³	Dated Signature for Verification of Compliance
<p>purposes.</p> <ul style="list-style-type: none"> Plant materials chosen shall be species which do not present any safety hazards, which allow native flora to reestablish in the area, and which require minimal maintenance, including watering, pest control, and clean-up of litter from fruit and droppings. 				
<p>Mitigation Measure 3B.1-3a: Conformance to Construction Lighting Standards. The City shall limit construction to daylight hours to the extent possible. If nighttime lighting or construction is necessary, the City shall ensure that unshielded lights, reflectors, or spotlights are not located and directed to shine toward or be directly visible from adjacent properties or streets. To the extent possible, the City shall minimize the use of nighttime construction lighting within 500 feet of existing residences. This measure shall be identified on grading plans and in construction contracts.</p>	<p>Prior to approval of grading plans and building permits for facilities.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3B.1-3b: Prepare and Submit a Lighting Master Plan. The City shall prepare a Lighting Master Plan that covers the Revised Proposed Off-site Water Facility Alternative. The Lighting Master Plan shall include the following minimum requirements:</p> <ul style="list-style-type: none"> Outdoor lighting shall be properly shielded and installed to prevent light trespass on adjacent properties; Flood or spot lamps installed as part of the Revised Proposed Off-site Water Facility Alternative shall be aimed no higher than 45 degrees above straight down (halfway between straight down and straight to the side) when the source is visible from any off-site residential property or public roadway; Prohibit the use of harsh mercury vapor, low-pressure sodium, or fluorescent bulbs for public lighting in residential neighborhoods; and Comply with requirements of local jurisdiction, if applicable. 	<p>Prior to approval of grading plans and building permits for booster pump facilities.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>3B.2 AIR QUALITY</p> <p>Mitigation Measure 3B.2-1a: Develop and Implement a Construction NOX Reduction Plan. Consistent with SMAQMD requirements, the City of Folsom shall provide a plan for demonstrating that the heavy-duty (> 50 horsepower) off-road vehicles to be used in the construction project, including owned, leased and subcontractor vehicles, will achieve a project wide fleet-average 20% NOX</p>	<p>Prior to construction of the Revised Proposed Off-site Water Facility</p>	<p>City of Folsom Utilities Department</p>	<p>1. City of Folsom Community Development Department 2. SMAQMD</p>	

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Mitigation Measure	Timing	Implementation ²	Enforcement ³	Dated Signature for Verification of Compliance
<p>reduction. Prior to construction, the City's contractor shall submit to the SMAQMD a comprehensive inventory of all off-road construction equipment, equal to or greater than 50 horsepower, that will be used an aggregate of 40 or more hours during any portion of the construction of the Revised Proposed Off-site Water Facility. The inventory shall include the horsepower rating, engine production year, and projected hours of use or fuel throughput for each piece of equipment. The inventory shall be updated and submitted quarterly throughout the duration of the project, except that an inventory shall not be required for any 30-day period in which no construction activity occurs. At least 48 hours prior to the use of subject heavy-duty off-road equipment, the Revised Proposed Off-site Water Facility representative shall provide SMAQMD with the anticipated construction timeline including start date, and name and phone number of the project manager and on-site foreman.</p>	<p>Alternative.</p>			
<p>Mitigation Measure 3B.9-1b: Properly Dispose of Hydrostatic Test Water and Construction Dewatering in Accordance with the Central Valley Regional Water Quality Control Board. All hydrostatic test water and construction dewatering shall be discharged to an approved land disposal area or drainage facility in accordance with Central Valley RWQCB requirements. The City or its construction contractor shall provide the Central Valley RWQCB with the location, type of discharge, and methods of treatment and monitoring for all hydrostatic test water discharges. Emphasis shall be placed on those discharges that would occur directly to surface water bodies.</p>	<p>Prior to construction of the Revised Proposed Off-site Water Facility Alternative.</p>	<p>City of Folsom Utilities Department</p>	<p>1. City of Folsom Community Development Department 2. SMAQMD</p>	
<p>Mitigation Measure 3B.2-1c: Implement Fugitive Dust Control Measures and a Particulate Matter Monitoring Program during Construction. The City shall implement fugitive dust control measures and a particulate matter monitoring program during construction. The City shall ensure implementation of dust control measures and a particulate matter monitoring program during each phase of construction. Dust control measures may include, but are not limited to, the following:</p> <ul style="list-style-type: none"> • Minimize on-site construction vehicle speeds on unpaved surfaces; • Post speed limits; • Suspend grading operations when wind is sufficient to generate visible dust clouds; • Pave, water, use gravel, cover, or spray a dust-control agent on all haul roads. 	<p>Prior to construction of the Revised Proposed Off-site Water Facility Alternative.</p>	<p>City of Folsom Utilities Department</p>	<p>1. City of Folsom Community Development Department 2. SMAQMD</p>	

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Mitigation Measure	Timing	Implementation ²	Enforcement ³	Dated Signature for Verification of Compliance
<ul style="list-style-type: none"> • Prohibit no open burning of vegetation during project construction; • Chip or deliver vegetative material to waste-to-energy facilities; • Reestablish vegetation as soon as possible after construction and maintain vegetation consistent with the parameters established in Mitigation Measure 3B.2.1a; • Clean earthmoving construction equipment with water once daily and clean all haul trucks leaving the site; and • Water and keep moist exposed earth surfaces, graded areas, storage piles, and haul roads as needed to prevent fugitive dust. 				
<p>Mitigation Measure 3B.2-3a: Cite Pump Siting Buffers Away from Sensitive Receptors. New pumping stations including back-up diesel generators shall be located more than 200 feet away from sensitive receptors. Electrically-powered pumps shall be used to power new pumps, to the extent practicable.</p>	<p>Prior to the approval of grading plans and building permits for the water pumping facilities associated with the Revised Proposed Off-site Water Alternative.</p>	<p>City of Folsom Utilities Department</p>	<p>1. City of Folsom Community Development Department 2. SMAQMD</p>	
<p>Mitigation Measure 3B.2-3b: Conduct Project-Level DPM Screening and Implement Measures to Reduce Annual DPM to Acceptable Concentrations. Screening-level DPM assessments shall be conducted for diesel-powered pump operations proposed within 200 feet of residences or other sensitive receptors. These analyses should include exact distances between the receptors and operations, and include the actual DPM emissions for the engines proposed. If the analysis shows an annual average DPM concentration from project operations at residences within 200 feet of the DPM source to be greater than 0.024µg/m³, the engine location shall be moved to a location where the annual average DPM concentration from project emissions at the residences is less than 0.024µg/m³. The acceptable concentration of 0.024µg/m³ was determined using the current OEHHA cancer potency factor and methodology for diesel exhaust (OEHHA 2003). If diesel exhaust concentrations at the affected receptor would be below 0.024µg/m³, then the cancer health risk would be less than 9.9 cancers in a population of a million.</p>	<p>Prior to the approval of grading plans and building permits for the water pumping facilities associated with the Revised Proposed Off-site Water Alternative.</p>	<p>City of Folsom Utilities Department</p>	<p>1. City of Folsom Community Development Department 2. SMAQMD</p>	

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<p>3B.4 CLIMATE CHANGE</p> <p>Mitigation Measure 3B.4-1a: Implement CHG Reduction Measures during Construction. The bid specifications for construction of the Revised Proposed Off-site Water Facility Alternative shall require that bidders demonstrate how they will comply with each of the following measures during all construction and demolition activities:</p> <ol style="list-style-type: none"> 1) Construction vehicles and equipment will be properly maintained at all times in accordance with manufacturer's specifications, including proper tuning and timing of engines. Equipment maintenance records and equipment design specification data sheets shall be kept on-site during construction and demolition activities and subject to inspection by the SMAQMD. 2) Operators will turn off all construction vehicles and equipment and all delivery vehicles when not in use, and not allow idling for more than 5 minutes or for such other more restrictive time as may be required in law or regulation. 3) On-site construction vehicles and equipment will use ARB-certified biodiesel fuel if available (a minimum of B20, or 20 percent of biodiesel) except for those with warranties that would be voided if B20 biodiesel fuel were used. Prior to issuance of grading or demolition permits, the contractor shall provide documentation to the City that verifies whether any equipment is exempt; that a biodiesel supply has been secured; and that the construction contractor is aware that the use of biodiesel is required. 4) A City-approved Solid Waste Diversion and Recycling Plan (or such other documentation to the satisfaction of the City) will be in place for the Off-site Water Facilities that demonstrates the diversion from landfills and recycling of all nonhazardous, salvageable and re-usable wood, metal, plastic and paper products during construction and demolition activities. The Plan or other documentation shall include the name of the waste hauler, their assumed destination for all waste and recycled materials, and the procedures that will be followed to ensure implementation of this measure. 	<p>Prior to the approval of grading plans and building permits for the Revised Proposed Off-site Water Facility Alternative.</p>	<p>City of Folsom Utilities Department</p>	<p>1. City of Folsom Community Development Department 2. SMAQMD</p>	
<p>Mitigation Measure 3B.4-1b Prepare and Implement an Off-site Water Facilities Climate Action Plan. Prior to operation, the City shall have in place a</p>	<p>Prior to the approval of</p>	<p>City of Folsom Utilities Department</p>	<p>1. City of Folsom Community Development Department</p>	

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Mitigation Measure	Timing	Implementation ²	Enforcement ³	Dated Signature for Verification of Compliance
<p>Climate Action Plan and Greenhouse Reduction Strategy (Plan) for the Revised Proposed Off-site Water Facility Alternative that has been adopted by the City following an opportunity for review and recommendation by the SMAQMD. At a minimum, the Plan shall include:</p> <ul style="list-style-type: none"> • Designation of Person Responsible for Implementation. The Plan shall designate the name and contact information of the person(s) responsible for ensuring continuous and on-going implementation of the Plan. • GHG Inventory and Reduction Target. The City shall prepare a complete GHG Inventory for the Revised Proposed Off-site Water Facilities components. • Off-site Water Facilities Design Features. The Revised Proposed Off-site Water Facilities shall include design features to reduce operational GHG emissions, as well as an estimate of the reduction in GHG emissions that is expected to result from each facility. Initial measures that may be considered include, but are not limited to: <ul style="list-style-type: none"> ○ design all conditioned occupancies with "cool roofs" using products certified by the Cool Roof Rating Council, and other exposed roof surfaces coated with "cool paints"; ○ design all conditioned occupancies to take advantage of shade through the planting of deciduous canopy-type trees and/or prevailing winds to reduce energy use; ○ make maximum use of EnergyStar-qualified energy efficient appliances, heating and cooling systems, office equipment and lighting products; ○ install a photovoltaic array (solar panels) or other source of renewable energy generation on-site, or otherwise acquire energy that has been generated by renewable sources to meet a portion of the electricity needs of the Offsite Water Facilities; and ○ in an effort to reduce GHG emissions from transportation sources, the bid specifications for the Revised Proposed Offsite Water Facilities should require that bidders 	<p>grading plans and building permits for the Revised Proposed Off-site Water Facility Alternative.</p>		<p>2. SMAQMD</p>	

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Mitigation Measure	Timing	Implementation ²	Enforcement ³	Dated Signature for Verification of Compliance
<p>demonstrate that they have given preference to local sources of building materials or offer evidence to support why such local sources have not been used.</p>				
3B.5 CULTURAL RESOURCES				
<p>Mitigation Measure 3A.5-1a: Comply with the Programmatic Agreement. The PA for the proposed FPA project is incorporated by reference. The PA provides a management framework for identifying historic properties, determining adverse effects, and resolving those adverse effects as required under Section 106 of the NHPA. This document is incorporated by reference. The PA is available for public inspection and review at the California Office of Historic Preservation 1725 23rd Street Sacramento, CA 95816.</p>	<p>The PA shall be prepared and executed (signed) prior to issuance of any Federal permit or authorization for any aspect or component of the specific plan project.</p>	<p>USACE (or designee) and the project applicant(s) of all project phases (as directed by USACE)</p>	<p>1. USACE 2. The City of Folsom Community Development Department of all project phases (as directed by USACE), with oversight by the SRPO</p>	
<p>Mitigation Measure 3A.5-1b: Perform an Inventory and Evaluation of Cultural Resources for the California Register of Historic Places, Minimize or Avoid Damage or Destruction, and Perform Treatment Where Damage or Destruction Cannot be Avoided. Management of cultural resources eligible for or listed on the CRHR under CEQA mirrors management steps required under Section 106. These steps may be combined with deliverables and management steps performed for Section 106 provided that management documents prepared for the PA also clearly reference the CRHR listing criteria and significance thresholds that apply under CEQA. Prior to ground disturbing work for each individual development phase of the Revised Proposed Off-site Water Facility Alternative, the City shall perform the following actions:</p> <ul style="list-style-type: none"> Retain the services of a qualified archaeologist to perform an inventory of cultural resources within each individual development phase or off-site element subject to approval under CEQA. Identified resources shall be evaluated for listing on the CRHR. The inventory report shall also identify locations that are sensitive for undiscovered cultural resources based upon the location of known resources, geomorphology, and topography. The inventory report shall specify the location of monitoring of ground-disturbing work in these areas by a qualified archaeologist, and monitoring in the vicinity of identified resources that may be damaged by construction, if appropriate. The identification of sensitive locations subject to monitoring during construction of each individual development phase shall be performed in concert with monitoring activities performed under the PA to minimize the potential 	<p>Before issuance of building permits and ground-disturbing activities.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	

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Mitigation Measure	Timing	Implementation ²	Enforcement ³	Dated Signature for Verification of Compliance
<p>for conflicting requirements. For each resource that is determined eligible for the CRHR, the applicable agency or the project applicant(s) for any particular discretionary development (under the agency's direction) shall obtain the services of a qualified archaeologist who shall determine if implementation of the individual project development would result in damage or destruction of "significant" (under CEQA) cultural resources. These findings shall be reviewed by the applicable agency for consistency with the significance thresholds and treatment measures provided in this EIR/EIS.</p> <ul style="list-style-type: none"> • Where possible, the project shall be configured or redesigned to avoid impacts on eligible or listed resources. Alternatively, these resources may be preserved in place if possible, as suggested under California Public Resources Code Section 21083.2. Avoidance of historic properties is required under certain circumstances under the Public Resource Code and 36 CFR Part 800. • Where impacts cannot be avoided, the applicable agency or the project applicant(s) of all project phases (under the applicable agency's direction) shall prepare and implement treatment measures that are determined to be necessary by a qualified archaeologist. These measures may consist of data recovery excavations for resources that are eligible for listing because of the data they contain (which may contribute to research). Alternatively, for historical architectural, engineered, or landscape features, treatment measures may consist of a preparation of interpretive, narrative, or photographic documentation. These measures shall be reviewed by the applicable oversight agency for consistency with the significance thresholds and standards provided in this EIR/EIS. • To support the evaluation and treatment required under this mitigation measure, the archaeologist retained by either the applicable oversight agency or the project applicant(s) of all project phases shall prepare an appropriate prehistoric and historic context that identifies relevant prehistoric, ethnographic, and historic themes and research questions against which to determine the significance of identified resources and appropriate treatment. • These steps and documents may be combined with the phasing of management and documents prepared pursuant to the PA to minimize the potential for inconsistency and duplicative management efforts. 				

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Mitigation Measure	Timing	Implementation²	Enforcement³	Dated Signature for Verification of Compliance
<p>Mitigation Measure 3A.5-2: Conduct Construction Personnel Education, Conduct On-Site Monitoring if Required, Stop Work if Cultural Resources are Discovered, Assess the Significance of the Find, and Perform Treatment or Avoidance as Required. To reduce potential impacts to previously undiscovered cultural resources, the project applicant(s) of all project phases shall do the following:</p> <ul style="list-style-type: none"> • Before the start of ground-disturbing activities, the project applicant(s) of all project phases shall retain a qualified archaeologist to conduct training for construction workers as necessary based upon the sensitivity of the project APE, to educate them about the possibility of encountering buried cultural resources, and inform them of the proper procedures should cultural resources be encountered. • As a result of the work conducted for Mitigation Measures 3A.5-1a and 3A.5-1b, if the archaeologist determines that any portion of the SPA or the off-site elements should be monitored for potential discovery of as-yet-unknown cultural resources, the project applicant(s) of all project phases shall implement such monitoring in the locations specified by the archaeologist. USACE should review and approve any recommendations by archaeologists with respect to monitoring. • Should any cultural resources, such as structural features, unusual amounts of bone or shell, artifacts, or architectural remains be encountered during any construction activities, work shall be suspended in the vicinity of the find and the appropriate oversight agency(ies) (identified below) shall be notified immediately. The appropriate oversight agency(ies) shall retain a qualified archaeologist who shall conduct a field investigation of the specific site and shall assess the significance of the find by evaluating the resource for eligibility for listing on the CRHR and the NRHP. If the resource is eligible for listing on the CRHR or NRHP and it would be subject to disturbance or destruction, the actions required in Mitigation Measures 3A.5-1a and 3A.5-1b shall be implemented. The oversight agency shall be responsible for approval of recommended mitigation if it is determined to be feasible in light of the approved land uses, and shall implement the approved mitigation before resuming construction activities at the archaeological site. 	<p>Before and during ground-disturbing activities.</p>	<p>City of Folsom Utilities Department</p>	<p>1. For actions taken to satisfy the requirements of Section 106: the SHPO and USACE 2. City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3A.5-3: Suspend Ground-Disturbing Activities if Human Remains are Encountered and Comply with California Health and</p>	<p>Upon the discovery of</p>	<p>City of Folsom Utilities Department</p>	<p>1. For actions taken to satisfy the requirements of Section</p>	

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Mitigation Measure	Timing	Implementation ²	Enforcement ¹	Dated Signature for Verification of Compliance
<p>Safety Code Procedures. In accordance with the California Health and Safety Code, if human remains are uncovered during ground-disturbing activities, including those associated with off-site elements, the project applicant(s) of all project phases shall immediately halt all ground-disturbing activities in the area of the find and notify the applicable county coroner and a professional archaeologist skilled in osteological analysis to determine the nature of the remains. The coroner is required to examine all discoveries of human remains within 48 hours of receiving notice of a discovery on private or public lands (California Health and Safety Code Section 7050.5[b]). If the coroner determines that the remains are those of a Native American, he or she must contact the NAHC by phone within 24 hours of making that determination (California Health and Safety Code Section 7050[e]). After the coroner's findings are complete, the project applicant(s), an archaeologist, and the NAHC-designated MLD shall determine the ultimate treatment and disposition of the remains and take appropriate steps to ensure that additional human interments are not disturbed. The responsibilities for acting on notification of a discovery of Native American human remains are identified in Section 5097.9 of the California Public Resources Code. Upon the discovery of Native American remains, the procedures above regarding involvement of the applicable county coroner, notification of the NAHC, and identification of an MLD shall be followed. The project applicant(s) of all project phases shall ensure that the immediate vicinity (according to generally accepted cultural or archaeological standards and practices) is not damaged or disturbed by further development activity until consultation with the MLD has taken place. The MLD shall have at least 48 hours after being granted access to the site to inspect the site and make recommendations. A range of possible treatments for the remains may be discussed: nondestructive removal and analysis, preservation in place, relinquishment of the remains and associated items to the descendants, or other culturally appropriate treatment. As suggested by Assembly Bill (AB) 2641 (Chapter 863, Statutes of 2006), the concerned parties may extend discussions beyond the initial 48 hours to allow for the discovery of additional remains. AB 2641(e) includes a list of site protection measures and states that the project applicant(s) shall comply with one or more of the following requirements:</p> <ul style="list-style-type: none"> • Record the site with the NAHC or the appropriate Information Center, • Use an open-space or conservation zoning designation or easement, or • Record a document with the county in which the property is located. <p>The project applicant(s) or its authorized representative of all project phases shall</p>	<p>suspected human remains.</p>		<p>106: the SHPO and USACE</p> <p>2. For all project-related improvements that would be located within the City of Folsom: City of Folsom Community Development Department</p>	

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<p>rebury the Native American human remains and associated grave goods with appropriate dignity on the property in a location not subject to further subsurface disturbance if the NAHC is unable to identify an MLD or if the MLD fails to make a recommendation within 48 hours after being granted access to the site. The project applicant(s) or its authorized representative may also reinter the remains in a location not subject to further disturbance if it rejects the recommendation of the MLD and mediation by the NAHC fails to provide measures acceptable to the landowner. Ground disturbance in the zone of suspended activity shall not recommence without authorization from the archaeologist.</p>				
<p>3B.7 GEOLOGY, SOILS, MINERALS, AND PALEONTOLOGICAL RESOURCES</p>				
<p>Mitigation Measure 3B.7-1a: Prepare Geotechnical Report(s) for the Revised Proposed Off-site Water Facilities and Implement Required Measures. Facility design for the Revised Proposed Off-site Water Facility Alternative shall comply with the site-specific design recommendations as provided by a licensed geotechnical or civil engineer to be retained by the City. The final geotechnical and/or civil engineering report shall address and make recommendations on the following:</p>	<p>Prior to completion of engineering plans for the Revised Proposed Off-site Water Facility Alternative.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<ul style="list-style-type: none"> • Site preparation; • Soil bearing capacity; • Appropriate sources and types of fill; • Potential need for soil amendments; • Road, pavement, and parking areas; • Structural foundations, including retaining-wall design; • Grading practices; • Soil corrosion of concrete and steel; • Erosion/winterization; • Seismic ground shaking; • Liquefaction; and 				

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City of Folsom
 Mitigation Monitoring and Reporting Program

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<ul style="list-style-type: none"> Expansive/unstable soils. <p>In addition to the recommendations for the conditions listed above, the geotechnical investigation shall include subsurface testing of soil and groundwater conditions, and shall determine appropriate foundation designs that are consistent with the version of the CBC that is applicable at the time building and grading permits are applied for. All recommendations contained in the final geotechnical engineering report shall be implemented by the City.</p>				
<p>Mitigation Measure 3B.7-1b: Incorporate Pipeline Failure Contingency Measures Into Final Pipeline Design. Isolation valves or similar devices shall be incorporated into all pipeline facilities to prevent substantial losses of surface water in the event of pipeline rupture, as recommended by a licensed geotechnical or civil engineer. The specifications of the isolation valves shall conform to the CBC and American Water Works Association standards.</p>	<p>Prior to completion of engineering plans for the Revised Proposed Off-site Water Facility Alternative.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3B.7-4: Implement Corrosion Protection Measures. As determined appropriate by a licensed geotechnical or civil engineer, the City shall ensure that all underground metallic fittings, appurtenances, and piping include a cathodic protection system to protect these facilities from corrosion.</p>	<p>Prior to completion of engineering plans for the Revised Proposed Off-site Water Facility Alternative.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3B.7-5: Conduct Construction Personnel Education, Stop Work if Paleontological Resources are Discovered, Assess the Significance of the Find, and Prepare and Implement a Recovery Plan as Required. To minimize potential adverse impacts on previously unknown potentially unique, scientifically important paleontological resources, the City shall implement appropriate measures during construction of the Revised Proposed Offsite Water Facility Alternative. These measures shall be required for construction activities at the following:</p> <ul style="list-style-type: none"> Before the start of any earthmoving activities, the project applicant(s) shall retain a qualified paleontologist or archaeologist to train all construction personnel involved with earthmoving activities, including the site superintendent, regarding the possibility of encountering fossils, the appearance and types of fossils likely to be seen during construction, and proper notification procedures should fossils be encountered. 	<p>During earthmoving activities.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	

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<ul style="list-style-type: none"> If paleontological resources are discovered during earthmoving activities, the construction crew shall immediately cease work in the vicinity of the find and notify Sacramento County Planning and Community Development Department. The project applicant(s) shall retain a qualified paleontologist to evaluate the resource and prepare a recovery plan in accordance with Society of Vertebrate Paleontology guidelines (1996). The recovery plan may include, but is not limited to, a field survey, construction monitoring, sampling and data recovery procedures, museum storage coordination for any specimen recovered, and a report of findings. Recommendations in the recovery plan that are determined by the County to be necessary and feasible shall be implemented before construction activities can resume at the site where the paleontological resources were discovered. 				
<p>3B.8 HAZARDOUS AND HAZARDOUS MATERIALS</p>				
<p>Mitigation Measure 3B.8-1a: Transport, Store, and Handle Construction-Related Hazardous Materials in Compliance with Relevant Regulations and Guidelines. The City shall ensure, through the enforcement of contractual obligations, that all contractors transport, store, and handle construction-related hazardous materials in a manner consistent with relevant regulations and guidelines, including those recommended and enforced by Caltrans, Central Valley RWQCB, local fire departments, and the County environmental health department. Recommendations shall include as appropriate transporting and storing materials in appropriate and approved containers, maintaining required clearances, and handling materials using applicable Federal, state and/or local regulatory agency protocols. In addition, all precautions required by the Central Valley RWQCB-issued NPDES construction activity stormwater permits shall be taken to ensure that no hazardous materials enter any nearby waterways.</p>	<p>Prior to construction and operation the Revised Proposed Off-site Water Facility Alternative</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>In the event of a spill, the City shall ensure, through the enforcement of contractual obligations, that all contractors immediately control the source of any leak and immediately contain any spill utilizing appropriate spill containment and countermeasures. If required by the local fire departments, the local environmental health department, or any other regulatory agency, contaminated media shall be collected and disposed of at an off-site facility approved to accept such media.</p> <p>The storage, handling, and use of the construction-related hazardous materials shall be in accordance with applicable Federal, state, and local laws. Construction-related hazardous materials and hazardous wastes (e.g., fuels and waste oils) shall be stored away from stream channels and steep banks to prevent</p>				

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<p>these materials from entering surface waters in the event of an accidental release. These materials shall be kept at sufficient distance (at least 500 feet) from nearby residences or other sensitive land uses. This includes materials stored for expected use, materials in equipment and vehicles, and waste materials.</p>				
<p>Mitigation Measure 3B.8-1b: Prepare and Implement a Hazardous Materials Management Plan. The City shall prepare a Hazardous Materials Management Plan (HMMP) for the proposed WTP. The HMMP shall provide for safe storage, containment, and disposal of chemicals and hazardous materials related to WTP operations, including waste materials. The plan shall include, but shall not be limited to, the following:</p> <ul style="list-style-type: none"> ▪ A description of hazardous materials and hazardous wastes; ▪ A description of handling, transport, treatment, and disposal procedures, as relevant for each hazardous material or hazardous waste; ▪ Preparedness, prevention, contingency, and emergency procedures, including emergency contact information; ▪ A description of personnel training including, but not limited to: (1) recognition of existing or potential hazards resulting from accidental spills or other releases; (2) implementation of evacuation, notification, and other emergency response procedures; (3) management, awareness, and handling of hazardous materials and hazardous wastes, as required by their level of responsibility; ▪ Instructions on keeping Materials Safety and Data Sheets (MSDS) on-site for each on-site, hazardous chemical; ▪ Identification of the locations of hazardous material storage areas, including temporary storage areas, which shall be equipped with secondary containment sufficient in size to contain the volume of the largest container or tank; and ▪ A description of equipment maintenance procedures. <p>The HMMP shall be made a condition of contractual obligation and shall be available for review by construction inspectors and implementation compliance shall be monitored.</p>	<p>Prior to construction and operation the Revised Proposed Off-site Water Facility Alternative</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	

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Mitigation Measure	Timing	Implementation ²	Enforcement ³	Dated Signature for Verification of Compliance
<p>Mitigation Measure 3B.8-5a: Conduct Phase 1 Environmental Site Assessment for Selected Alignment. Prior to construction, the City shall conduct a Phase 1 Environmental Site Assessment according to American Society for Testing and Materials (ASTM) protocol for the selected conveyance pipeline alignment, pump station, well, and WTP site. If any hazardous materials or waste sites are identified during the Phase 1 Environmental Site Assessment, the City shall implement Mitigation Measure 3.8-5b.</p>	<p>Prior to construction and operation the Revised Proposed Off-site Water Facility Alternative</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3B.8-5b: Develop and Implement a Remediation Plan. If determined necessary to mitigate for potential hazards resulting from disturbance of existing contaminated areas, the extent of contamination from hazardous materials sites within or adjacent to the Off-site Water Facilities construction area shall be delineated during final design. Disturbance to contaminated areas during construction shall be avoided, or any work done within contaminated areas shall be undertaken in compliance with standards approved by the DTSC or Sacramento County Department of Environmental Health to ensure that hazardous materials will not be released as a result of the ground disturbance. Additionally, if unidentified contaminated soil or groundwater are encountered, or if suspected contamination is encountered during any construction activities, work shall be halted in the area of potential exposure, and the type and extent of contamination shall be identified. A qualified professional, in consultation with appropriate regulatory agencies, will then develop and implement a plan to remediate the contamination and properly dispose of the contaminated material.</p>	<p>Prior to construction and operation the Revised Proposed Off-site Water Facility Alternative</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3B.8-7a: Keep Construction Area Clear of Combustible Materials. The City shall ensure, through the enforcement of contractual obligations that during construction, staging areas, welding areas, or areas slated for development using spark-producing equipment shall be cleared of dried vegetation or other materials that could serve as fire fuel. The contractor shall keep these areas clear of combustible materials in order to maintain a firebreak. Any construction equipment that normally includes a spark arrester shall be equipped with an arrester in good working order. This includes, but is not limited to, vehicles, heavy equipment, and chainsaws.</p>	<p>Prior to construction and operation the Revised Proposed Off-site Water Facility Alternative</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3B.8-7b: Provide Accessible Fire Suppression Equipment. Work crews shall be required to carry or have sufficient fire suppression equipment to ensure that any fire resulting from construction activities is immediately extinguished. All off-road equipment using internal combustion engines shall be equipped with spark arrestors.</p>	<p>Prior to construction and operation the Revised Proposed Off-site Water Facility</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	

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Mitigation Measure	Timing Alternative	Implementation ²	Enforcement ³	Dated Signature for Verification of Compliance
<p>3B.9 HYDROLOGY AND WATER QUALITY</p> <p>Mitigation Measure 3B.9-1a: Acquire Appropriate Regulatory Permits and Prepare and Implement SWPPP and BMPs. The City shall prepare a SWPPP specific to the Revised Proposed selected Off-site Water Facility Alternative and secure coverage under SWRCB's NPDES stormwater permit for general construction activity (Order 2009-0009-DWQ). The SWPPP shall identify specific actions and BMPs relating to the prevention of stormwater pollution from project-related construction sources by identifying a practical sequence for site restoration. BMP implementation, contingency measures, responsible parties, and agency contacts. The SWPPP shall reflect localized surface hydrological conditions and shall be reviewed and approved by the City prior to commencement of work and shall be made conditions of the contract with the contractor selected to build the Revised Proposed Off-site Water Facility Alternative. The SWPPP shall incorporate control measures in the following categories:</p> <ul style="list-style-type: none"> • Soil stabilization and erosion control practices (e.g., hydroseeding, erosion control blankets, mulching, etc.); • Dewatering and/or flow diversion practices, if required (see Mitigation Measure 3B.9-1b); • Sediment control practices (temporary sediment basins, fiber rolls, etc.); • Temporary and post-construction on- and off-site runoff controls; • Special considerations and BMPs for water crossings, wetlands, drainages, and vernal pools; • Monitoring protocols for discharge(s) and receiving waters, with emphasis placed on the following water quality objectives: dissolved oxygen, floating material, oil and grease, pH, and turbidity; • Waste management, handling, and disposal control practices; • Corrective action and spill contingency measures; • Agency and responsible party contact information, and Training 	<p>Develop SWPPP prior to construction of the Proposed Off-site Water Facility Alternative and implementation throughout construction.</p>	<p>City of Folsom Utilities Department</p>	<p>1. Central Valley Regional Water Quality Control Board 2. City of Folsom Community Development Department</p>	

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Mitigation Measure	Timing	Implementation²	Enforcement³	Dated Signature for Verification of Compliance
<p>procedures that shall be used to ensure that workers are aware of permit requirements and proper installation methods for BMPs specified in the SWPPP.</p> <p>The SWPPP shall be prepared by a qualified SWPPP practitioner with BMPs selected to achieve maximum pollutant removal and represent the best available technology that is economically achievable. Emphasis for BMPs shall be placed on controlling discharges of oxygen depleting substances, floating material, oil and grease, acidic or caustic substances or compounds, and turbidity. Performance and effectiveness of these BMPs shall be determined either by visual means where applicable (i.e., observation of above-normal sediment release), or by actual water sampling in cases where verification of contaminant reduction or elimination, (inadvertent petroleum release) as required to determine adequacy of the measure.</p>				
<p>Mitigation Measure 3B.9-1b: Property Dispose of Hydrostatic Test Water and Construction Dewatering in Accordance with the Central Valley Regional Water Quality Control Board. All hydrostatic test water and construction dewatering shall be discharged to an approved land disposal area or drainage facility in accordance with Central Valley RWQCB requirements. The City or its construction contractor shall provide the Central Valley RWQCB with the location, type of discharge, and methods of treatment and monitoring for all hydrostatic test water discharges. Emphasis shall be placed on those discharges that would occur directly to surface water bodies.</p>	<p>Incorporation measures into SWPPP prior to construction and implementation throughout construction, as appropriate</p>	<p>City of Folsom Utilities Department</p>	<p>1. Central Valley Regional Water Quality Control Board 2. City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3B.9-3a: Prepare and Implement Drainage Plan(s) for Structural Facilities. The City shall prepare a Drainage Plan for the Revised Proposed Off-site Water Facility Alternative and shall incorporate measures to maintain off-site runoff during peak conditions to preconstruction discharge levels. The Drainage Plan shall provide both short- and long-term drainage solutions to ensure the proper sequencing of drainage facilities during and following construction. The City shall evaluate options for on-site detention including, but not limited to, providing temporary storage within a portion or portions of proposed paved areas, linear infiltration facilities along the site perimeter, and/or other on-site opportunities for detention, retention, and/or infiltration facilities. Design specifications for the detention, retention, and/or infiltration facilities shall provide sufficient storage capacity to accommodate the 10-year, 24-hour storm event. In addition, the Drainage Plan shall delineate the overlaid release path for flows generated by a 100-year frequency storm, so that structural pad elevations for above ground facilities are placed a minimum of one foot above the property's highest frontage curb elevation. The Drainage Plan</p>	<p>Development of the Drainage Plan prior to start of construction.</p>	<p>City of Folsom Utilities Department</p>	<p>1. Central Valley Regional Water Quality Control Board 2. City of Folsom Community Development Department</p>	

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Mitigation Measure	Timing	Implementation ²	Enforcement ³	Dated Signature for Verification of Compliance
<p>shall also provide sufficient attenuation of flows to ensure no net increase in off-site discharges to waterways.</p> <p>Mitigation Measure 3B.9-3b: Ensure the Provision of Sufficient Outlet Protection and On-site Containment. Energy dissipaters, vegetated rip-rap, soil drain outlets to slow runoff velocities and prevent erosion at discharge locations for the WTP. A long-term maintenance plan shall be implemented for all drainage discharge control devices. The WTP layout shall also include sufficient on-site containment and pollution-control devices for drainage facilities to avoid the off-site release of water quality pollutants, oil and grease.</p>	<p>Incorporation of measures into the Drainage Plan prior to start of construction.</p>	<p>City of Folsom Utilities Department</p>	<p>1. Central Valley Regional Water Quality Control Board 2. City of Folsom Community Development Department</p>	
3B.11 NOISE				
<p>Mitigation Measure 3B.11-1a: Limit Construction Hours. Construction activities shall be limited to daylight hours between 7 a.m. and 7 p.m. Monday through Friday, and 9 a.m. and 5 p.m. on Saturday. No construction shall be allowed on Sundays or holidays.</p>	<p>During construction of The Revised Proposed Off-site Water Facility Alternative components.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3B.11-1b: Minimize Noise from Construction Equipment and Staging. Construction equipment noise shall be minimized during project construction by muffling and shielding intakes and exhaust on construction equipment (per the manufacturer's specifications) and by shrouding or shielding impact tools, where used. The City's construction specifications shall also require that the contractor select staging areas as far as feasibly possible from sensitive receptors.</p>	<p>During construction of The Revised Proposed Off-site Water Facility Alternative components.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3B.11-1c: Maximize the Use of Noise Barriers. Construction contractors shall locate fixed construction equipment (such as compressors and generators) and construction staging areas as far as possible from nearby residences. If feasible, noise barriers shall be used at the construction site and staging area. Temporary walls, stockpiles of excavated materials, or moveable sound barrier curtains would be appropriate in instances where construction noise would exceed 90 dBA and occur within less than 50 feet from a sensitive receptor. The final selection of noise barriers will be subject to the City's approval and shall provide a minimum 10 dBA reduction in construction noise levels.</p>	<p>Prior to and during construction.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3B.11-1d: Prohibit Non-Essential Noise Sources During Construction. No amplified sources (e.g., stereo "boom boxes") shall be used in</p>	<p>Prior to and during Construction.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	

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Mitigation Measure the vicinity of residences during project construction.	Timing	Implementation ²	Enforcement ³	Dated Signature for Verification of Compliance
<p>Mitigation Measure 3B.11-1e: Monitor Construction Noise and Provide a Mechanism for Filing Noise Complaints. An on-site complaint and enforcement manager shall track and respond to noise complaints. The City shall also provide a mechanism for residents, businesses, and agencies to register complaints with the City if construction noise levels are overly intrusive or construction occurs outside the required hours.</p>	<p>Prior to and during construction.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>Mitigation Measure 3B.11-3: Implement Operational Noise Minimization Measures. The following mitigation measures shall be implemented for the design of the WTP and the pump station(s) to ensure that operational noise levels at the property line do not exceed the City/County standards:</p> <ul style="list-style-type: none"> • Shielding and other specified measures as deemed appropriate and effective by the design engineer shall be incorporated into the design in order to comply with performance standards. • Pumps located underground shall be shielded to not affect nearby sensitive receptors. • Project equipment shall be outfitted and maintained with noise-reduction devices such as equipment closures, fan silencers, mufflers, acoustical louvers, noise barriers, and acoustical panels to minimize operational noise. • Particularly noisy equipment shall be located as far away as feasibly possible from nearby sensitive receptors. • The orientation of acoustical exits shall always be facing away from nearby sensitive receptors. Buildings and landscaping shall be incorporated, where possible, to absorb or redirect noise away from nearby sensitive receptors. 	<p>Approval of engineering plans for the Revised Proposed Off-site booster pumping facilities prior to Construction.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	
<p>3B.12 PARKS AND RECREATION</p>				
<p>Mitigation Measure 3B.12-1: Provide for Continued Recreational Access as Identified in Mitigation Measure 3.14-1a. As part of the Traffic Control Plan identified in Mitigation Measure 3.14-1a, the City shall ensure that bike trail access is maintained throughout the construction period through the use of detours. Proper signage shall be included in multiple locations, where necessary, to provide advance notice to hikers of up-coming construction activities.</p>	<p>Prior to and during construction.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	

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<p>3B.15 TRAFFIC AND TRANSPORTATION</p> <p>Mitigation Measure 3B.15-1a: Prepare Traffic Control Plan. Prior to construction, the City shall prepare a Traffic Control Plan for roadways and intersections affected by the Revised Proposed Off-site Water Facility's-related construction. The Traffic Control Plan shall designate haul routes and comply with requirements in the encroachment permits issued by the City. The Traffic Control Plan to be prepared by the construction contractor(s) shall, at minimum, include the following measures:</p> <ul style="list-style-type: none"> • Maintaining the maximum amount of travel lane capacity during non-construction periods, possible, and advanced notice to drivers through the provision of construction signage. • Maintaining alternate one-way traffic flow past the lay down area and site access when feasible. • Heavy trucks and other construction transport vehicles shall avoid the busiest commute hours (7 a.m. to 8 a.m. and 5 p.m. to 6 p.m. on weekdays). • The City shall provide a minimum 72-hour advance notice of access restrictions for residents, businesses, and local emergency response agencies. This shall include the identification of alternative routes and detours to enable for the avoidance of the immediate construction zone. • The City, in cooperation with its contractor(s), shall provide a phone number and community contact for inquiries about the schedule of the Revised Proposed Off-site Water Facilities throughout the construction period. This information will be posted in a local newspaper, via the City's web site, or at City Hall and will be updated on a monthly basis. • To the extent practical and depending on the alignment of the Revised Proposed Off-site Water Facility Alternative, the City shall maximize opportunities for coordinated construction and installation of the conveyance pipeline with other planned roadway improvement projects. <p>Mitigation Measure 3B.15-1b: Assess Pre-Off-site Water Facilities Roadway Conditions. Prior to construction, the City's construction contractor(s) shall be responsible for assessing current road conditions for Revised Proposed Off-site</p>	<p>Prior to and during construction.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department</p>	

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<p>Water Facility Alternative's haul routes including the local access roads and develop post construction road restoration requirements. As part of the encroachment permitting process, an agreement shall be entered into with applicable jurisdictions prior to construction that details post construction road restoration requirements. The City and/or its contractor shall perform roadway repairs or rehabilitation as necessary such that post construction requirements are met.</p>				
<p>3B.16 UTILITIES AND SERVICE SYSTEMS</p>				
<p>Mitigation Measure 3B.16-3a: Minimize Utility Conflicts by Implementing an Underground Services Alert. Underground utilities and service connections shall be identified prior to commencing any excavation work through the implementation of an Underground Services Alert (USA). The exact utility locations will be determined by hand-excavated test pits dug at locations determined and approved by the construction manager (also referred to as "pot-holing"). Temporary disruption of service may be required to allow for construction. No service on such lines would be disrupted until prior approval is received from the construction manager and the service provider.</p>	Prior to and during construction.	City of Folsom Utilities Department	City of Folsom Utilities Department	
<p>Mitigation Measure 3B.16-3b: Coordinate with Utility Providers and Implement Appropriate Installation Methods to Minimize Potential Utility Service Disruptions. Prior to installation, the City determine proper installation methods and final design criteria to minimize the potential for disruptions to existing and planned utilities.</p>		City of Folsom Utilities Department	City of Folsom Utilities Department	
<p>3B.17 GROUNDWATER</p>				
<p>Mitigation Measure 3B.17-1a: Implement Construction Dewatering Best Management Practices. During construction at site locations containing high groundwater, if groundwater from dewatering activities cannot be contained within the construction area, it shall be pumped to an authorized onsite land area, existing detention facilities, or Baker tanks or equivalent with sufficient capacity to control the volume of groundwater. Tanks shall be equipped with a gel coagulant, a filter system, and/or other containment to remove sediment. The Revised Proposed Off-site Water Facility Alternative's Stormwater Pollution Prevention Plan (SWPPP) shall include BMPs, as appropriate, to retain, treat, and dispose of groundwater from dewatering activities. Measures shall include, but not limited to, the following:</p> <ul style="list-style-type: none"> Temporarily retain pumped groundwater, as appropriate, to reduce turbidity and concentrations of suspended sediments before discharge to surface waterways; 	Prior to and during construction.	City of Folsom Utilities Department	<ol style="list-style-type: none"> California Department of Fish and Game or Regional Water Quality Control Board City of Folsom Community Development Department 	

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<ul style="list-style-type: none"> Convey pumped groundwater to a suitable land disposal area capable of percolating flows; and/or Incorporate other applicable measures from the Caltrans Storm Water Quality Handbook, Section 7: Dewatering Operations (2004). 	Prior to and during construction.	City of Folsom Utilities Department	<ol style="list-style-type: none"> California Department of Fish and Game or Regional Water Quality Control Board City of Folsom Community Development Department 	
<p>3B.17 WATER SUPPLY</p> <p>Mitigation Measure 3B.17-1b: Implement a Dewatering Discharge Monitoring Program. A groundwater discharge monitoring program shall be implemented to ensure that receiving water quality does not exceed levels that impact beneficial uses. If monitoring reveals that water quality would impact these beneficial uses, discharges to surface waterways shall be reduced or diluted to acceptable levels, or terminated. If discharges are reduced or terminated, groundwater shall be disposed through land application. Groundwater collected during dewatering shall be tested for contamination prior to disposal and comply with Central Valley RWQCB requirements.</p>				
<p>3B.18 WATER SUPPLY</p> <p>Mitigation Measure 3A.18-1: Submit Proof of Surface Water Supply Availability.</p>	Before approval of final maps and issuance of building permits for any project phases.	City of Folsom Utilities Department	City of Folsom Community Development Department and City of Folsom Public Works Department.	
<ul style="list-style-type: none"> Prior to approval of any small-lot tentative subdivision map subject to Government Code Section 66473.7 (SB 221), the City shall comply with that statute. Prior to approval of any small-lot tentative subdivision map for a proposed residential project not subject to that statute, the City need not comply with Section 66473.7, or formally consult with any public water system that would provide water to the affected area; nevertheless, the City shall make a factual showing or impose conditions similar to those required by Section 66473.7 to ensure an adequate water supply for development authorized by the map. Prior to recordation of each final subdivision map, or prior to City approval of any similar project-specific discretionary approval or entitlement required for nonresidential uses, the project applicant(s) of that project phase or activity shall demonstrate the availability of a reliable and sufficient water supply from a public water system for the amount of development that would be authorized by the final subdivision map or project-specific discretionary nonresidential approval or entitlement. Such a demonstration shall consist of 				

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<p>information showing that both existing sources are available or needed supplies and improvements will be in place prior to occupancy.</p> <p>Mitigation Measure 3A.18-2a: Submittal Proof of Adequate Off-Site Water Conveyance Facilities and Implement Off-Site Infrastructure Service System or Ensure That Adequate Financing Is Secured. Before the approval of the final subdivision map and issuance of building permits for all project phases, the project applicant(s) of any particular discretionary development application shall submit proof to the City of Folsom that an adequate off-site water conveyance system either has been constructed or is ensured or other surties to the City's satisfaction. The off-site water conveyance infrastructure sufficient to provide adequate service to the project shall be in place for the amount of development identified in the tentative map before approval of the final subdivision map and issuance of building permits for all project phases. Their financing shall be ensured to the satisfaction of the City. A certificate of occupancy shall not be issued for any building within the SPA until the water conveyance infrastructure sufficient to serve such building has been constructed and is in place.</p>	<p>Before approval of final maps and issuance of building permits for any project phases.</p>	<p>City of Folsom Utilities Department</p>	<p>City of Folsom Community Development Department and City of Folsom Public Works Department.</p>	

Attachment 22
Site Photographs

